

**WHEELING PLAN COMMISSION
THURSDAY, JANUARY 14, 2016 6:30 P.M.**

**AGENDA FOR A REGULAR MEETING OF THE PLAN COMMISSION
to be held in the Board Room of the Village Hall
2 Community Boulevard, Wheeling, Illinois**

Revised January 8, 2016

THIS MEETING WILL BE TELEVISED ON WHEELING'S CABLE CHANNELS 17 & 99

1. **CALL TO ORDER**
2. **PLEDGE OF ALLEGIANCE**
3. **ROLL CALL**
4. **CHANGES TO THE AGENDA**
5. **CITIZEN CONCERNS AND COMMENTS**
6. **CONSENT ITEMS**
 - A*) [SCBA 16-01](#)
Windy City Linen
1150 Willis Avenue
Appearance Approval of a Wall Sign
7. **ITEMS FOR REVIEW**
 - A) [Docket No. 2015-5](#) (Continued from December 17, 2015)
Wheeling Town Center Development
351 W. Dundee Road
Special Use-Site Plan Approval of a Preliminary Retail & Residential Planned
Unit Development
8. **APPROVAL OF MINUTES** – [December 10, 2015](#) (includes Findings for Docket No. 2015-11AB) and [December 17, 2015](#) (includes Findings for Docket No. 2015-5)
9. **OTHER BUSINESS**
10. **ADJOURNMENT**

IF YOU WOULD LIKE TO ATTEND A VILLAGE MEETING BUT REQUIRE AUXILIARY AID SUCH AS A SIGN LANGUAGE INTERPRETER, PLEASE CALL (847) 459-2600 AT LEAST 72 HOURS PRIOR TO THE MEETING.

**Public Hearing Information
Wheeling Plan Commission Meeting
January 14, 2016
(Attachment to Agenda)**

Docket No. 2015-5 WTC LLC, contract owner, is seeking the following for the property known as the Wheeling Town Center Development: Special Use-Site Plan Approval of a Preliminary Planned Unit Development for Retail and Multi-Family Residential Uses in the MXT Transit Oriented Mixed Use District, as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections. The subject property consists of: the vacant parcel at 351 W. Dundee Road (former Wicke's Furniture), the commuter parking for the Wheeling Metra Station, and the existing right-of-way of Northgate Parkway, all of which is zoned MXT Transit Oriented Mixed Use District and is comprised of a total of 16.25 acres.

REQUEST FOR PLAN COMMISSION ACTION
STAFF PROJECT REVIEW

TO: Chairperson Ruffatto and Members of the
Wheeling Plan Commission

FROM: Andrew C. Jennings, Director of Community Development
Brooke A. Jones, Senior Planner

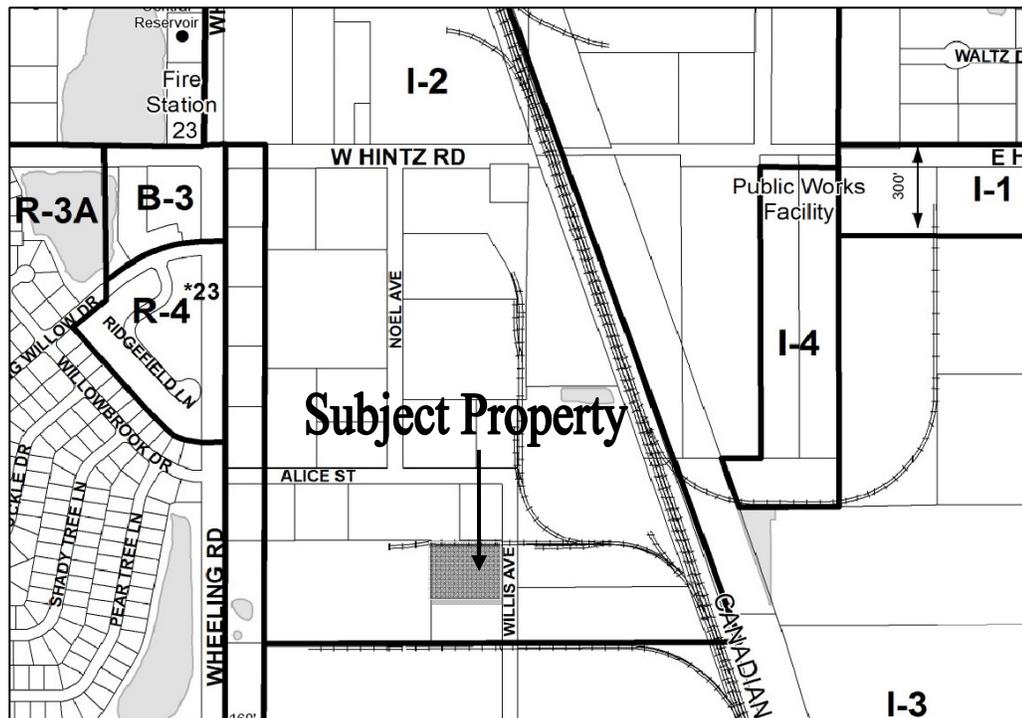
RE: **Docket No. SCBA 16-01**
Windy City Linen
1150 Willis Avenue
Appearance Approval of a Wall Sign

DATE OF REPORT: January 8, 2016

DATE OF MEETING: January 14, 2016

PROJECT OVERVIEW: The petitioner is seeking appearance approval of a wall sign at a new industrial facility.

LOCATION MAP:



GENERAL PROPERTY INFORMATION

<u>Applicant Name:</u>	Creative Edge Visual Solutions
<u>Property Owner:</u>	Rob Spiro, Windy City Linen
<u>Common Property Address:</u>	1150 Willis Avenue

Common Location: Located on the west side of Willis Avenue, between Alice Street and Gilman Avenue

Existing Use of Property: Warehouse

Existing Property Zoning: I-2 Limited Industrial District

Previous Zoning Action on Property:
397 Ordinance No. 1347, passed March 17, 1975, granted a side yard setback variation.

PROJECT DESCRIPTION

The petitioner is requesting appearance approval to install a business identification wall sign for a relocated business that warehouses rental linen for special events.

SIGNAGE PLAN REVIEW

Sign Location: The proposed sign will be located on the front façade, facing east towards Willis Avenue.

Sign Type and Size: The applicant is proposing to install a stud-mounted wall sign with white polycarbonate faces that states the business name. The sign will not be illuminated. The proposed sign is 35 sq. ft. The building frontage along Willis Avenue is approximately 138 feet. The proposed sign meets the size requirements of the Sign Code.

STAFF REVIEW

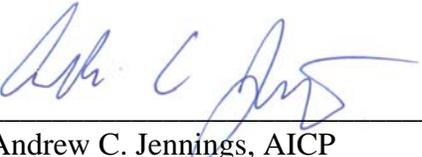
Impact on Adjacent Uses: No impact on adjacent uses is expected.

Staff Recommended Action: Staff recommends approval of the proposed wall sign.

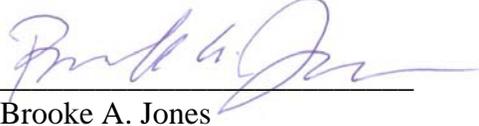
PROPOSED MOTION

If the Plan Commission finds that the petitioner has satisfied the requirements for appearance approval of the wall sign, an appropriate motion would be to:

Approve Docket No. SCBA 06-01 to permit installation of the wall sign in accordance with the sign drawing submitted January 8, 2016 by Creative Edge Visual Solutions, on behalf of Windy City Linens, located at 1150 Willis Avenue, Wheeling, Illinois;



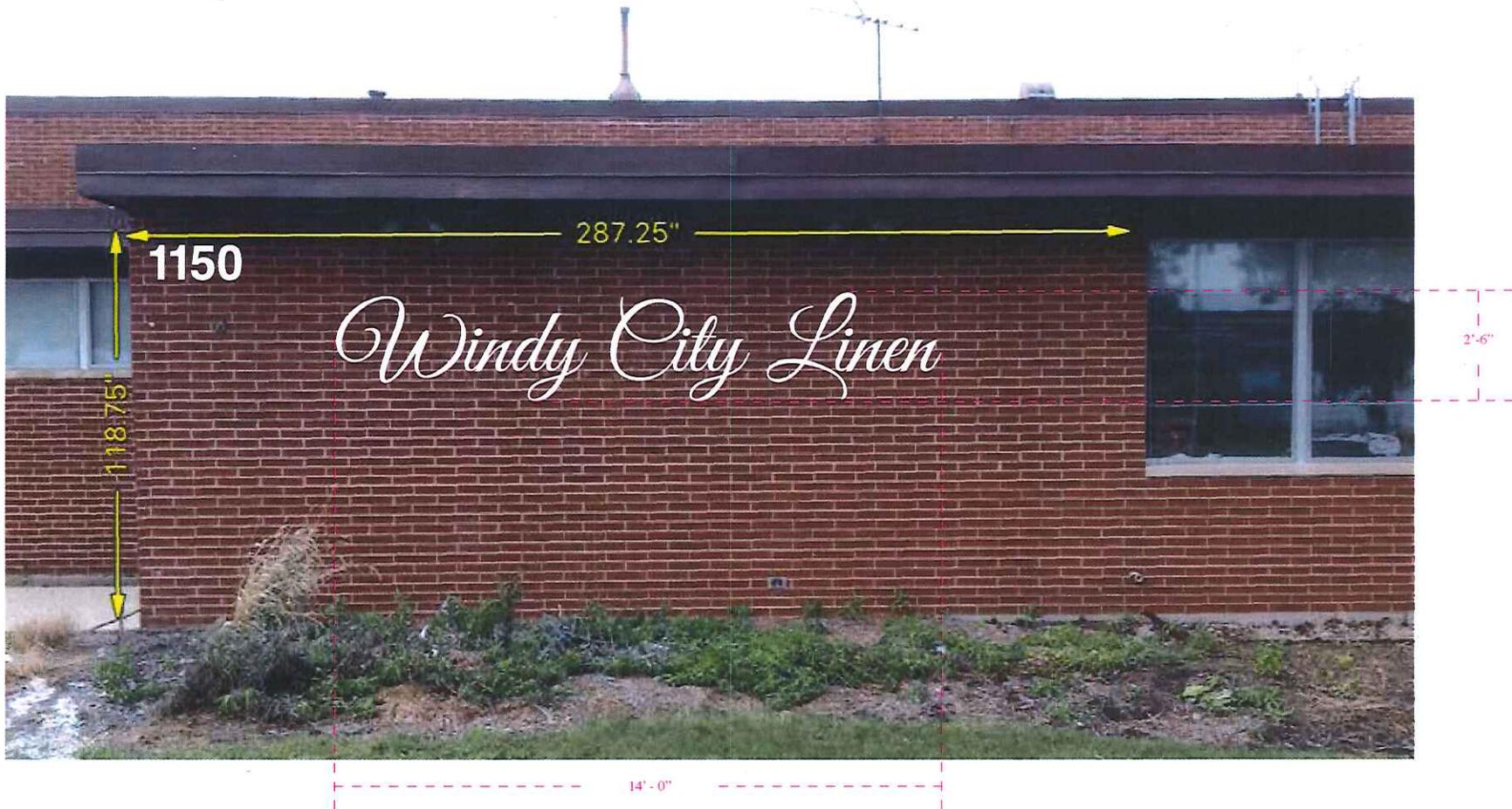
Andrew C. Jennings, AICP
Director of Community Development



Brooke A. Jones
Senior Planner

Attachment: [Wall sign plan](#)

STUD MOUNTED
W/ POLYCARBONATE



Art Not to Scale

Exhibit received Jan. 8, 2016



1414 Armour Blvd. • Suite A • Mundelein, IL 60060
p: (847) 962-5151 • f: (847) 850-5371
andrew@CreativeEdgeChicago.com
WWW.CREATIVEEDGECHICAGO.COM

Client Name: **Windy City Linen**
Location: **Wheeling, IL**
Proof Date: **01/05/16** Version: **v3**
Requested By: **Robert Spiro**
Client Approval: _____
Date of Approval: _____

PLEASE NOTE:

Prices **DO NOT** include tax freight or installation where applicable unless noted. Standard production time is 7-10 business days from a signed approval.

You are the last person to approve your project. It is your responsibility to proof your project carefully. Creative Edge Visual Solutions, Inc. is not responsible for errors that are overlooked during the proofing process.

REQUEST FOR PLAN COMMISSION ACTION
STAFF PROJECT REVIEW

TO: Chairperson Ruffatto and Members of the
Wheeling Plan Commission

FROM: Andrew C. Jennings, Director of Community Development
Brooke A. Jones, Senior Planner

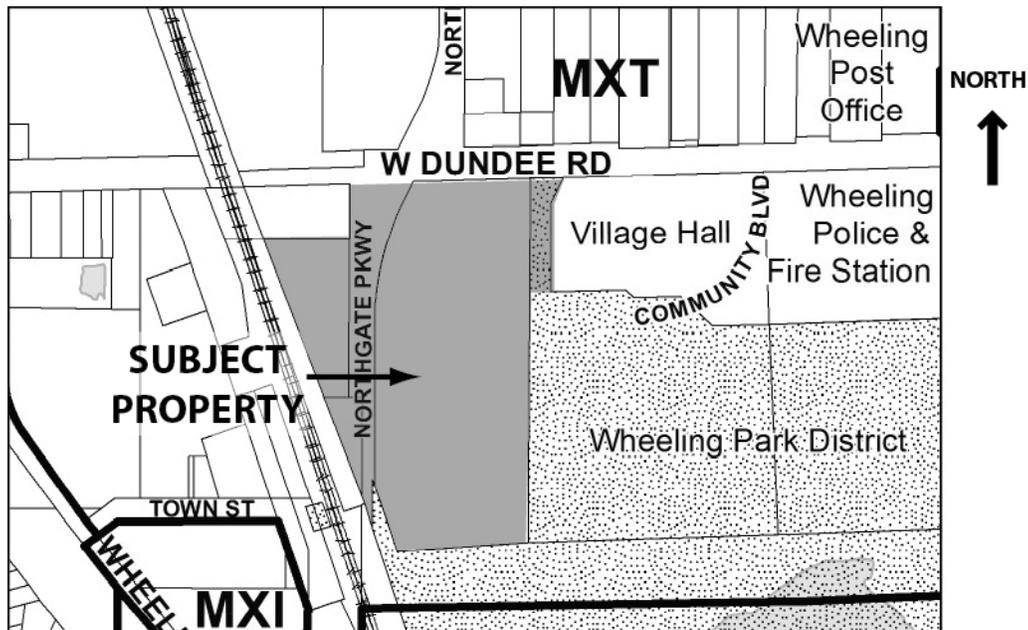
RE: **Docket No. 2015-5**
Wheeling Town Center Development
351 W. Dundee Road
**Special Use-Site Plan Approval of a Preliminary Retail &
Residential Planned Unit Development**

DATE OF REPORT: January 8, 2016

DATE OF MEETING: January 14, 2016

PROJECT OVERVIEW: The petitioner is requesting Preliminary Planned Unit Development approval to facilitate the construction of the Wheeling Town Center Development, which consists of the vacant parcel at 351 W. Dundee Road (former Wicke's Furniture), the commuter parking for the Wheeling Metra Station, and the existing right-of-way of Northgate Parkway, all of which is zoned MXT Transit Oriented Mixed Use District and is comprised of a total of 16.25 acres.

LOCATION MAP:



GENERAL PROPERTY INFORMATION

<u>Applicant Name:</u>	Brad Friedman, WTC LLC
<u>Property Owner Name:</u>	Village of Wheeling
<u>Common Property Address:</u>	351 W. Dundee Road
<u>Common Location:</u>	The vacant parcel at 351 W. Dundee Road (former Wicke's Furniture), the commuter parking for the Wheeling Metra Station, and the existing right-of-way of Northgate Parkway
<u>Neighboring Property Land Use(s):</u>	North: vacant across Dundee Road & commercial adjacent to Metra lot (Burger King) West: Transportation (Railroad) South: Open Space (Heritage Park) East: Public (Village Hall and Recreation Center)
<u>Comprehensive Plan Designation:</u>	Transit-oriented mixed use
<u>Property size:</u>	Approximately 16.25 acres
<u>Existing Use of Property:</u>	Vacant; Metra parking; Northgate Parkway R-O-W
<u>Proposed Use of Property:</u>	Mixed-Use: apartments (295 units) and retail (100,000 sq. ft.).
<u>Existing Property Zoning:</u>	MXT – Transit-Oriented Mixed Use District
<u>Previous Zoning Action on Property:</u>	None.

PLAN COMMISSION REVIEW TO DATE

The Plan Commission has discussed the Preliminary Planned Unit Development submittal for the Wheeling Town Center development at meetings on November 12, November 19, and December 17. The summary of the general direction at the conclusion of each meeting is as follows:

November 12th:

The meeting concluded with a consensus to re-convene on November 19th to complete the initial review.

November 19th:

The meeting concluded with a specific request from Plan Commission for a revised submittal. The request included the following items:

1. Provide feedback from IDOT;
2. Resolve Burger King access;
3. Increase amount of landscaping;
4. Consider additional pedestrian access to the campus;

5. Consider additional access to the anchor buildings, Flix and residential;
6. Provide greater detail especially for lighting, materials, overnight parking, outside agency agreements;
7. Turning radius, etc.;
8. Review of the sign;
9. Make sure there is no conflict near retail E that would affect the placement of the building;
10. Define ground level patios at residential building;
11. Elevations of Flix.

December 17th:

The meeting concluded with a specific request from Plan Commission for a revised submittal. The request included the following items:

1. Specify all building materials and colors;
2. Address Burger King's concerns;
3. Explore options for a parking garage;
4. Consider reconfiguration of the triangular parking area to create a larger landscaped island;
5. Provide a landscape irrigation plan at final PUD;
6. Consider options for an additional east/west connection to create a circular traffic flow;
7. Consider options for an additional access to/from the residential building separate from the retail traffic;
8. Explore options for monument signs;
9. Resolve dedicated right turn requirement from northbound Northgate as noted by IDOT;
10. Clarify phasing plans with existing conditions.
11. Resolve all utility conflicts.

STAFF REVIEW OF SUBMITTAL FOR CONTINUED HEARING

The hearing was continued to January 14th in order for the petitioner to provide responses to several comments and requests made during the meetings in November and December. The petitioner has submitted a revision to the Project Description narrative to summarize the response on each topic (**see attached**). Staff has reviewed the submittal, and informed the development team that the package as submitted does not provide adequate analysis to support several of the statements made in the updated Project Description. During the presentation on January 14th, the developer may provide additional information to describe the analysis that led to the conclusions in the submittal.

The staff review for each item from the December 17th meeting is as follows.

- 1. Specify building materials and colors.** This appears to have been addressed to the extent required at this stage. There may be a staff concern from the Fire Department with respect to the barn wood on the Flix building. It should also be noted that Staff has

confirmed that the parapet on the residential building is extended to screen the parking deck from the ground level view, and the label “book” is a typo.

2. **Burger King.** The supporting documentation for this item appears to be deficient. It is not clear which aspects of the plan have been agreed to. Burger King previously objected to parking spots located east of the Burger King property, and would need to approve an easement to allow these vehicles to exit through the south drive.
3. **Parking garage.** The supporting documentation for this item appears to be deficient. Only one option is shown - a multi-story garage at the northeast quadrant of the site. It is not clear whether other locations or configurations were considered.
4. **Consider reconfiguration of island east of residential building.** The petitioner submitted several draft plans, but determined through discussions with Staff that the emergency access could not be accommodated with the revised layout.
5. **Provide landscape irrigation plan at Final PUD.** Direction for future submittal.
6. **Explore east-west connection to create circular traffic flow.** The supporting documentation for this item appears to be deficient. The update letter states that vehicular traffic is not desirable in this area. However, there is no analysis to demonstrate that such access would negatively impact the development.
7. **Consider dedicated access for residential building.** See Item 6 above.
8. **Explore options for monument signs.** The sign location plan has been refined. See Sheet A-012.
9. **Resolve dedicated right-turn exiting development on Northgate.** The petitioner has included the lane on the plans, and intends to remove the lane if allowed by IDOT.
10. **Clarify phasing with existing conditions.** Revised drawings have been submitted, but Staff believes that the proposal remains confusing. The transition from existing, to intermediate, to final condition would be more clearly illustrated without showing the final building footprints on the intermediate steps.
11. **Resolve utility conflicts.** The supporting documentation for this item appears to be deficient. The proposed locations of the residential building and Retail E are not possible without utility easements from the Park District. It is not clear whether the Park District is in agreement with the easement.

Additional Staff Comments – Submittal for January 14th Hearing

The petitioner has provided a more detailed plan for the residential patios in response to a request originally made by the Commission made on November 19th. The patio sketches expose a potential concern not previously discussed. While the Commission had a concern with the north facing patios and a lack of privacy due to pedestrian traffic, the new detail shows that the east

and west facing patios have little separation from the commuter and commercial parking lots. The Commission may wish to discuss this aspect of the plan with the petitioner.

The Fire Department and Village Engineer have provided comment memos (attached) in response to the revised PUD submittal. The collective review by the Village Engineer, Fire Department, and Planning Staff, is that while the review is at a preliminary PUD stage, there are features on the plan that are likely to be revised that will affect items of concern to the Plan Commission. Due to the limited land area around the proposed buildings, accommodating larger vehicles (deliveries, fire apparatus, etc.) will translate into removing parking stalls, reducing building footprints, reducing walkways, and reducing landscaping. Similarly, accommodating utilities will require some existing utilities to be relocated on to property that is not controlled by the developer.

If the Commission determines that adequate supporting documentation has been provided at the time of the hearing on the 14th, then Staff recommends conditions of approval to memorialize the status of the plans and the impact of likely revisions. A draft condition is included in the motion below. The Commission may wish to revise the language of the condition as a result of discussing the following topics:

- Storm sewer location. This issue is not fully resolved. Current plan requires a Park District easement near the east side of the residential building to shift an existing water main away from path of storm sewer. The construction may require removal of trees on the Park District property. The Commission may wish to recommend conditional approval that requires a conceptual agreement by the Park District prior to Final PUD.
- Parking lot and drive aisle geometry. The drive aisles have been adjusted during the preliminary PUD review. The full impact of these adjustments has not been analyzed. The final PUD plans may require minor reductions in building dimensions, landscaping, parking, and sidewalks. The Plan Commission may wish to establish an acceptable range for the loss of parking and landscaping.
- Storm sewer construction. The regional storm sewer is required to be constructed in conjunction with development. It must be fully incorporated into the design and schedule.

PROPOSED MOTION

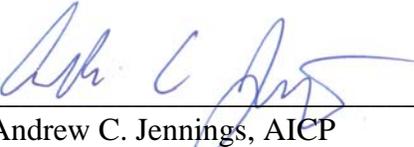
If the Plan Commission finds that the petitioner has satisfied the requirements for the granting of a Preliminary PUD Plan, an appropriate motion would be to:

Recommend approval of Docket No. 2015-5, Granting PRELIMINARY Approval of a Planned Unit Development, including Special Use-Site Plan-Building Appearance for the Wheeling Town Center Planned Unit Development, consisting of a master plan for a mixed-use transit-oriented development, as required under Chapter 19-05, Mixed-Use and Overlay Districts, Chapter 19-09 Planned Unit Developments, Chapter 19-10 Use Regulations, and

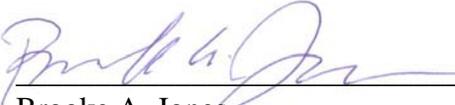
Chapter 19-12 Site Plan Approval Requirements, as shown on the plans/exhibits submitted on December 31, 2015, by WTC LLC, as shown on the Exhibit List for the Wheeling Town Center Planned Unit Development, to be located on the property consisting of approximately 16.25 acres described as the vacant parcel currently known as 351 W. Dundee Road, the commuter parking lot adjacent to the Wheeling Metra Station, and the right-of-way of Northgate Parkway, located in Wheeling, Illinois;

And with the following conditions of approval:

1. That the phasing plan shall be revised to clearly illustrate the built condition at the conclusion of each step of construction;
2. That written acknowledgment for the easements required for modifications to and cross access through the Burger King property shall be provided;
3. That written acknowledgment for the easements required for all utility relocations on the Park District property shall be provided;
4. That reductions in parking, landscaping, building square footage, and walkways are anticipated in order to demonstrate proper accommodations for larger vehicles and utilities. The Final PUD plan shall clearly document the extent of these reductions;
5. That the Final PUD submittal shall include an exhibit summarizing all of the agreements associated with the development, including but not limited to: easement for use of the Metra parcel, extension of the Station Area Development agreement, Park District roadway modification, Park District utility relocation easement, maintenance of roadways and parking areas, easements related to Burger King modifications and access, pipeline relocation agreement, and declarations associated with common property maintenance;
6. TO BE DETERMINED THROUGH DISCUSSION AT PUBLIC HEARING



Andrew C. Jennings, AICP
Director of Community Development



Brooke A. Jones
Senior Planner

- Attachments:** [Fire Department Comment Memo, dated 01.07.2016](#)
- [Engineering Division Comment Memo, dated 01.07.2016](#)
- [Preliminary PUD Updates and Narrative](#)
- [PUD Stat Sheet and Preliminary Variations](#)
- [Shared Parking Study Update](#)

[Traffic Study Update](#)

[Site Survey](#)

[Parking Deck Study](#)

[Monument Sign Visual Representation](#)

[Response to Design Standards for Planned Unit Developments](#)

[Response to Zoning Variation Standards – Landscape & Trees](#)

[Response to Zoning Variation Standards – Minimum Floor Area](#)

[Response to Zoning Variation Standards – Open Space](#)

[Response to Zoning Variation Standards – Parking Stall Size](#)

[Response to Zoning Variation Standards – Parking Count](#)

[Response to Zoning Variation Standards – Sign Location](#)

[A-000 Cover Sheet](#)

[A-010 Construction Phasing Diagrams](#)

[A-011 Construction Phasing Diagrams](#)

[A-012 Signage Location Plan](#)

[A-100 Architectural Site Plan](#)

[A-111 Residential Level 1 Floor Plan](#)

[A-112 Residential Level 2 Floor Plan](#)

[A-113 Residential Level 3-5 Floor Plan](#)

[A-116 Residential Roof Plan](#)

[A-201 Residential Exterior Elevations](#)

[A-202 Residential Exterior Elevations](#)

[A-210 Flix Exterior Elevations](#)

[A-211 Flix Exterior Elevations](#)

[C-0.1 Notes & Legends](#)

[C-1.1 Site Demolition Plan \(North\)](#)

[C-1.2 Site Demolition Plan \(South\)](#)

[C-2.1 Site Geometry Plan \(North\)](#)

[C-2.2 Site Geometry Plan \(South\)](#)

[C-3.1 Site Utility Plan \(North\)](#)

[C-3.2 Site Utility Plan \(South\)](#)

[C-4.1 Site Grading/Paving Plan \(North\)](#)

[C-4.2 Site Grading/Paving Plan \(South\)](#)

[C-5.1 Sitework Details](#)

[C-5.2 Sitework Details](#)

[CX-1.1 Fire Truck Movement](#)

[L0.0 Existing Conditions Plan](#)

[L1.1 General Landscape Plan](#)

[L1.2 Sample Landscape Plan](#)

[L2.1 Village Green Enlargement](#)

[L2.2 Northgate Parkway Section](#)

[L2.3 Plant Palette](#)

[E-100.C Site Lighting Plan](#)

[E-100.D Site Photometric Plan](#)



Memo

To: Andrew Jennings, Director of Community Development
Brooke Jones, Senior Planner
Jon Tack, Village Engineer

From: Keith S. Maclsaac *KSM*

CC: File – Wheeling Town Center PUD

Date: 1/7/2016

Re: Review Comments – Preliminary Wheeling Town Center PUD

I have reviewed the various documents regarding the above project dated December 31, 2015 and offer the following comments:

- Drawing Site Plan A-100: The Fire Department access located at the East side of the project needs to be designed with raised concrete curbs on both ends to prohibit non-emergency vehicle access between sites.

(NOTE: this is a repeat issue raised in previous plan reviews.)

- Drawing Site Plan A-100: The wide path running from East to West between the cinema and the residential midrise has already been discussed as a “fire lane”. It needs to be indicated as such and be designed with raised concrete curbs on both ends to prohibit non-emergency vehicle access. Furthermore, it must be designed to adequately support the weight of the Fire Department’s vehicles (i.e. 22 tons – minimum). This is particularly critical if paving bricks are utilized in the construction. Appropriate signage shall be required.

(NOTE: this is a repeat issue raised in previous plan reviews.)

- Drawing A-111, A-112, and A-113: Residential Floor Plans: These drawings do not indicate required dedicated space on the first floor for the structure’s fire pump, fire sprinkler system riser(s), fire suppression incoming water service, and fire alarm system controls. In addition, no indication is made regarding required dedicated space for the required fire equipment box systems (FEBS). Inclusion of these required dedicated spaces will alter the interior layout of the structure and may reduce the distribution of one (1) and two (2) bedroom units.
- Drawing Site Plan A-100: All areas not already shown as designated parking spaces and/or valet staging locations shall be designated as “fire lanes”. Appropriate signage shall be provided, as well as execution of a parking enforcement agreement with the Wheeling Police Department.

(NOTE: this is a repeat issue raised in previous plan reviews.)

- Drawing A-211 Flix Exterior Elevation: the Eastside of the structure indicates the application of “Barn Wood” on the exterior siding. While no specific information regarding the physical product is provided, it is assumed to be reclaimed barn siding or “weathered wood”. If this is correct, this material does not meet the fire resistant standards for vertical products applied to a fire-resistant rated structure.

- Drawing CX-1.1 Fire Truck Movement Exhibit: The documented turning radius shows potential points of conflict within the parking lot where rear wheels of the fire apparatus will “clip” or “jump” over the curbs. This problem will be further exacerbated during periods of snow where piles of snow may exist and vehicles will not be able to fully pull into the available parking spaces. These turning radiuses must be adjusted to allow adequate turning in all types of weather and without “clipping” or “jumping” of the curb. Two (2) key areas of conflict are at the Northwest corner of Retail E where the vehicle will need to go into the straight through lane in order to turn right or “jump” the curb, as well as at the parking lot island on the West end of the project where the turning radius is only eight (8) feet.

(NOTE: this is a repeat issue raised in previous plan reviews.)

- Drawings C-3.1 and C-3.2 Site Utility Plan: There are a number of issues with the water main layout and fire hydrant distribution plan. In general, these issues are:
 - Fire hydrant spacing must not exceed three hundred (300) feet and must be placed in logical locations where they can be readily accessed by fire apparatus. An acceptable layout would involve fire hydrants situated near street corners where they could be accessed from two (2) or more directions with additional fire hydrants intermediately spaced along the street(s).
 - This project presents a unique feature known as the “Village Green”. In this particular layout, additional fire hydrants must be situated at both the North and South ends of the feature to permit adequate fire suppression efforts from the interior courtyard.
 - Fire hydrants are required along “fire lanes”. The wide path running from East to West between the cinema and the residential midrise has already been discussed as a “fire lane”. Therefore, both fire hydrants and a looped water main down the length of the “fire lane” are required.
 - Fire hydrants need to be situated within fifty (50) feet of all fire department connections. The practice of installing fire hydrants on the supply mains leading to the fire sprinkler systems is unacceptable since it actually depletes the water supply already going to the fire sprinkler system. Fire hydrants must be placed only on primary water mains.
 - The domestic water service and the fire sprinkler service to each building are required to be two (2) separate service lines terminating at the primary water main. They may run in parallel with each other but must remain completely separate (i.e. their own piping and sectional valves, etc.).
 - In order to maintain minimal pressure loss (i.e. friction loss) in water mains, as well as maintain laminar flow characteristics, a 90 degree bend in the water main system must be avoided whenever possible. The 90 degree bend located at the Southeast corner of the project site must be replaced with a series of less degree elbows and intermediate spool pieces of piping (ex. 2 – 45 degree elbows, etc.). The 90 degree bend located on “Main Street” between Retail D and Retail H.1 must be replaced with a series of less degree elbows and intermediate spool pieces of piping (ex. 2 – 45 degree elbows, etc.).
 - Sectional valves must be installed on the water main system to limited potential water main breaks and system isolations to no more than two (2) fire hydrants and one (1) structure at a time, while limiting the total number of sectional valves that must be closed. There are a number of circumstances where this engineering requirement was not met.

(NOTE: these are repeat issues raised in previous plan reviews.)

- Drawing C-3.1, C-3.2 General Landscape Plan, and L1.1 Site Plan A-100: A minimum of forty-eight (48) inches (four (4) feet) must be maintained around all fire hydrants. This open space requirement applies to both landscaping (i.e. trees, shrubs, and bushes), as well as street light stanchions. Due to the limited open space/green space associated with this project, it appears that numerous conflicts regarding landscaping/street light stanchions and fire hydrants exist. These will likely increase as the additional required fire hydrants are added to the plans. In order to resolve this matter, close coordination between the project engineers and landscape architects will be necessary.

(NOTE: this is a repeat issue raised in previous plan reviews.)

- Drawing L1.1 General Landscape Plan: The wide path running from East to West between the cinema and the residential midrise has already been discussed as a “fire lane”. The revised landscape plan shows the inclusion of trees along the Northside of the path. Depending upon the type of trees selected, this will create an obstruction within the fire lane. As previously discussed, only miniature or low growing landscaping may be selected for this area.
- General Comments:
 - All exterior natural gas meters shall be protected with a rigid, physical barrier in order to prevent damage from vehicle impacts.
 - Due to the limited amount of open space/green space associated with this project, a defined snow removal plan is necessary. Without a carefully defined plan, the Fire Department is concerned that snow piles could result in blocked and/or restricted access to portions of the site, as well as potentially buried fire hydrants throughout the project site. Due to the mixed use of the site development, plowing and hauling of snow off site would be very difficult and most likely not practical.
 - It is assumed that all buildings will fully comply with the Village’s Building and Fire Codes including but not limited to; being fully equipped with fire sprinkler systems, fire alarm systems, and fire equipment box systems (FEBS) within the residential structure. It is further assumed that the commercial buildings and the residential midrise building will be Type II construction. Should the Developer pursue an alternate form of construction with a less fire resistant rating, extensive site plan modifications will be required in order to add more dedicated fire lanes, increased fire flow capabilities will need to be designed into the water main system, and building separation distances must be increased. These changes would adversely impact the overall design, layout, available building square footage, and available parking space associated with this project.

(NOTE: these are repeat issues raised in previous plan reviews.)

These comments are based upon a review the current information provided and is subject to further modifications as the project enters its permit phase. If you should have any questions, please feel free to contact me at ext. 2665.



MEMORANDUM

TO: Brooke Jones, Senior Planner

FROM: Jon M. Tack, Village Engineer

COPY: Andrew Jennings, Director CD

DATE: January 7, 2016

SUBJECT: Wheeling Town Center
Preliminary PUD Submittal Review

The Engineering Division received a Project Description and Preliminary Plans for the subject project on 12-31-15.

The Engineering Division has completed a review of the above referenced submittal and offers the following comments at this time:

1. In general the engineering plans require more detail and an overall cleanup of what's been presented all of which should be able to be addressed in the final engineering phase. Many of the significant concerns were stated in the engineering memorandum of November 5th.
2. It should be noted that there are several agency approvals that will be required for this project to begin construction and necessary prior to the village issuing a permit.
3. Sheet CX 1.1, Fire Truck Movement will need to include the entrance and exit from the Burger King Lot.
4. The narrative "Updates from Workshop Meeting on December 10, 2015", under Challenges, second paragraph states "for the installation of an elliptical pipe that the Village intends to install". As a point of clarification the elliptical pipe will be constructed under the construction contracts for this development.
5. As stated in the narrative an easement is required from the Wheeling Park District for the relocation of the water main and the installation of the elliptical pipe along the east property line. Should the easement not be granted, significant plan revisions would be necessary to complete the installation of the elliptical pipe as proposed. As it appears

on the engineering plans the easement starting at the SE property corner and running north would require an additional width of 9' for a total width of 15' and approximately 235' long easement on the park district property. The construction of the elliptical pipe along with the other proposed site improvements will require that approximately 1,100' of water main along the east property line be relocated further east to meet code requirements.

Wheeling Town Center
Updates and Project Description

Updates from Workshop Meeting on December 10, 2015:

1. Specify all building materials and colors

We have updated the Residential Building elevations and will continue to provide samples for review.

2. Address Burger King concerns

We have adjusted the stripping and the curb cuts in order to accommodate BK concerns. We have also provided super-imposed visuals of the monument sign to show that it no longer blocks their visibility.

3. Explore options for parking garage

We have modeled a parking garage in the north east corner of the parking field as requested. It obstructs the visibility of retail building E. It is not a viable option.

4. Consider reconfiguration of triangular parking area to create larger landscaped island

We considered the sketch from Commissioner Zangara. However, it was requested that we leave the parking field as originally designed in order to provide an access lane for the Fire Dept.

5. Provide a landscape irrigation plan

This will be provided at FINAL PUD.

6. Consider options for an additional east-west connection to create a circular traffic flow

We have considered this option many times. However, it is more desirous to maintain the existing pathway versus adding another drive aisle that would impede upon the pedestrian friendly Plaza and Residential living experience. Additionally, the parking study shows that there is not a need for an additional east-west connection.

7. Consider Options for additional access to/from the residential building

We have had several meetings with the Park District in an effort to obtain curb cuts and easements that would provide additional access for the residential tenants. Unfortunately, the Park District is not interested at this time. They are very protective of their private land, and also the safety of the pre-school drop-off area.

8. Explore options for monument signs

The monument sign has been shifted to the south in order to accommodate concerns from BK. We have also provided super-imposed visuals.

9. Resolve dedicated right-turn lane requirement (from NB Northgate) noted by IDOT

We have provided an alternative site plan with the dedicated right-turn lane to satisfy IDOT. However, we will continue to have discussions with IDOT to eliminate this requirement as we feel it is not necessary based on the two additional access points for vehicles wishing to travel east on Dundee Rd.

10. Clarify phasing plans

We have updated the phasing plan with additional detail.

11. Resolve all utility conflicts

We have met with the Village engineer and have made adjustments to the utility plan. We have eliminated a portion of retail building E in order to accommodate the proposed elliptical pipe. We are still waiting on confirmation from the Park District with regards to an easement to accommodate the relocated utilities.

Updates from PUD Preliminary Hearings on November 12 & 19, 2015:

1. Redesign Northgate Pkwy curb cuts and BK access

The Northgate Parkway driveway curb cuts and Burger King accesses have been redesigned in keeping with the discussion had with Village staff and Burger King representatives. Driveway locations have been aligned to better accommodate turning vehicles without impeding through traffic.

2. IDOT Preliminary Approval

IDOT has provided their comments and they have been incorporated into the new site plan. We are discussing the need for a northbound right-turn lane exiting the Town Center given the low volume and the other opportunities to turn right onto Dundee Road. Both options (with and without the right turn lane) have been prepared.

3. Provide sight lines of proposed signage in relation to BK

The monument sign has been shifted to the south in order to accommodate concerns from BK. We have also provided super-imposed visuals.

4. Add pedestrian cross walks in NE parking field

Cross walks from the NE parking field have been added for access to the sidewalk along the North side of Building E providing additional connectivity to the Plaza and the Municipal Campus.

5. Define overnight parking areas

Overnight parking will be designated as necessary based on feedback from the operations group.

6. Increase landscaping and show detail of Plaza

The site landscaping has been increased 26% throughout the entire Town Center. Details of the Plaza have been provided showing the different activity areas including the fountain, entry features, seating, pedestrian walkways and café areas. Sample photos of each are included.

7. Provide lighting plan and photometric study

Lighting plan and photometric plans have been provided.

8. Review dual access into the Residential Parking Garage.

We feel that the single access to the Residential Building on the East side is sufficient per our traffic study and does not warrant a secondary entrance. We have included an additional right turn lane between Building H.1 and E to allow for traffic exiting the Southeast parking area and the option of traveling Eastbound on Community Boulevard.

9. Flix elevations and materials

We have updated the Flix elevations and have provided samples.

10. Provide detail of Res Bldg materials and colors (include samples)

We have updated the Residential Building elevations and have provided samples for review.

11. Show detail of residential ground floor patio space and define materials

A detail of the ground floor patio is shown on the Sample Landscape Plan in the updated PUD Submittal Package. The patio material is concrete paving while the fence is an ornamental metal fence, black in color. The plant materials include ornamental grasses, flowering shrubs, evergreen shrubs and perennials.

Project Description

Summary

Ever since its inception, the master plan concept for the Wheeling Town Center was intended to create and develop a walkable downtown central square that would provide a sense of community and serve as a public destination, celebrating civic life. The plan includes a 5-Story residential apartment building with approximately 295 luxury rental units, a 6-story parking garage, and a large courtyard that will offer the residents a wide range of resort-like amenities. The site will also incorporate around 100,000 SF of retail space that will revolve around a pedestrian friendly Plaza.

Flix Brewhouse has committed to open an 8-screen movie theater and anchor the Wheeling Town Center. They will operate a 38,000 SF free-standing building. Flix provides food and beverage service and brews its own beer on site. Flix is a unique first-run cinema that is distinguished from the traditional theaters.

The focal point of the Town Center will integrate a pedestrian friendly Plaza surrounded by multiple commercial spaces and residential living. Interested restaurant tenants have already expressed the desire to provide customers with a remarkable outdoor dining experience that plays off the vibe and energy created by this public realm. Interactive water fountains, ornamental pots, planters, trees, built-in benches, natural stone seating, pergolas, and sculptures will all contribute to the symbiotic energy. It will also serve as an ideal setting to host public events.

The Village of Wheeling is partnering with the development team in order to help ensure that the project achieves a legacy status that will have a lasting effect on the community. The Village is providing Tax Incremental Financing to cover most of the infrastructure costs and public improvements. The support from the Village will allow the Wheeling Town Center to achieve

greater levels of construction and design elements with a dynamic central Plaza that will draw people in.

The subject property is ideally situated adjacent to the existing Metra Station, making this project a true Transit Oriented Development that is committed to creating a pedestrian friendly Town Center atmosphere. Urban Land Institute has ranked TODs a best bet for investors 5 years in a row, and they estimate that ¼ of all households are likely to live near transit / high-density housing by the year 2030.

The site is also surrounded by the Village Hall and several Park District facilities including the Wheeling Aquatic Center / Water Park, the Community Recreation Center, and Heritage Park which is in the process of being renovated with new baseball and soccer fields to host all of the community sporting events. The site will naturally become a family friendly gathering place with several entertainment and dining establishments to serve the public.

Retail Plan

The site plan has 98,183 Sq Ft of ground floor retail space divided into 10 retail pads which are identified on the site plan and building stat sheet. The retail portion of the project will be anchored by Flix Brewhouse, a unique cinema complex featuring a food and beverage service along with hand crafted beer that is brewed on the premises. Local restaurateurs and national eateries have expressed great interest and support for the Wheeling Town Center, and are eager to participate and be a part of its success. Letters of intent for various retail pads throughout the project were received during the planning stage. Most retail centers obtain competitive bids from interested tenants after the anchor tenant has broken ground. Flix Brewhouse will serve as a traffic generator that will draw interest from several retailers as the project becomes entitled and we break ground. The most recent LOI received from Terra Fiamma was for 5,500 SF for an Italian Restaurant with outdoor seating along the pedestrian Plaza. We also have an LOI from Starbucks for a free-standing outlot with a drive-thru.

Flix Brewhouse

Flix is a Texas based company with strong financials. Additionally, Allan Raegan, the CEO, has provided a 10 year personal guarantee on the rent. Flix is the “cool” place to watch movies and functions as a true destination, bringing in over 335,000 guest visits per year. They are a significant sales tax generator, and this location is projected to provide over 150 new jobs to the local community.

Flix will be opening 15 locations throughout the US over the next 5 years. They are committed to opening an 8-screen, 38,000 SF cinema at the Wheeling Town Center. They feel the project is exciting, offers easy access to a large trade area, has excellent population density, strong demographics, and is located in an “open film zone” which allows them to access first-run movies from the studios.

Residential Apartment Building

In addition to the energetic retail and convenient shopping, the site will feature a 5-story residential apartment building with approximately 295 luxurious rental units and a first-class amenity package that will outshine the competing suburban rental buildings. The building will feature a large courtyard with several attractions for the residents including a sleek outdoor pool and patio area with grill stations, lounge chairs, cabanas, and fire features. The outdoor area will also offer residents several leisurely activities to choose from including bocce ball, corn hole, ping pong, shuffle board, and a putting green. The building will also

feature an indoor club room with state-of-the-art media services, gaming tables, and a demonstration kitchen so residents can gather for sporting events and private parties. Additional amenities will include a business center, conference room, café lounge, dog walk, fitness area, golf simulator, and a yoga studio featuring Fitness on Demand.

The units will be built-out with luxury finishes including granite counters, dimmable light pendants over floating kitchen islands, stone backsplash, brushed fixtures, and slick window shades. Each unit will have a stacked washer-dryer, and a self-contained Magic-Pak HVAC system for ultimate end-user control and comfort.

Parking

The residential building will wrap around a 6-story parking deck, eliminating any unsightly views of a cold concrete garage. The parking deck will have a total of 582 parking spaces. 483 spaces will be reserved for the residential tenants, and the remaining 99 spaces will be for resident guests, retail employees. Use of the parking garage will be controlled with a gate system to limit its use to residents and guests and the designated employees.

The retail portion of the project will be serviced by 719 open surface parking spaces plus the 99 parking spaces for employees in the residential parking garage. We are required to reserve 150 parking spaces for Metra Monday through Friday until noon. However, the current demand for the commuter parking on the east side of the tracks is only 103. Flix Brewhouse and Metra have both agreed to enter into a reciprocal parking agreement based on the inverse demand for each use.

A comprehensive Parking Study has been completed and has been provided for your review.

Challenges

An underground utility pipe was identified in the fall of 2014 which interferes with the site plan. The estimated cost to relocate the pipe is \$1.5 million dollars. The Developer and the Village have agreed to split the cost. However, West Shore Pipe Line ultimately controls the process. Construction for the Wheeling Town Center cannot commence until the pipe has been relocated.

An easement is needed from the Park District in order to allow for the installation of an elliptical pipe that the Village intends to install to service future developments on the north side of Dundee Rd.

The Development team has had several meetings and conversations with the Park District in order to obtain cross access to some of the road ways on their private property. Unfortunately, the Park District has no interest in granting cross access at this time.

For further information, please contact:

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WHEELING TOWN CENTER PRELIMINARY PUD STATISTICAL SHEET AND LIST OF VARIATIONS
12/31/15

Preliminary PUD Statistical Sheet:		SF	Acres	Coverage	Notes
Gross Land Area:		728,351	16.72		
Maximum amount of land covered by principal buildings and maximum amount of land covered by accessory buildings:		205,971	4.73	28%	
Maximum amount of land devoted to parking, drives and parking structures:		350,950	8.06	48%	
Minimum amount of land devoted to landscaped open space:		65,360	1.50	9%	
Maximum proposed dwelling unit density, if residential, and/or total square footage devoted to non-residential uses:		-	-	-	17.64 Units/Acre=Residential SF=Commercial 98,183
Proposed number of buildings:		-	-	-	11 Buildings
Maximum number of dwelling units per building:		-	-	-	1 Building w/295 Residential Units
Bedrooms per unit:		-	-	-	Ranges from 1 to 3
Number of motor vehicle and bicycle parking spaces provided, whether surface or in structures, and ratio per unit if residential, or thousand square feet of building area if non-residential:		-	-	-	1.64/Unit=Residential 8.33/1,000 SF=Commercial
Preliminary list of variations from Title 19 (Zoning), Title 17 (Planning, Subdivision, and Developments), and Title 21 (Signs):					
Zoned: MXT (Transit Orientated Mixed Use)		Portions of a development that are completely commercial in nature shall be considered with respect to the B-3 district regulations.			
		Portions of a development that are completely residential in nature shall be considered with respect to the R-4 district regulations with the exception of the building material requirements outlined in Section 19.05.010(L)6, below.			
Code/Zoning Section Requirement Actual Notes					
17.46.020: Open Space	25% of net site area	182,088 SF		65,360 SF	
19.04.060: R4 Multiple-Family Residential District					
19.04.060-D: Lot Requirements	Minimum lot area	20,000 SF		>20,000 SF	
	Minimum lot width	125 FT		>125 FT	
	Minimum lot depth	125 FT		>125 FT	
19.04.060-E: Minimum Floor Area for Dwelling Units	Efficiency units	675 SF		662 SF	
	One-bedroom units	675 SF		833 SF	
	Two-bedroom units	800 SF		961 SF	
	Three-bedroom units	925 SF		1,227 SF	
19.04.060-F: Setbacks and Height Restrictions - Principal Building	Minimum front and street side setback	30 FT		>30 FT	
	Minimum setback, interior side	30 FT		23 FT	
	Minimum rear setback	30 FT		12 FT	
	Maximum building height 35 feet, or no more than 3 stories	35 FT		55 FT	
		3 ST		5 ST	
	Distance between buildings:				
	One story building 20 feet	20 FT		N/A FT	
	Two-story building 30 feet	30 FT		N/A FT	
	Three-story building 40 feet	40 FT		N/A FT	
				50 FT	
19.04.060-H: Density Limited	Maximum density, other (20 units per net acre)	334 UNITS		295 UNITS	Btwn K/J and K/H.2
19.06.040: B3 General Commercial and Office District					
19.04.060-D: Lot Requirements	Maximum lot size	10 AC		<10 AC	
	Maximum lot coverage	35 %		<35 %	
	Minimum Green Space	25 %		9%	
19.04.060-E: Setbacks, Size & Height Restrictions - Principal Building	Minimum setback from any street (25 FT or Height of the building, whichever is greater)	25 FT		<25 FT	Buildings A/B/F only
	Minimum rear yard setback (25 FT or Height of the building, whichever is greater)	25 FT		>25 FT	
	Minimum setback from any residential lot line (25 FT or Height of the building, whichever is greater)	25 FT		>25 FT	
	Minimum parking setback, all sides	10 FT		<10 FT	
	Maximum building height 35 feet, or no more than 3 stories	50 FT		25 FT	
		4 ST		1 ST	
Landscape Requirements					
19.11.020	Interior Landscaping for Off-street Parking Areas: landscaped islands must be a min of 200 SF landscaped peninsulas must be a min 100 SF the min width of islands between curbs is 5 feet				We are meeting the # of trees required, but not the size of the parking islands as noted
	Screening for Off-Street Parking Areas street frontage 8' greenbelt in width 6' greenbelt along all interior lot lines				This is not the ROW area, but inside of the property line.
	Foundation Plantings: Foundation plantings to be incorporated along each building façade visible from a public right-of-way.				Many buildings are adjacent to the sidewalk and do not have room for landscape unless some changes are made.
Parking Requirements					
19.11.010 (Vehicle)	Multi-family, other up to 1 bedroom 2 or more bedrooms the min width of islands between curbs is 5 feet	1.7 /Unit 2.2 /Unit		1.3 /Unit 2.0 /Unit	Refer to December 2015 Shared Parking Study for more detailed information
	Retail				
	Accommodation and Food Service Uses				
	Entertainment and Recreation Uses				
19.11.010 (Bicycle)	Commercial (10+5% of amount of parking over 100) Residential ((1) for every (2) Dwelling Units)	36 148		36 148	
Signage Requirements					
21.06.100 (b) 2	Sign Dimensions Height Area	20 FT 100 SF		30 FT 450 SF	

December 2015

Wheeling Town Center

Shared Parking Study Update



Prepared for:

WTC Development

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INTRODUCTION

Eriksson Engineering Associates (EEA) was retained by Wheeling Town Center Development to analyze the parking needs for the Wheeling Town Center (WTC) in Wheeling, Illinois. WTC will be a mixed-use project with residential, retail, restaurants, and theater land-uses along with parking for commuters. The proposed development plan will have eleven buildings containing 295 apartments, a 972-seat cinema, and 60,183 square feet of retail and restaurant uses. Parking will be provided by a 582 space garage with 719 spaces in surface lots. A total of 1,301 spaces will serve the development. Metra commuters will have the use of parking within the development during the weekday.

This report provides an updated parking calculation reflecting changes to the site plan that has occurred during the Wheeling Plan Commission review process.

These changes included:

1. A reduction in the overall retail building square footage (-5%) due to changes to the Northgate Parkway intersection and storm water pipes.
2. A reduction in parking supply (-2%) to accommodate additional landscaping and entrance modifications.
3. A increased parking variation from the zoning requirement from 372 spaces to 386 spaces (+4%).

ZONING CODE PARKING REQUIREMENTS

The parking requirements were updated for the WTC development using the Village of Wheeling's Zoning Code (see **Table 1**). With 1,301 spaces provided, the plan provides 77% of the requirement including the Metra reserved spaces. A parking variation is required for the project.

Overall, the development requires a parking variation of 386 spaces. The largest component of the parking variation is for the 156 commuter spaces (40%) which will be occupied during the day when the spaces will not be needed for the retail/restaurant spaces. For the residential portion of the development, the proposed parking supply is 97 spaces short of the zoning requirement. The parking demand of apartments within a transit-oriented development supports a lower parking ratio of 1.64 spaces per unit. Commercial parking is projected to be short by 130 spaces. Part of this shortage will be dependent on the actual seating and staffing plans for the individual restaurants.

RESIDENTIAL PARKING REQUIREMENTS

A review of national and local parking data for apartments, mixed-use developments, and transit-oriented projects clearly indicated a lower parking ratio is warranted for the project. The proposed parking ratio for the apartments at WTC at 1.64 spaces per unit allows for 50% of the units to park two vehicles and maintain some overnight guest parking. This ratio exceeds the national and local parking data (1.23 to 1.39 spaces per unit). These ratios do not include a discount for transit or mixed use developments. Data collected by the RTA and the Village of Palatine at TOD projects support lower rates also at 1.3 to 1.41 spaces per unit. The provision of car sharing at the development will further reduce the residential parking demand.

**Table 1
Zoning Code Parking Requirements**

Use	Size	Zoning Code	Required Parking	Spaces Provided	
Apartments	20 studios	1.7 spaces per unit	234.6		
	118 1-bedroom				
	139 2-bedroom	2.2 spaces per unit	345.4		
	18 3-bedroom				
	295 units	Residential Parking Required	580	483 (83%)	
Theater	38,000 sq. ft. (972 seats/60 bar seats) (100 employees peak shift)	1 space per 3 seats plus one space per employee	444		
Retail	35,063 sq. ft.	4 spaces per 1,000 sq. ft.	140		
Bank	3,404 sq. ft. (7 employees estimated)	3 spaces per 1,000 sq. ft. plus one space per employee	17		
Coffee Shop	1,716 sq. ft. (54 seats)	1 space per 3 seats plus one space per employee	23		
Restaurants	20,000 sq. ft.	1 space per 3 seats plus one space per employee ⁽²⁾	327		
	101,451 sq. ft.	Commercial Parking Required	951		818 (86%)
Wheeling Town Center Parking Requirement			1,531		1,301(77%)
Metra East Commuter Lot Parking Agreement			156		

(1) Theater building is 40,226 sq. ft. minus truck dock (2,226 sq. ft.)
 (2) Seats based on 25 gross sq. ft. per seat and three employees per 1,000 sq. ft.

The lack of parking at any development is always a concern with the possibility overflow parking impacting adjacent properties. The proposed apartments are part of a larger development with 1,301 parking spaces of which 483 spaces are reserved for the exclusive use the renters in the parking garage.

Within the center of the apartment building, a six level 582 space parking garage is planned with 483 spaces reserved for the residents and their guests. The remaining 99 spaces will be reserved for employees or valet parking of the commercial uses within WTC on the top level. Use of the parking garage will be controlled with a gate system to limit its use to residents and guests and the designated employees or valet operators.

Table 2 shows the updated residential parking demand throughout the day. Peak demand occurs in the evening and overnight time periods when the residents are at home. Detailed calculations of the apartment guest and resident parking are included in the **Appendix**.

COMMERCIAL PARKING REQUIREMENTS

Using shared parking principals, the hourly parking demand was recalculated for the retail, bank, coffee shop with drive-thru, the theater, and quality or family/fast casual restaurants for a weekday and a weekend. The weekday and potential weekend Metra commuter parking demand was included. The parking supply of 818 spaces will be provided in the east surface lot (321spaces), the west lot (398 spaces), and one level of the residential parking garage (99 spaces). **Table 3** summaries the commercial parking needed for the development with detailed calculations in the **Appendix**.

During the weekday, the peak demand is 691 spaces at 8:00 PM or 84% of the lots' capacity. This leaves sufficient surplus parking spaces available to minimize parking lot traffic searching for an open space.

On weekends, the peak-demand occurred in the evening at 8:00 PM with 760 vehicles occupying 93% of the available spaces due to the restaurant/theater uses. Ideally, with retail/restaurant uses, up to 10% of the spaces should be available to minimize excessive parking lot traffic searching for an open space.

**Table 2
Hourly Apartment Parking Demand**

Hour	Weekday Residential Parking			Weekend Residential Parking		
	Total Vehicles	Percent of Capacity	Open Spaces	Total Vehicles	Percent of Capacity	Open Spaces
6:00 AM	413	86%	70	413	86%	70
7:00 AM	376	78%	107	381	79%	102
8:00 AM	360	75%	123	360	75%	123
9:00 AM	339	70%	144	339	70%	144
10:00 AM	319	66%	164	319	66%	164
11:00 AM	298	62%	185	298	62%	185
Noon	277	57%	206	277	57%	206
1:00 PM	298	62%	185	298	62%	185
2:00 PM	298	62%	185	298	62%	185
3:00 PM	298	62%	185	298	62%	185
4:00 PM	319	66%	164	319	66%	164
5:00 PM	369	76%	114	369	76%	114
6:00 PM	398	82%	85	398	82%	85
7:00 PM	445	92%	38	445	92%	38
8:00 PM	449	93%	34	449	93%	34
9:00 PM	453	94%	30	453	94%	30
10:00 PM	457	95%	26	457	95%	26
11:00 PM	448	93%	35	448	93%	35
Midnight	435	90%	48	435	90%	48

Note: 483 resident parking spaces provided in parking garage.

**Table 3
Hourly Commercial/Metra Parking Demand**

Hour	Weekday Parking			Weekend Parking		
	Total Vehicles	Percent of Capacity	Open Spaces	Total Vehicles	Percent of Capacity	Open Spaces
6:00 AM	104	13%	715	57	7%	762
7:00 AM	207	25%	612	118	14%	701
8:00 AM	256	31%	563	170	21%	649
9:00 AM	306	37%	513	244	30%	575
10:00 AM	376	46%	443	302	37%	517
11:00 AM	434	53%	385	350	43%	469
Noon	601	73%	218	538	66%	281
1:00 PM	673	82%	146	611	75%	208
2:00 PM	650	79%	169	613	75%	206
3:00 PM	603	74%	216	586	72%	233
4:00 PM	622	76%	197	587	72%	232
5:00 PM	655	80%	164	631	77%	188
6:00 PM	625	76%	194	667	81%	152
7:00 PM	663	81%	156	717	88%	102
8:00 PM	691	84%	128	760	93%	59
9:00 PM	637	78%	182	676	83%	143
10:00 PM	537	66%	282	643	79%	176
11:00 PM	428	52%	391	530	65%	289
Midnight	235	29%	584	243	30%	576

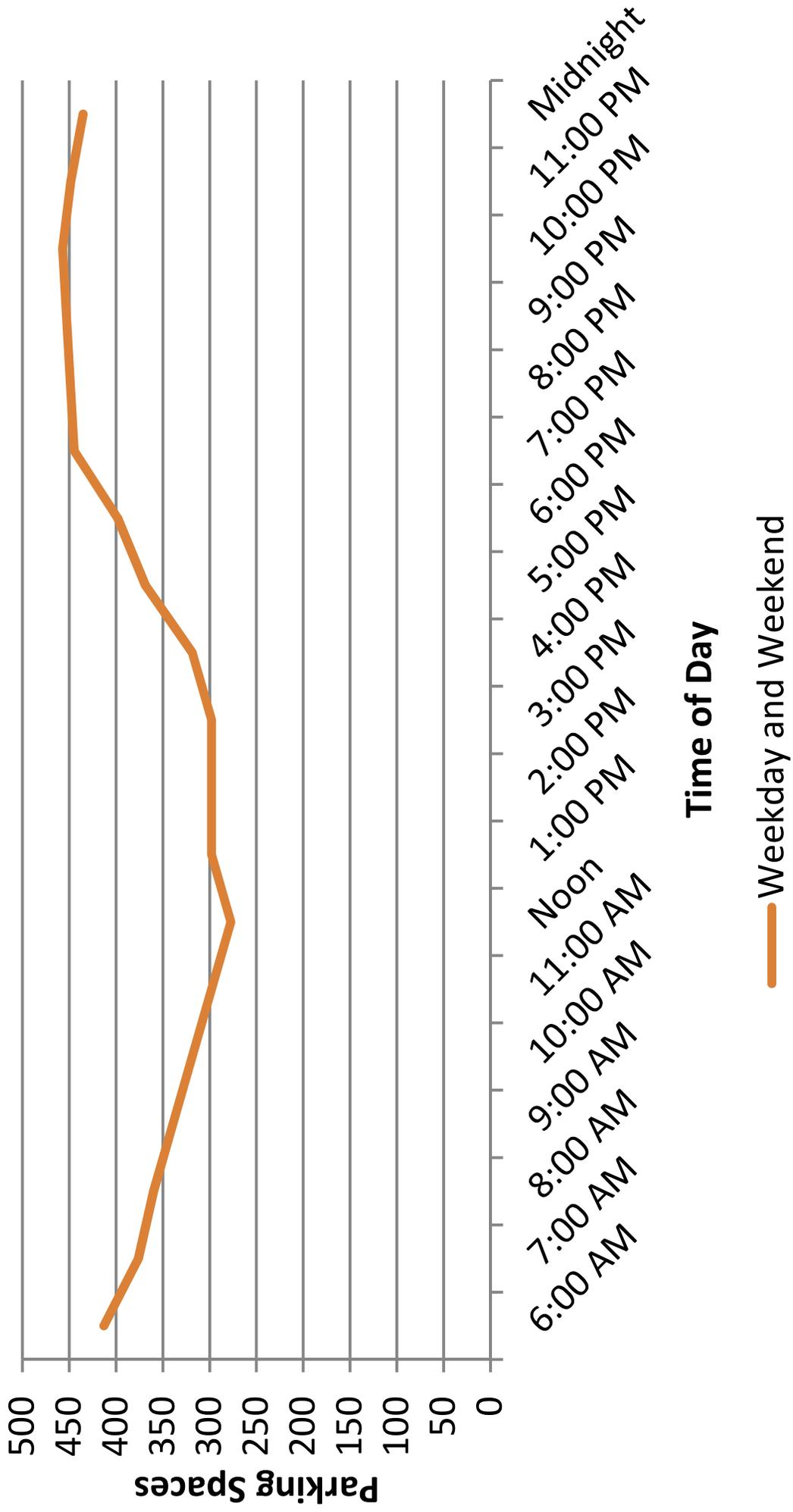
Note: 818 spaces provided for commercial and Metra uses.

RECCOMENDATIONS

Based on the shared parking analysis the following conclusions and recommendations were reached:

- The zoning code requirement of 1,531 parking spaces and 156 reserved commuter spaces exceeds the proposed supply of 1,301 spaces (778%).
- The parking analysis is based on a maximum of 20,000 square feet of restaurant uses on the site (not including the Flix). Additional restaurant space could be considered if the number of seats and parking demand is less than projected.
- Parking requirements for the apartment portion of the development should be reduced to 1.64 spaces per unit based on national and local studies along and its location in a transit oriented development.
- Parking for the apartments will be provided in 483 reserved spaces in the 582 space parking garage. Access to the parking garage will be controlled to only allow residents or their guests.
- The remaining commercial uses and Metra commuters will have 719 surface spaces and 99 spaces in the parking garage. The parking garage spaces will be reserved for employees or parking valet one of first level.
- Based on the shared parking analysis, there is sufficient parking to accommodate the commercial parking demand on a weekday and weekend without overflow.

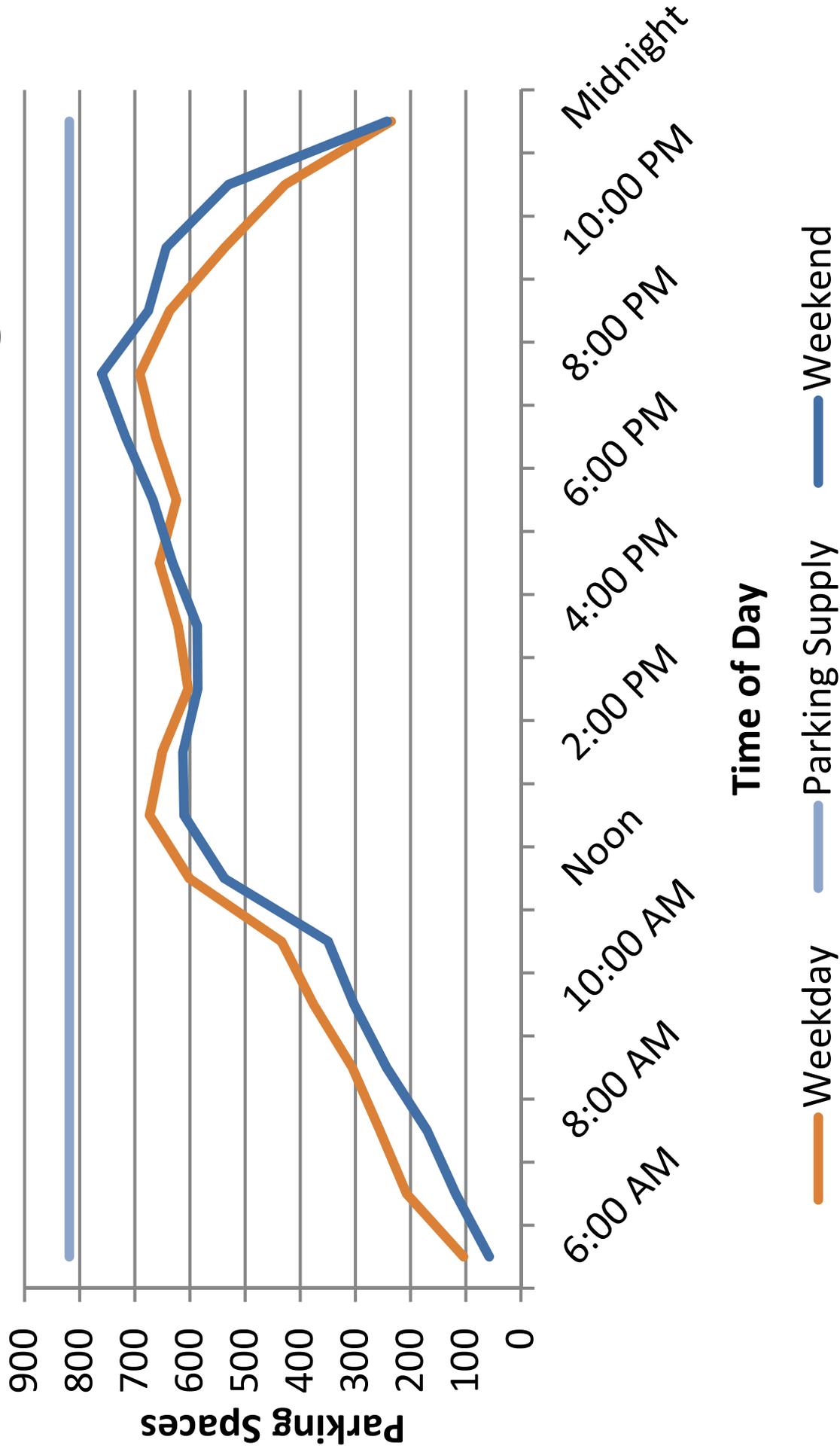
WTC Apartment Parking



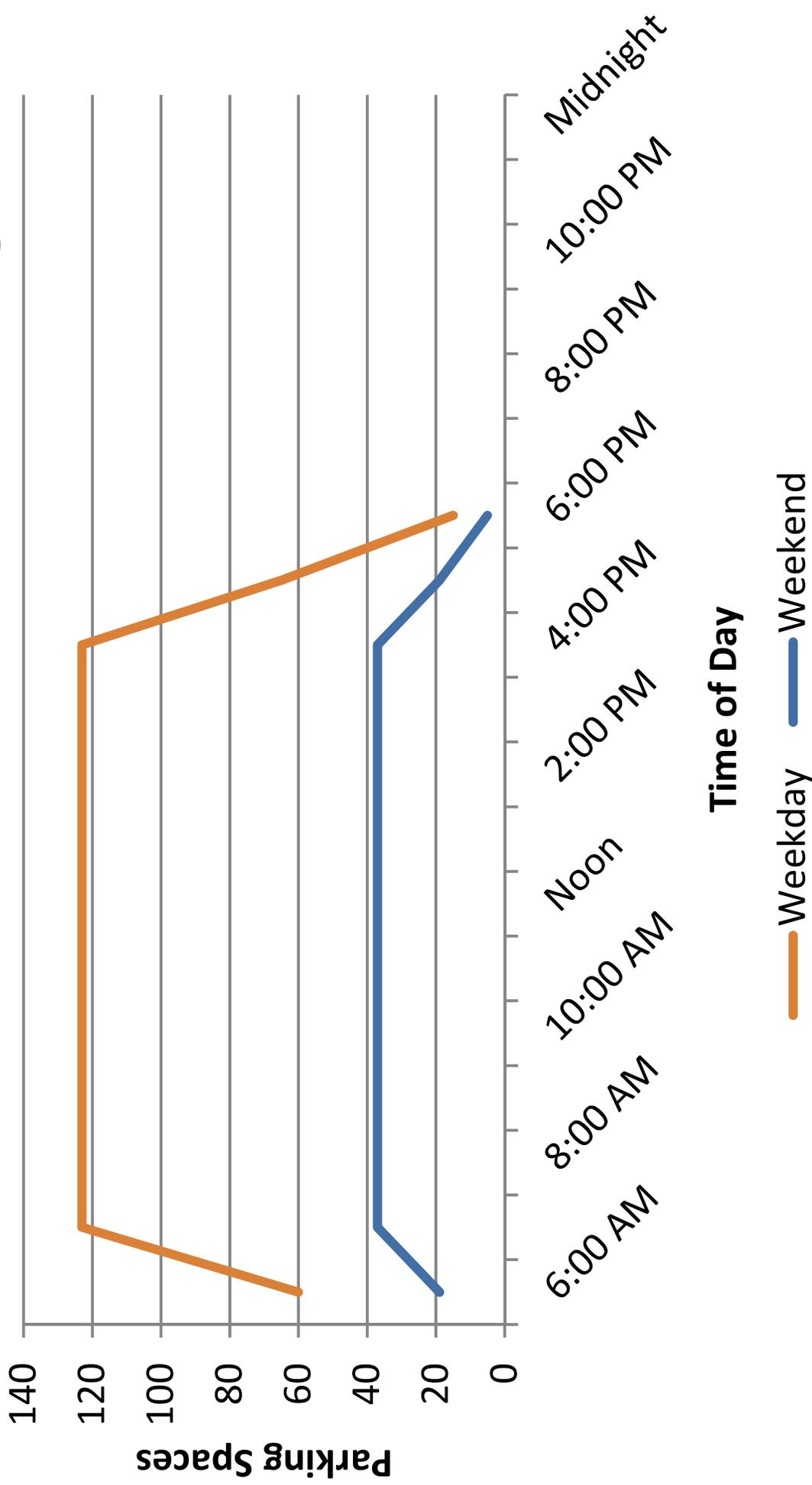
Appendix Table 1
Wheeling Town Center
Hourly Apartment Parking Calculations

Weekday	Visitor Parking		Resident Parking		Total Apt. Parking	Weekday		Open Spaces
	Size	295	Size	295		Garage Parking Provided	Percent Occupancy	
	Parking Ratio	0.15	Parking Ratio	1.4				
	Peak Demand %	44.3 veh.	Peak Demand %	413.0 veh.				
6:00 AM	0%	0.0	100%	413.0	413	483	86%	70
7:00 AM	10%	4.4	90%	371.7	376	483	78%	107
8:00 AM	20%	8.9	85%	351.1	360	483	75%	123
9:00 AM	20%	8.9	80%	330.4	339	483	70%	144
10:00 AM	20%	8.9	75%	309.8	319	483	66%	164
11:00 AM	20%	8.9	70%	289.1	298	483	62%	185
Noon	20%	8.9	65%	268.5	277	483	57%	206
1:00 PM	20%	8.9	70%	289.1	298	483	62%	185
2:00 PM	20%	8.9	70%	289.1	298	483	62%	185
3:00 PM	20%	8.9	70%	289.1	298	483	62%	185
4:00 PM	20%	8.9	75%	309.8	319	483	66%	164
5:00 PM	40%	17.7	85%	351.1	369	483	76%	114
6:00 PM	60%	26.6	90%	371.7	398	483	82%	85
7:00 PM	100%	44.3	97%	400.6	445	483	92%	38
8:00 PM	100%	44.3	98%	404.7	449	483	93%	34
9:00 PM	100%	44.3	99%	408.9	453	483	94%	30
10:00 PM	100%	44.3	100%	413.0	457	483	95%	26
11:00 PM	80%	35.4	100%	413.0	448	483	93%	35
Midnight	50%	22.1	100%	413.0	435	483	90%	48
WEEKEND								
Weekend	Visitor Parking		Resident Parking		Total Apt. Parking	Weekend		Open Spaces
	Size	295	Size	295		Garage Parking Provided	Percent Occupancy	
	Parking Ratio	0.15	Parking Ratio	1.4				
	Peak Demand %	44.3 veh.	Peak Demand %	413.0 veh.				
6:00 AM	0%	0.0	100%	413.0	413	483	86%	70
7:00 AM	20%	8.9	90%	371.7	381	483	79%	102
8:00 AM	20%	8.9	85%	351.1	360	483	75%	123
9:00 AM	20%	8.9	80%	330.4	339	483	70%	144
10:00 AM	20%	8.9	75%	309.8	319	483	66%	164
11:00 AM	20%	8.9	70%	289.1	298	483	62%	185
Noon	20%	8.9	65%	268.5	277	483	57%	206
1:00 PM	20%	8.9	70%	289.1	298	483	62%	185
2:00 PM	20%	8.9	70%	289.1	298	483	62%	185
3:00 PM	20%	8.9	70%	289.1	298	483	62%	185
4:00 PM	20%	8.9	75%	309.8	319	483	66%	164
5:00 PM	40%	17.7	85%	351.1	369	483	76%	114
6:00 PM	60%	26.6	90%	371.7	398	483	82%	85
7:00 PM	100%	44.3	97%	400.6	445	483	92%	38
8:00 PM	100%	44.3	98%	404.7	449	483	93%	34
9:00 PM	100%	44.3	99%	408.9	453	483	94%	30
10:00 PM	100%	44.3	100%	413.0	457	483	95%	26
11:00 PM	80%	35.4	100%	413.0	448	483	93%	35
Midnight	50%	22.1	100%	413.0	435	483	90%	48

WTC Commercial Parking



Metra East Lot Commuter Parking



December 2015

Wheeling Town Center

Traffic Study Update



Prepared for:

WTC Development

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This report summarizes Eriksson Engineering Associates' (EEA) traffic study update for the Wheeling Town Center in Wheeling, Illinois. The development plan has been revised based on comments received during the Plan Commission process. The plan now consists of 295 apartments (no change), a 38,000 square foot cinema (no change), and 60,183 square feet of retail, bank, and restaurant buildings (3,259 sq. ft. or 5% reduction). Existing Metra commuter parking on the site will be maintained.

The purpose of this study was to update the capacity analysis to reflect recent review comments from the Illinois Department of Transportation for the signalized intersection of Northgate Parkway and Dundee Road. IDOT required the installation of an eastbound right-turn lane, a northbound right-turn lane, and the restriping of southbound Northgate Parkway from separate right-turn and thru lanes to a separate right-turn lane and a shared thru/right-right-turn lane.

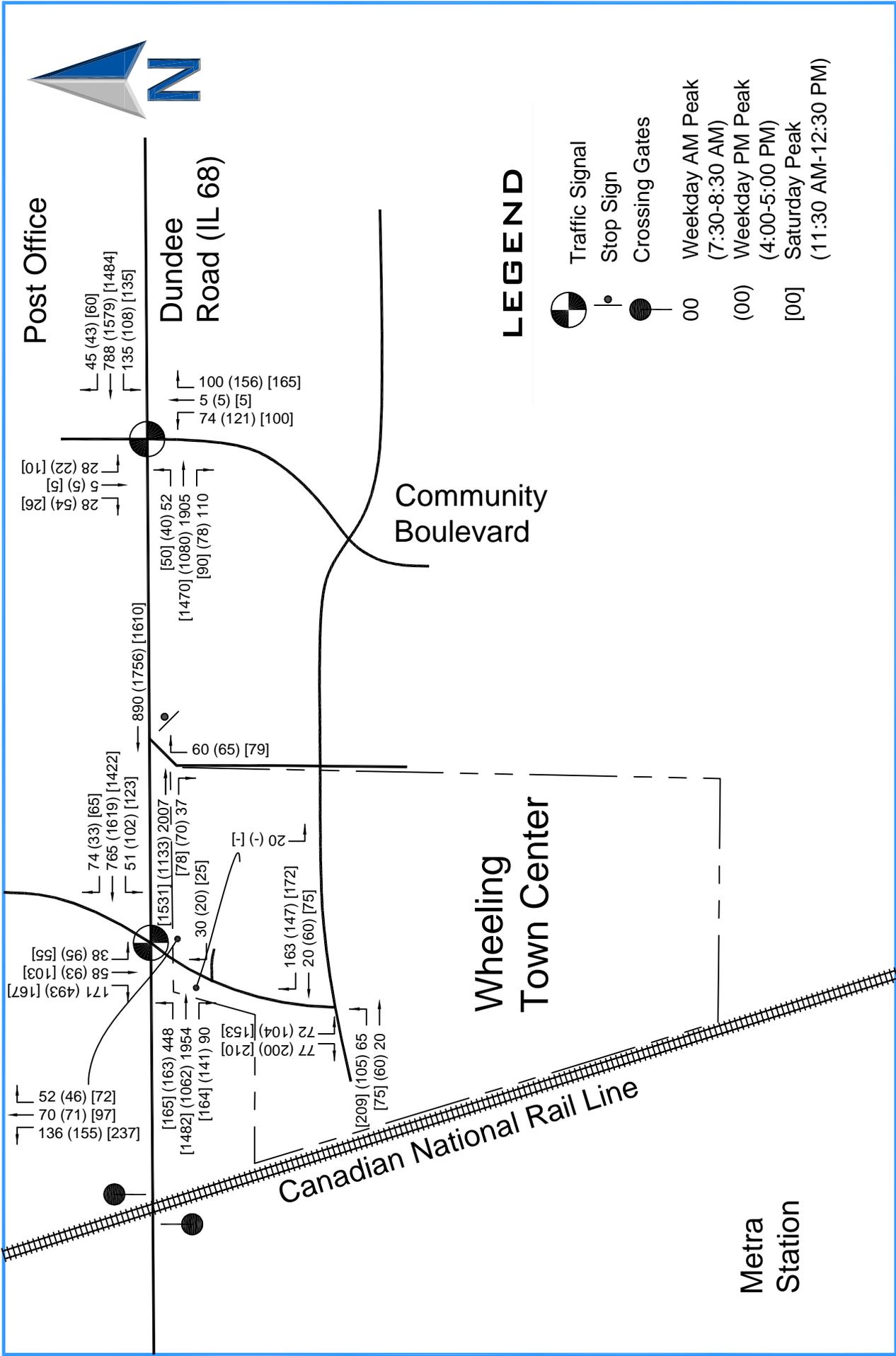
Total Traffic Volumes

The future traffic conditions are based the Year 2026 traffic volumes, which projects traffic beyond initial construction of the development. The Year 2026 total traffic volumes include the following:

- Existing peak hour traffic volumes from the 2015 counts.
- Estimated site-generated traffic for the complete build out of WTC and Northgate Crossings
- Regional growth in traffic volumes applied to the surrounding roadway system.

Regional growth represents an increase in existing traffic volumes due to ambient growth not attributable to any one particular planned nearby development. A regional growth rate of 1 percent per year was applied to the existing traffic volumes based on a previous study conducted for the Dundee Road corridor.

Recent changes to the site plan reduced the retail portion of the development which would result in a slight drop in the development related total traffic volumes. However, for this analysis the Year 2026 traffic volumes were not changed. **Figure 7** (from the October, 2015 report) is shown with the total traffic volumes for reference.



2026 Projected Traffic Volumes

Figure 7

Northgate Parkway and Dundee Road

The capacity analysis (see **Table 1**) for future conditions covers three scenarios:

1. The original geometrics proposed in the October 2015 report which includes the improvement of Northgate Boulevard's south leg to have dual left-turn lanes with a shared through/right-turn lane or three outbound lanes in total. This scenario is provided as a reference.
2. The IDOT requested geometrics to add eastbound and northbound right-turn lanes and the restriping of southbound Northgate Parkway from separate right-turn and thru lanes to a separate right-turn lane and a shared thru/right-right-turn lane.
3. The IDOT geometrics without the separate northbound right-turn lane on Northgate Parkway.

Capacity analyses were revised based on these scenarios. Copies of the capacity analysis summaries are included in the **Appendix**.

Under Scenario 2, the eastbound right-turn lane, northbound right-turn lane, and the restriping of southbound Northgate Parkway reduces vehicular delays 18-30% and improves traffic operations compared to Scenario 1. Please note that the eastbound right-turn lane requires cooperation of an adjacent property owner.

Scenario 3 removes the separate northbound right-turn lane from Scenario 2 resulting in a nominal change in intersection delay when a shared thru/right-turn lane is provided. Traffic volumes for the northbound right-turn are low with two other opportunities to turn right east of the Northgate Parkway traffic signal. Right-turn volumes range from 46 to 72 vehicles per hour or about one car per minute.

The development proposal is to provide shared northbound thru/right-turn lane subject to final review from the Illinois Department of Transportation. In the event that the northbound right-turn lane is required, the site plan can accommodate their request.

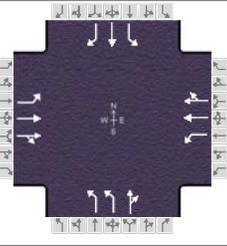
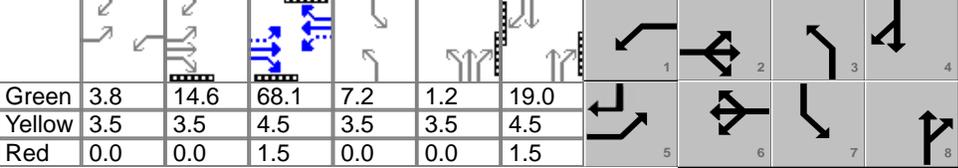
Table 1
Year 2026 Intersection Level of Service and Total Delay
Dundee Road at Northgate Parkway

Leg	Number of Lanes			AM Peak Hour			PM Peak Hour			Sat. Peak Hour		
	October	IDOT	w/o Nb Rt	October	IDOT	w/o Nb Rt	October	IDOT	w/o Nb Rt	October	IDOT	w/o Nb Rt
Eastbound	Single Left-Turn Lane			C-22.7	C-21.4	C-21.4	D-53.6	D-53.9	D-53.9	C-27.5	C-24.2	C-21.5
	Two Thru/Right	Two Thru Lanes		C-25.1	B-11.5	B-11.5	B-17.5	B-12.8	B-12.8	C-28.4	C-15.0	B-12.9
	None	Right-Turn Lane		-	A-4.7	A-4.7	-	A-7.5	A-7.5	-	A-6.0	A-6.0
Westbound	Single Left-Turn Lane			C-32.2	C-21.0	C-21.0	C-20.3	B-17.8	B-17.8	C-30.0	C-20.5	B-18.4
	Two Thru Lanes with Shared Right-Turns			B-15.9	B-15.0	B-15.0	D-40.8	D-38.0	D-38.0	C-22.3	B-19.7	B-17.0
Northbound	Dual-Left Turn Lanes			E-61.4	D-44.7	D-44.7	E-58.5	E-58.5	E-58.5	D-50.7	D-50.9	D-53.7
	Shared Thru/Rt	Thru	Shared Thru/Rt	D-51.9	D-51.0	D-53.1	D-43.2	D-43.2	D-44.2	D-39.2	D-39.7	D-43.9
	None	1	None	-	D-44.9	-	D-35.6	D-35.6	-	-	C-32.0	-
Southbound	Single Left-Turn Lane			E-66.1	E-67.6	E-67.6	E-71.0	E-71.0	E-71.0	D-54.5	D-54.5	D-54.5
	Thru Only	Shared Thru/Right-Turn		D-53.8	D-54.0	D-54.0	D-47.4	E-77.1	E-77.1	D-42.5	D-45.6	C-45.6
	Right-Turn Lane			D-37.4	-	-	F-116.1	E-78.7	E-78.7	D-36.0	D-45.2	D-45.2
Overall				C-25.8	B-18.1	C-18.3	D-44.3	D-37.2	D-37.3	C-28.4	C-22.0	C-20.6

Wheeling Town Center Traffic Appendix

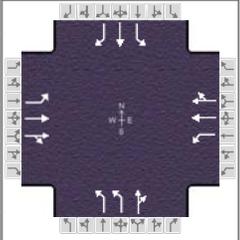
- Revised Capacity Analyses
 - Northgate Parkway

HCS 2010 Signalized Intersection Input Data

General Information						Intersection Information											
Agency		Eriksson Engineering				Duration, h		0.25									
Analyst		SBC		Analysis Date		Aug 12, 2015		Area Type		Other							
Jurisdiction		IDOT/Wheeling		Time Period		AM Peak Hour		PHF		0.91							
Urban Street		Dundee Road (IL 68)		Analysis Year		2026		Analysis Period		1 > 7:30							
Intersection		Northgate Parkway		File Name		Northgate AM Total Dual Shared Th Rt.xus											
Project Description		Future Conditions															
Demand Information				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Demand (v), veh/h				448	1954	90	51	765	74	138	70	52	38	58	171		
Signal Information																	
Cycle, s		140.0	Reference Phase		2												
Offset, s		0	Reference Point		End												
Uncoordinated		No	Simult. Gap E/W		On												
Force Mode		Fixed	Simult. Gap N/S		On												
Green				3.8	14.6	68.1	7.2	1.2	19.0								
Yellow				3.5	3.5	4.5	3.5	3.5	4.5								
Red				0.0	0.0	1.5	0.0	0.0	1.5								
Traffic Information				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Demand (v), veh/h				448	1954	90	51	765	74	138	70	52	38	58	171		
Initial Queue (Q _b), veh/h				0	0	0	0	0	0	0	0	0	0	0	0		
Base Saturation Flow Rate (s ₀), veh/h				1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900		
Parking (N _m), man/h				None			None			None			None				
Heavy Vehicles (P _{HV}), %				3	3		3	3		3	3		3	3	3		
Ped / Bike / RTOR, /h				0	0	0	0	0	0	0	0	0	0	0	0		
Buses (N _b), buses/h				0	0	0	0	0	0	0	0	0	0	0	0		
Arrival Type (AT)				3	4	3	3	4	3	3	3	3	3	3	3		
Upstream Filtering (I)				1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Lane Width (W), ft				12.0	12.0		12.0	12.0		12.0	12.0		12.0	12.0	12.0		
Turn Bay Length, ft				355	0		150	0		350	350		200	0	450		
Grade (P _g), %					0			0			0			0			
Speed Limit, mi/h				35	35	35	35	35	35	25	25	25	30	30	30		
Phase Information				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT						
Maximum Green (G _{max}) or Phase Split, s				51.8	84.0	12.0	44.2	19.0	30.0	14.0	25.0						
Yellow Change Interval (Y), s				3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5						
Red Clearance Interval (R _c), s				0.0	1.5	0.0	1.5	0.0	1.5	0.0	1.5						
Minimum Green (G _{min}), s				3	15	3	15	12	8	9	19						
Start-Up Lost Time (I _t), s				2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0						
Extension of Effective Green (e), s				3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0						
Passage (PT), s				3.0	4.0	3.0	4.0	3.0	4.0	3.0	4.0						
Recall Mode				Off	Max	Off	Off	Off	Min	Off	Off						
Dual Entry				No	Yes	No	Yes	No	Yes	No	Yes						
Walk (Walk), s				0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0						
Pedestrian Clearance Time (PC), s				0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0						
Multimodal Information				EB			WB			NB			SB				
85th % Speed / Rest in Walk / Corner Radius				35	No	50	35	No	50	30	No	50	30	No	50		
Walkway / Crosswalk Width / Length, ft				9.0	8	75	9.0	8	75	9.0	8	75	9.0	8	75		
Street Width / Island / Curb				0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes		
Width Outside / Bike Lane / Shoulder, ft				12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0		
Pedestrian Signal / Occupied Parking				Yes	0.00		Yes	0.00		Yes	0.00		Yes	0.00			

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Aug 12, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30
Intersection	Northgate Parkway	File Name	Northgate AM Total Dual Shared Th Rt.xus		
Project Description	Future Conditions				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information				Signal Timing (s)								Signal Phases			
Cycle, s	140.0	Reference Phase	2	Green	3.8	14.6	68.1	7.2	1.2	19.0	1	2	3	4	
Offset, s	0	Reference Point	End	Yellow	3.5	3.5	4.5	3.5	3.5	4.5	5	6	7	8	
Uncoordinated	No	Simult. Gap E/W	On	Red	0.0	0.0	1.5	0.0	0.0	1.5					
Force Mode	Fixed	Simult. Gap N/S	On												

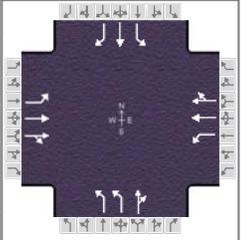
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	25.4	92.3	7.3	74.1	15.5	29.7	10.7	25.0
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	3.5	6.0	3.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	19.9		4.2		7.9	11.7	5.2	15.0
Green Extension Time (g _e), s	2.0	0.0	0.1	0.0	0.3	2.0	0.0	1.3
Phase Call Probability	1.00		0.89		1.00	1.00	0.80	1.00
Max Out Probability	0.00		0.01		0.06	0.04	0.24	0.50

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	492	1123	1123	56	468	454	152	134		42	64	188
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1845	1816	1757	1845	1787	1706	1713		1757	1845	1563
Queue Service Time (g _s), s	17.9	59.2	67.1	2.2	16.3	17.6	5.9	9.7		3.2	4.3	13.0
Cycle Queue Clearance Time (g _c), s	17.9	59.2	67.1	2.2	16.3	17.6	5.9	9.7		3.2	4.3	13.0
Green Ratio (g/C)	0.67	0.65	0.65	0.53	0.52	0.52	0.09	0.18		0.06	0.15	0.32
Capacity (c), veh/h	559	1203	1184	132	964	933	316	315		103	277	501
Volume-to-Capacity Ratio (X)	0.881	0.934	0.949	0.426	0.486	0.486	0.480	0.426		0.405	0.230	0.375
Available Capacity (c _a), veh/h	934	1203	1184	235	964	933	402	358		144	323	541
Back of Queue (Q), veh/ln (95 th percentile)	11.9	15.8	22.2	1.8	10.0	10.7	4.7	7.8		2.7	3.7	8.8
Queue Storage Ratio (RQ) (95 th percentile)	0.86	0.00	0.00	0.30	0.00	0.00	0.35	0.57		0.35	0.00	0.50
Uniform Delay (d ₁), s/veh	17.1	8.7	8.7	30.1	13.5	14.1	60.3	50.6		63.5	53.2	36.7
Incremental Delay (d ₂), s/veh	5.6	14.3	16.4	2.2	1.8	1.8	1.1	1.3		2.5	0.6	0.7
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	22.7	23.0	25.1	32.2	15.2	15.9	61.4	51.9		66.1	53.8	37.4
Level of Service (LOS)	C	C	C	C	B	B	E	D		E	D	D
Approach Delay, s/veh / LOS	23.8		C	16.5		B	57.0		E	45.0		D
Intersection Delay, s/veh / LOS	25.8						C					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS	3.8	D	2.4	B

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Aug 12, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30		
Intersection	Northgate Parkway	File Name	Northgate AM Total Dual Shared Th Rt.xus				
Project Description	Future Conditions						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information				Signal Timing (s)								Signal Phases												
Cycle, s	140.0	Reference Phase	2	Green	3.8	14.6	68.1	7.2	1.2	19.0	Yellow	3.5	3.5	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	0.0	0.0	1.5
Offset, s	0	Reference Point	End	Diagram 1				Diagram 2				Diagram 3												
Uncoordinated	No	Simult. Gap E/W	On	Diagram 4				Diagram 5				Diagram 6												
Force Mode	Fixed	Simult. Gap N/S	On	Diagram 7				Diagram 8				Diagram 9												

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.984			0.969			0.929			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3501		1757	3311		3412	983		1757	1845	
Proportion of Vehicles Arriving on Green (P)	0.16	0.87	0.65	0.03	0.70	0.52	0.09	0.18	0.18	0.06	0.15	0.15
Incremental Delay Factor (k)	0.13	0.50	0.50	0.11	0.50	0.50	0.11	0.15		0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	2.5	4.0	2.5	4.0
Green Ratio (g/C)	0.67	0.65	0.53	0.52	0.09	0.18	0.06	0.15
Permitted Saturation Flow Rate (s_p), veh/h/ln	598	0	167	0	0	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	71.1	0.0	69.1	0.0	0.0	0.0	0.0	0.0
Permitted Service Time (g_u), s	51.5	0.0	17.1	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	51.5		17.1					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln								1563
Protected Right Effective Green Time (g_R), s								23.9

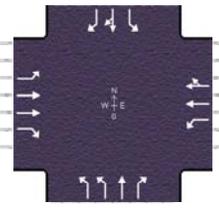
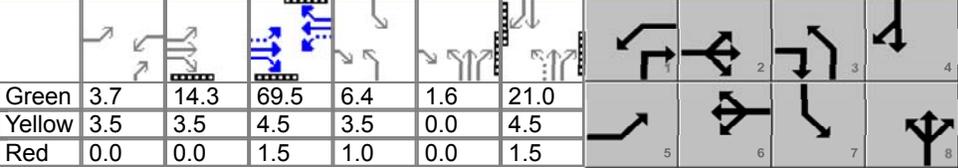
Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1232.48	10.31	973.35	18.45	339.16	48.27	271.43	52.29
Bicycle F_w / F_v	-2.57	2.26	-2.57	0.81	-2.57	0.47	-2.57	0.48

--- **Messages** ---

No errors or warnings exist.

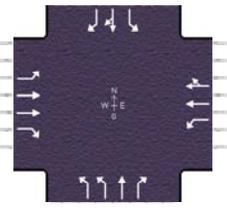
--- **Comments** ---

HCS 2010 Signalized Intersection Input Data

General Information					Intersection Information											
Agency	Eriksson Engineering				Duration, h	0.25										
Analyst	SBC	Analysis Date	Dec 21, 2015		Area Type	Other										
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour		PHF	0.91										
Urban Street	Dundee Road (IL 68)	Analysis Year	2026		Analysis Period	1 > 7:30										
Intersection	Northgate Parkway	File Name	Northgate AM Total with IDOT Recs.xus													
Project Description	IDOT Geometrics															
Demand Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					448	1954	90	51	765	74	138	70	52	38	58	171
Signal Information																
Cycle, s	140.0	Reference Phase	2													
Offset, s	0	Reference Point	End													
Uncoordinated	No	Simult. Gap E/W	On													
Force Mode	Fixed	Simult. Gap N/S	On													
Green					3.7	14.3	69.5	6.4	1.6	21.0						
Yellow					3.5	3.5	4.5	3.5	0.0	4.5						
Red					0.0	0.0	1.5	1.0	0.0	1.5						
Traffic Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					448	1954	90	51	765	74	138	70	52	38	58	171
Initial Queue (Q _b), veh/h					0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h					1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h					None			None			None			None		
Heavy Vehicles (P _{HV}), %					3	3	0	3	3		3	3	0	3	3	3
Ped / Bike / RTOR, /h					0	0	0	0	0	0	0	0	0	0	0	50
Buses (N _b), buses/h					0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)					3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)					1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft					12.0	12.0	12.0	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0
Turn Bay Length, ft					355	0	145	150	0		350	350	75	200	0	450
Grade (P _g), %						0			0			0			0	
Speed Limit, mi/h					35	35	35	35	35	35	25	25	25	30	30	30
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Maximum Green (G _{max}) or Phase Split, s					51.8	84.0	12.0	44.2	14.0	30.0	14.0	30.0				
Yellow Change Interval (Y), s					3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5				
Red Clearance Interval (R _c), s					0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5				
Minimum Green (G _{min}), s					3	15	3	15	8	8	8	21				
Start-Up Lost Time (lt), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0				
Extension of Effective Green (e), s					3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0				
Passage (PT), s					3.0	4.0	3.0	4.0	3.0	4.0	3.0	4.0				
Recall Mode					Off	Max	Off	Off	Off	Min	Off	Off				
Dual Entry					No	Yes	No	Yes	No	Yes	No	Yes				
Walk (Walk), s					0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0				
Pedestrian Clearance Time (PC), s					0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0				
Multimodal Information					EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius					35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft					9.0	8	75	9.0	8	75	9.0	8	75	9.0	8	75
Street Width / Island / Curb					0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft					12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking					Yes	0.00		Yes	0.00		Yes	0.00		Yes	0.00	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30		
Intersection	Northgate Parkway	File Name	Northgate AM Total with IDOT Recs.xus				
Project Description	IDOT Geometrics						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information				Signal Timing Diagram								
Cycle, s	140.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
Green	3.7	14.3	69.5	6.4	1.6	21.0						
Yellow	3.5	3.5	4.5	3.5	0.0	4.5						
Red	0.0	0.0	1.5	1.0	0.0	1.5						

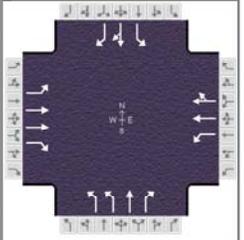
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	3.0	2.0	3.0
Phase Duration, s	25.0	93.3	7.2	75.5	12.5	28.6	10.9	27.0
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	19.5		4.1		7.1	7.0	5.2	10.5
Green Extension Time (g _e), s	2.0	0.0	0.1	0.0	0.1	1.8	0.0	1.6
Phase Call Probability	1.00		0.89		1.00	1.00	0.80	1.00
Max Out Probability	0.00		0.00		1.00	0.00	0.89	0.03

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	492	2147	99	56	468	454	152	77	57	42	90	106
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1787	1706	1845	1610	1757	1762	1563
Queue Service Time (g _s), s	17.5	43.6	2.3	2.1	15.6	16.9	5.1	5.0	4.0	3.2	6.3	8.5
Cycle Queue Clearance Time (g _c), s	17.5	43.6	2.3	2.1	15.6	16.9	5.1	5.0	4.0	3.2	6.3	8.5
Green Ratio (g/C)	0.68	0.66	0.75	0.54	0.53	0.53	0.22	0.18	0.22	0.05	0.16	0.16
Capacity (c), veh/h	563	2437	1211	165	982	951	617	324	348	93	289	257
Volume-to-Capacity Ratio (X)	0.875	0.881	0.082	0.339	0.477	0.477	0.246	0.238	0.164	0.448	0.312	0.414
Available Capacity (c _a), veh/h	955	2437	1211	282	982	951	654	383	400	132	346	307
Back of Queue (Q), veh/ln (95 th percentile)	11.6	10.8	1.3	1.6	9.6	10.3	4.0	4.3	3.0	2.8	5.2	6.3
Queue Storage Ratio (RQ) (95 th percentile)	0.83	0.00	0.23	0.28	0.00	0.00	0.29	0.32	1.01	0.35	0.00	0.36
Uniform Delay (d ₁), s/veh	16.4	6.5	4.6	19.8	12.6	13.3	44.5	50.5	44.6	64.3	52.4	52.5
Incremental Delay (d ₂), s/veh	5.0	5.0	0.1	1.2	1.7	1.7	0.2	0.5	0.3	3.3	0.9	1.5
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	21.4	11.5	4.7	21.0	14.3	15.0	44.7	51.0	44.9	67.6	53.2	54.0
Level of Service (LOS)	C	B	A	C	B	B	D	D	D	E	D	D
Approach Delay, s/veh / LOS	13.0		B	15.0		B	46.5		D	56.1		E
Intersection Delay, s/veh / LOS	18.1						B					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS	3.8	D	2.4	B

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30
Intersection	Northgate Parkway	File Name	Northgate AM Total with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information												
Cycle, s	140.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	3.7	14.3	69.5	6.4	1.6	21.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	0.0	4.5		
				Red	0.0	0.0	1.5	1.0	0.0	1.5		

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.969			0.000			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3311		3412	1845		1757	1762	
Proportion of Vehicles Arriving on Green (P)	0.16	0.88	0.66	0.03	0.71	0.53	0.06	0.18	0.18	0.05	0.16	0.16
Incremental Delay Factor (k)	0.12	0.50	0.50	0.11	0.50	0.50	0.11	0.15	0.15	0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.68	0.66	0.54	0.53	0.22	0.18	0.05	0.16
Permitted Saturation Flow Rate (s_p), veh/h/ln	598	0	184	0	1320	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	72.5	0.0	70.5	0.0	22.1	0.0	0.0	0.0
Permitted Service Time (g_u), s	53.5	0.0	41.6	0.0	15.7	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	53.5		12.7		0.4			
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610				1610		0
Protected Right Effective Green Time (g_R), s		13.0				5.7		0.0

Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1247.09	9.92	992.90	17.75	322.21	49.26	300.00	50.57
Bicycle F_w / F_v	-2.57	2.26	-2.57	0.81	-2.57	0.47	-2.57	0.39

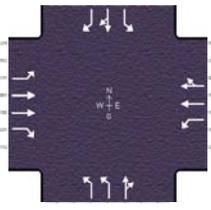
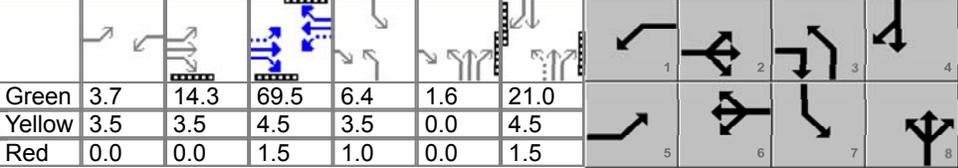
--- Messages ---

WARNING: Since queue spillover from turn lanes and spillback into upstream intersections is not accounted for in the HCM procedures, use of a simulation tool may be advised in situations where the Queue Storage Ratio exceeds 1.0.

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

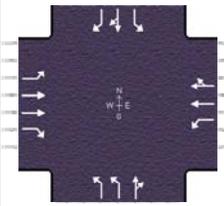
--- Comments ---

HCS 2010 Signalized Intersection Input Data

General Information					Intersection Information											
Agency	Eriksson Engineering				Duration, h	0.25										
Analyst	SBC	Analysis Date	Dec 21, 2015		Area Type	Other										
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour		PHF	0.91										
Urban Street	Dundee Road (IL 68)		Analysis Year	2026		Analysis Period	1 > 7:30									
Intersection	Northgate Parkway		File Name	Northgate AM Total with IDOT wo Nb Rt.xus												
Project Description	IDOT Geometrics															
Demand Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					448	1954	90	51	765	74	138	70	52	38	58	171
Signal Information																
Cycle, s	140.0	Reference Phase	2													
Offset, s	0	Reference Point	End													
Uncoordinated	No	Simult. Gap E/W	On													
Force Mode	Fixed	Simult. Gap N/S	On													
Green					3.7	14.3	69.5	6.4	1.6	21.0						
Yellow					3.5	3.5	4.5	3.5	0.0	4.5						
Red					0.0	0.0	1.5	1.0	0.0	1.5						
Traffic Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					448	1954	90	51	765	74	138	70	52	38	58	171
Initial Queue (Q _b), veh/h					0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h					1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h					None			None			None			None		
Heavy Vehicles (P _{HV}), %					3	3	0	3	3		3	3		3	3	3
Ped / Bike / RTOR, /h					0	0	0	0	0	0	0	0	0	0	0	50
Buses (N _b), buses/h					0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)					3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)					1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft					12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	12.0
Turn Bay Length, ft					355	0	145	150	0		350	350		200	0	450
Grade (P _g), %						0			0			0			0	
Speed Limit, mi/h					35	35	35	35	35	35	25	25	25	30	30	30
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Maximum Green (G _{max}) or Phase Split, s					51.8	84.0	12.0	44.2	14.0	30.0	14.0	30.0				
Yellow Change Interval (Y), s					3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5				
Red Clearance Interval (R _c), s					0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5				
Minimum Green (G _{min}), s					3	15	3	15	8	8	8	21				
Start-Up Lost Time (lt), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0				
Extension of Effective Green (e), s					3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0				
Passage (PT), s					3.0	4.0	3.0	4.0	3.0	4.0	3.0	4.0				
Recall Mode					Off	Max	Off	Off	Off	Min	Off	Off				
Dual Entry					No	Yes	No	Yes	No	Yes	No	Yes				
Walk (Walk), s					0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0				
Pedestrian Clearance Time (PC), s					0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0				
Multimodal Information					EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius					35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft					9.0	8	75	9.0	8	75	9.0	8	75	9.0	8	75
Street Width / Island / Curb					0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft					12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking					Yes	0.00		Yes	0.00		Yes	0.00		Yes	0.00	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30		
Intersection	Northgate Parkway	File Name	Northgate AM Total with IDOT wo Nb Rt.xus				
Project Description	IDOT Geometrics						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information													
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	3.7	14.3	69.5	6.4	1.6	21.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	0.0	4.5			
				Red	0.0	0.0	1.5	1.0	0.0	1.5			

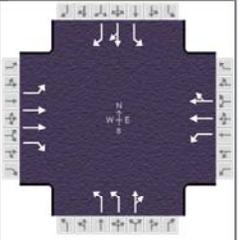
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	2.0	3.0
Phase Duration, s	25.0	93.3	7.2	75.5	12.5	28.6	10.9	27.0
Change Period, ($Y+R_c$), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g_s), s	19.5		4.1		7.1	11.8	5.2	10.5
Green Extension Time (g_e), s	2.0	0.0	0.1	0.0	0.1	1.6	0.0	1.6
Phase Call Probability	1.00		0.89		1.00	1.00	0.80	1.00
Max Out Probability	0.00		0.00		1.00	0.03	0.89	0.03

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	492	2147	99	56	468	454	152	134		42	90	106
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1787	1706	1713		1757	1762	1563
Queue Service Time (g_s), s	17.5	43.6	2.3	2.1	15.6	16.9	5.1	9.8		3.2	6.3	8.5
Cycle Queue Clearance Time (g_c), s	17.5	43.6	2.3	2.1	15.6	16.9	5.1	9.8		3.2	6.3	8.5
Green Ratio (g/C)	0.68	0.66	0.75	0.54	0.53	0.53	0.22	0.18		0.05	0.16	0.16
Capacity (c), veh/h	563	2437	1211	165	982	951	617	300		93	289	257
Volume-to-Capacity Ratio (X)	0.875	0.881	0.082	0.339	0.477	0.477	0.246	0.446		0.448	0.312	0.414
Available Capacity (c_a), veh/h	955	2437	1211	282	982	951	654	356		132	346	307
Back of Queue (Q), veh/ln (95 th percentile)	11.6	10.8	1.3	1.6	9.6	10.3	4.0	7.8		2.8	5.2	6.3
Queue Storage Ratio (RQ) (95 th percentile)	0.83	0.00	0.23	0.28	0.00	0.00	0.29	0.57		0.35	0.00	0.36
Uniform Delay (d_1), s/veh	16.4	6.5	4.6	19.8	12.6	13.3	44.5	51.6		64.3	52.4	52.5
Incremental Delay (d_2), s/veh	5.0	5.0	0.1	1.2	1.7	1.7	0.2	1.5		3.3	0.9	1.5
Initial Queue Delay (d_3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	21.4	11.5	4.7	21.0	14.3	15.0	44.7	53.1		67.6	53.2	54.0
Level of Service (LOS)	C	B	A	C	B	B	D	D		E	D	D
Approach Delay, s/veh / LOS	13.0		B	15.0		B	48.7		D	56.1		E
Intersection Delay, s/veh / LOS	18.3						B					

Multimodal Results	EB	WB	NB	SB				
Pedestrian LOS Score / LOS								
Bicycle LOS Score / LOS	3.8	D	2.4	B	2.0	B	2.0	A

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	AM Peak Hour	PHF	0.91
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 7:30
Intersection	Northgate Parkway	File Name	Northgate AM Total with IDOT wo Nb Rt.xus		
Project Description	IDOT Geomtrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	448	1954	90	51	765	74	138	70	52	38	58	171

Signal Information												
Cycle, s	140.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	3.7	14.3	69.5	6.4	1.6	21.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	0.0	4.5		
				Red	0.0	0.0	1.5	1.0	0.0	1.5		

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.969			0.929			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3311		3412	983		1757	1762	
Proportion of Vehicles Arriving on Green (P)	0.16	0.88	0.66	0.03	0.71	0.53	0.06	0.18	0.18	0.05	0.16	0.16
Incremental Delay Factor (k)	0.12	0.50	0.50	0.11	0.50	0.50	0.11	0.15		0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.68	0.66	0.54	0.53	0.22	0.18	0.05	0.16
Permitted Saturation Flow Rate (s_p), veh/h/ln	598	0	184	0	1320	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	72.5	0.0	70.5	0.0	22.1	0.0	0.0	0.0
Permitted Service Time (g_u), s	53.5	0.0	41.6	0.0	15.7	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	53.5		12.7		0.4			
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610						0
Protected Right Effective Green Time (g_R), s		13.0						0.0

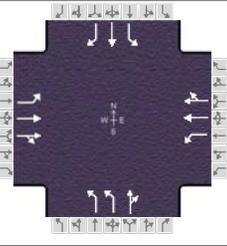
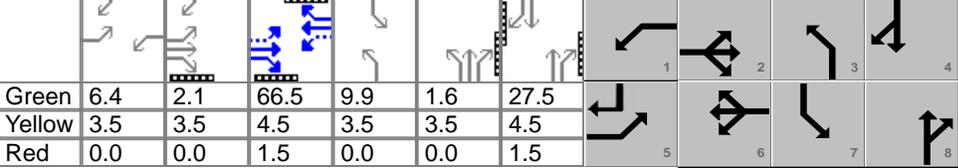
Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1247.09	9.92	992.90	17.75	322.21	49.26	300.00	50.57
Bicycle F_w / F_v	-2.57	2.26	-2.57	0.81	-2.57	0.47	-2.57	0.39

--- Messages ---

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

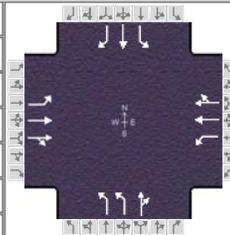
--- Comments ---

HCS 2010 Signalized Intersection Input Data

General Information						Intersection Information											
Agency		Eriksson Engineering				Duration, h		0.25									
Analyst		SBC		Analysis Date		Aug 12, 2015		Area Type		Other							
Jurisdiction		IDOT/Wheeling		Time Period		PM Peak Hour		PHF		0.93							
Urban Street		Dundee Road (IL 68)		Analysis Year		2026		Analysis Period		1 > 16:00							
Intersection		Northgate Parkway		File Name		Northgate PM Total Dual Lt Shared Th Rt.xus											
Project Description		Total Conditions															
Demand Information				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Demand (v), veh/h				163	1062	141	102	1619	33	155	71	46	95	93	493		
Signal Information																	
Cycle, s		140.0	Reference Phase		2												
Offset, s		0	Reference Point		End												
Uncoordinated		No	Simult. Gap E/W		On												
Force Mode		Fixed	Simult. Gap N/S		On												
Green				6.4	2.1	66.5	9.9	1.6	27.5								
Yellow				3.5	3.5	4.5	3.5	3.5	4.5								
Red				0.0	0.0	1.5	0.0	0.0	1.5								
Traffic Information				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Demand (v), veh/h				163	1062	141	102	1619	33	155	71	46	95	93	493		
Initial Queue (Q _b), veh/h				0	0	0	0	0	0	0	0	0	0	0	0		
Base Saturation Flow Rate (s ₀), veh/h				1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900		
Parking (N _m), man/h				None			None			None			None				
Heavy Vehicles (P _{HV}), %				3	3		3	3		3	3		3	3	3		
Ped / Bike / RTOR, /h				0	0	0	0	0	0	0	0	0	0	0	0		
Buses (N _b), buses/h				0	0	0	0	0	0	0	0	0	0	0	0		
Arrival Type (AT)				3	4	3	3	4	3	3	3	3	3	3	3		
Upstream Filtering (I)				1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Lane Width (W), ft				12.0	12.0		12.0	12.0		12.0	12.0		12.0	12.0	12.0		
Turn Bay Length, ft				355	0		150	0		350	350		200	0	450		
Grade (P _g), %					0			0			0			0			
Speed Limit, mi/h				35	35	35	35	35	35	25	25	25	30	30	30		
Phase Information				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT						
Maximum Green (G _{max}) or Phase Split, s				16.0	73.0	15.0	72.0	20.0	32.0	20.0	32.0	20.0	32.0				
Yellow Change Interval (Y), s				3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5				
Red Clearance Interval (R _c), s				0.0	1.5	0.0	1.5	0.0	1.5	0.0	1.5	0.0	1.5				
Minimum Green (G _{min}), s				12	15	3	15	15	8	10	8						
Start-Up Lost Time (I _t), s				2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0				
Extension of Effective Green (e), s				3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0	3.0	4.0				
Passage (PT), s				3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0	3.0	4.0				
Recall Mode				Off	Min	Off	Min	Off	Off	Off	Off	Off					
Dual Entry				No	Yes	No	Yes	No	Yes	No	Yes	No	Yes				
Walk (Walk), s				0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0				
Pedestrian Clearance Time (PC), s				0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0	0.0	29.0				
Multimodal Information				EB			WB			NB			SB				
85th % Speed / Rest in Walk / Corner Radius				35	No	50	35	No	50	30	No	50	30	No	50		
Walkway / Crosswalk Width / Length, ft				9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65		
Street Width / Island / Curb				0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes		
Width Outside / Bike Lane / Shoulder, ft				12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0		
Pedestrian Signal / Occupied Parking				Yes	0.00		Yes	0.00		Yes	0.00		Yes	0.00			

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Aug 12, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00		
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual Lt Shared Th Rt.xus				
Project Description	Total Conditions						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information				Signal Timing Diagram									
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On										
Force Mode	Fixed	Simult. Gap N/S	On										
		Green		6.4	2.1	66.5	9.9	1.6	27.5				
		Yellow		3.5	3.5	4.5	3.5	3.5	4.5				
		Red		0.0	0.0	1.5	0.0	0.0	1.5				

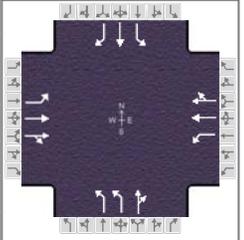
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	15.5	78.1	9.9	72.5	18.5	38.6	13.4	33.5
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	3.5	6.0	3.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.4	4.2	5.4
Queue Clearance Time (g _s), s	10.5		6.3		8.4	10.3	10.0	31.5
Green Extension Time (g _e), s	0.1	0.0	0.1	0.0	0.4	5.2	0.1	0.0
Phase Call Probability	1.00		0.99		1.00	1.00	0.98	1.00
Max Out Probability	1.00		0.38		0.04	0.07	0.11	1.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	175	659	635	110	889	887	167	126		102	100	530
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1845	1769	1757	1845	1831	1706	1723		1757	1845	1563
Queue Service Time (g _s), s	8.5	25.3	28.2	4.3	60.2	61.3	6.4	8.3		8.0	6.3	29.5
Cycle Queue Clearance Time (g _c), s	8.5	25.3	28.2	4.3	60.2	61.3	6.4	8.3		8.0	6.3	29.5
Green Ratio (g/C)	0.59	0.55	0.55	0.53	0.51	0.51	0.11	0.25		0.08	0.21	0.31
Capacity (c), veh/h	226	1016	974	270	942	936	389	426		137	389	486
Volume-to-Capacity Ratio (X)	0.775	0.648	0.651	0.407	0.944	0.948	0.428	0.296		0.746	0.257	1.091
Available Capacity (c _a), veh/h	233	1016	974	334	942	936	426	426		220	389	486
Back of Queue (Q), veh/ln (95 th percentile)	11.2	13.5	15.5	3.3	33.7	34.5	5.1	6.6		7.0	5.4	36.1
Queue Storage Ratio (RQ) (95 th percentile)	0.81	0.00	0.00	0.56	0.00	0.00	0.37	0.48		0.89	0.00	2.06
Uniform Delay (d ₁), s/veh	39.0	12.6	14.1	19.3	22.3	21.1	57.8	42.8		63.2	46.9	48.2
Incremental Delay (d ₂), s/veh	14.7	3.2	3.4	1.0	18.5	19.3	0.7	0.5		7.8	0.5	67.8
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	53.6	15.8	17.5	20.3	40.8	40.4	58.5	43.4		71.0	47.4	116.1
Level of Service (LOS)	D	B	B	C	D	D	E	D		E	D	F
Approach Delay, s/veh / LOS	21.0		C	39.4		D	52.0		D	100.4		F
Intersection Delay, s/veh / LOS	44.3						D					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS	2.8	C	3.1	C

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Aug 12, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00		
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual Lt Shared Th Rt.xus				
Project Description	Total Conditions						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information													
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On										
Force Mode	Fixed	Simult. Gap N/S	On										
		Green		6.4	2.1	66.5	9.9	1.6	27.5				
		Yellow		3.5	3.5	4.5	3.5	3.5	4.5				
		Red		0.0	0.0	1.5	0.0	0.0	1.5				

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.959			0.993			0.934			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3191		1757	3603		3412	1045		1757	1845	
Proportion of Vehicles Arriving on Green (P)	0.09	0.73	0.55	0.05	0.68	0.51	0.11	0.25	0.25	0.08	0.21	0.21
Incremental Delay Factor (k)	0.31	0.50	0.50	0.11	0.50	0.50	0.11	0.15		0.11	0.15	0.50

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	2.5	4.0	2.5	4.0
Green Ratio (g/C)	0.59	0.55	0.53	0.51	0.11	0.25	0.08	0.21
Permitted Saturation Flow Rate (s_p), veh/h/ln	264	0	421	0	0	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	69.5	0.0	67.5	0.0	0.0	0.0	0.0	0.0
Permitted Service Time (g_u), s	6.3	0.0	41.9	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	6.3		9.0					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln								1563
Protected Right Effective Green Time (g_R), s								14.0

Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1030.54	16.45	950.20	19.29	465.56	41.20	393.17	45.18
Bicycle F_w / F_v	-2.57	1.21	-2.57	1.56	-2.57	0.48	-2.57	1.21

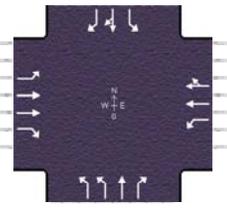
--- **Messages** ---

WARNING: Since queue spillover from turn lanes and spillback into upstream intersections is not accounted for in the HCM procedures, use of a simulation tool may be advised in situations where the Queue Storage Ratio exceeds 1.0.

--- **Comments** ---

HCS 2010 Signalized Intersection Input Data

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 20, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information													
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	6.3	1.2	67.6	9.9	0.6	26.5			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	3.5	4.5			
				Red	0.0	0.0	1.5	1.0	1.0	1.5			

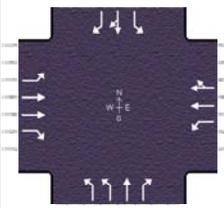
Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		None			None			None			None	
Heavy Vehicles (P _{HV}), %	3	3	0	3	3		3	3	0	3	3	3
Ped / Bike / RTOR, /h	0	0	0	0	0	0	0	0	0	0	0	50
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0	12.0	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0
Turn Bay Length, ft	355	0	0	150	0		350	350	150	200	0	450
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	25	25	25	30	30	30

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	16.0	73.0	15.0	72.0	20.0	32.0	20.0	32.0
Yellow Change Interval (Y), s	3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5
Red Clearance Interval (R _c), s	0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5
Minimum Green (G _{min}), s	11	15	3	15	15	8	10	8
Start-Up Lost Time (lt), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Passage (PT), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s	0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0
Pedestrian Clearance Time (PC), s	0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft	9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65
Street Width / Island / Curb	0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking	Yes	0.00										

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 20, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information				Signal Timing Diagram								
Cycle, s	140.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
Green	6.3	1.2	67.6	9.9	0.6	26.5						
Yellow	3.5	3.5	4.5	3.5	3.5	4.5						
Red	0.0	0.0	1.5	1.0	1.0	1.5						

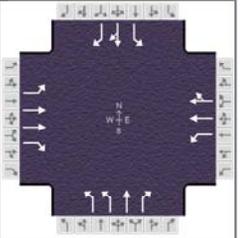
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	2.0	3.0	2.0	3.0
Phase Duration, s	14.5	78.2	9.8	73.6	19.5	37.6	14.4	32.5
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	10.1		6.3		8.4	6.6	10.0	26.4
Green Extension Time (g _e), s	0.1	0.0	0.1	0.0	0.3	4.5	0.1	0.0
Phase Call Probability	1.00		0.99		1.00	1.00	0.98	1.00
Max Out Probability	1.00		0.35		0.10	0.03	0.28	1.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	175	1142	152	110	889	887	167	76	49	102	295	281
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1831	1706	1845	1610	1757	1659	1563
Queue Service Time (g _s), s	8.1	19.4	4.4	4.3	58.4	59.4	6.4	4.6	3.1	8.0	24.2	24.4
Cycle Queue Clearance Time (g _c), s	8.1	19.4	4.4	4.3	58.4	59.4	6.4	4.6	3.1	8.0	24.2	24.4
Green Ratio (g/C)	0.59	0.55	0.69	0.54	0.52	0.52	0.11	0.24	0.30	0.08	0.20	0.20
Capacity (c), veh/h	219	2040	1118	319	956	949	389	442	481	137	337	318
Volume-to-Capacity Ratio (X)	0.800	0.560	0.136	0.343	0.930	0.935	0.428	0.173	0.103	0.747	0.875	0.884
Available Capacity (c _a), veh/h	239	2040	1118	385	956	949	402	443	482	207	338	318
Back of Queue (Q), veh/ln (95 th percentile)	11.4	10.4	2.8	3.2	32.0	32.8	5.1	3.9	2.3	7.0	17.9	17.4
Queue Storage Ratio (RQ) (95 th percentile)	0.82	0.00	0.00	0.54	0.00	0.00	0.37	0.29	0.38	0.89	0.00	0.99
Uniform Delay (d ₁), s/veh	37.7	11.7	7.2	17.1	21.6	20.0	57.8	43.0	35.5	63.2	55.0	54.2
Incremental Delay (d ₂), s/veh	16.2	1.1	0.3	0.6	16.5	17.1	0.7	0.3	0.1	7.9	22.1	24.5
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	53.9	12.8	7.5	17.8	38.0	37.2	58.5	43.2	35.6	71.1	77.1	78.7
Level of Service (LOS)	D	B	A	B	D	D	E	D	D	E	E	E
Approach Delay, s/veh / LOS	17.1	B		36.5	D		50.6	D		76.9	E	
Intersection Delay, s/veh / LOS	37.2						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS												
Bicycle LOS Score / LOS	2.8	C		3.1	C		2.0	B		2.7	B	

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 20, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information													
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	6.3	1.2	67.6	9.9	0.6	26.5			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	3.5	4.5			
				Red	0.0	0.0	1.5	1.0	1.0	1.5			

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.993			0.000			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3603		3412	1845		1757	1659	
Proportion of Vehicles Arriving on Green (P)	0.09	0.74	0.55	0.05	0.69	0.52	0.11	0.24	0.24	0.08	0.20	0.20
Incremental Delay Factor (k)	0.29	0.50	0.50	0.11	0.50	0.50	0.11	0.15	0.15	0.11	0.41	0.42

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.59	0.55	0.54	0.52	0.11	0.24	0.08	0.20
Permitted Saturation Flow Rate (s_p), veh/h/ln	264	0	486	0	0	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	70.6	0.0	68.6	0.0	0.0	0.0	0.0	0.0
Permitted Service Time (g_u), s	9.1	0.0	50.8	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	9.1		5.2					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610				1610		0
Protected Right Effective Green Time (g_R), s		20.0				8.3		0.0

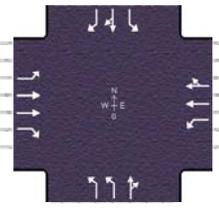
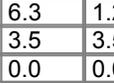
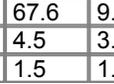
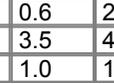
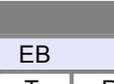
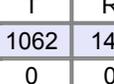
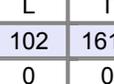
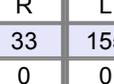
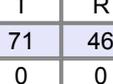
Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1032.02	16.40	965.06	18.74	450.90	42.00	378.30	46.02
Bicycle F_w / F_v	-2.57	1.21	-2.57	1.56	-2.57	0.48	-2.57	1.12

--- Messages ---

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

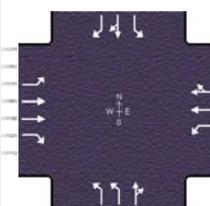
--- Comments ---

HCS 2010 Signalized Intersection Input Data

General Information					Intersection Information																		
Agency	Eriksson Engineering				Duration, h	0.25																	
Analyst	SBC	Analysis Date	Dec 20, 2015		Area Type	Other																	
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour		PHF	0.93																	
Urban Street	Dundee Road (IL 68)		Analysis Year	2026		Analysis Period	1 > 16:00																
Intersection	Northgate Parkway		File Name	Northgate PM Total Dual with IDOT Recs wo Nb...																			
Project Description	IDOT Geometrics																						
Demand Information					EB			WB			NB			SB									
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R							
Demand (v), veh/h					163	1062	141	102	1619	33	155	71	46	95	93	493							
Signal Information																							
Cycle, s	140.0	Reference Phase	2																				
Offset, s	0	Reference Point	End		Green	6.3	1.2	67.6	9.9	0.6	26.5												
Uncoordinated	No	Simult. Gap E/W	On		Yellow	3.5	3.5	4.5	3.5	3.5	4.5												
Force Mode	Fixed	Simult. Gap N/S	On		Red	0.0	0.0	1.5	1.0	1.0	1.5												
Traffic Information					EB			WB			NB			SB									
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R							
Demand (v), veh/h					163	1062	141	102	1619	33	155	71	46	95	93	493							
Initial Queue (Q _b), veh/h					0	0	0	0	0	0	0	0	0	0	0	0							
Base Saturation Flow Rate (s ₀), veh/h					1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900							
Parking (N _m), man/h					None			None			None			None									
Heavy Vehicles (P _{HV}), %					3	3	0	3	3		3	3		3	3	3							
Ped / Bike / RTOR, /h					0	0	0	0	0	0	0	0	0	0	0	50							
Buses (N _b), buses/h					0	0	0	0	0	0	0	0	0	0	0	0							
Arrival Type (AT)					3	4	3	3	4	3	3	3	3	3	3	3							
Upstream Filtering (I)					1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00							
Lane Width (W), ft					12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	12.0							
Turn Bay Length, ft					355	0	0	150	0		350	350		200	0	450							
Grade (P _g), %						0			0			0			0								
Speed Limit, mi/h					35	35	35	35	35	35	25	25	25	30	30	30							
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT											
Maximum Green (G _{max}) or Phase Split, s					16.0	73.0	15.0	72.0	20.0	32.0	20.0	32.0											
Yellow Change Interval (Y), s					3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5											
Red Clearance Interval (R _c), s					0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5											
Minimum Green (G _{min}), s					11	15	3	15	15	8	10	8											
Start-Up Lost Time (lt), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0											
Extension of Effective Green (e), s					3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0											
Passage (PT), s					3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0											
Recall Mode					Off	Min	Off	Min	Off	Off	Off	Off											
Dual Entry					No	Yes	No	Yes	No	Yes	No	Yes											
Walk (Walk), s					0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0											
Pedestrian Clearance Time (PC), s					0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0											
Multimodal Information					EB			WB			NB			SB									
85th % Speed / Rest in Walk / Corner Radius					35	No	50	35	No	50	30	No	50	30	No	50							
Walkway / Crosswalk Width / Length, ft					9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65							
Street Width / Island / Curb					0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes							
Width Outside / Bike Lane / Shoulder, ft					12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0							
Pedestrian Signal / Occupied Parking					Yes	0.00		Yes	0.00		Yes	0.00		Yes	0.00								

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 20, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual with IDOT Recs wo Nb...		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information				Signal Timing Diagram									
Cycle, s	140.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On										
Force Mode	Fixed	Simult. Gap N/S	On										
		Green		6.3	1.2	67.6	9.9	0.6	26.5				
		Yellow		3.5	3.5	4.5	3.5	3.5	4.5				
		Red		0.0	0.0	1.5	1.0	1.0	1.5				

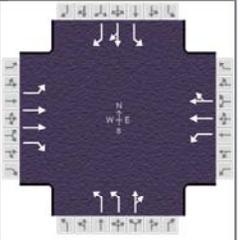
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	14.5	78.2	9.8	73.6	19.5	37.6	14.4	32.5
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	10.1		6.3		8.4	10.4	10.0	26.4
Green Extension Time (g _e), s	0.1	0.0	0.1	0.0	0.3	4.3	0.1	0.0
Phase Call Probability	1.00		0.99		1.00	1.00	0.98	1.00
Max Out Probability	1.00		0.35		0.10	0.06	0.28	1.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	175	1142	152	110	889	887	167	126		102	295	281
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1831	1706	1723		1757	1659	1563
Queue Service Time (g _s), s	8.1	19.4	4.4	4.3	58.4	59.4	6.4	8.4		8.0	24.2	24.4
Cycle Queue Clearance Time (g _c), s	8.1	19.4	4.4	4.3	58.4	59.4	6.4	8.4		8.0	24.2	24.4
Green Ratio (g/C)	0.59	0.55	0.69	0.54	0.52	0.52	0.31	0.24		0.08	0.20	0.20
Capacity (c), veh/h	219	2040	1118	319	956	949	389	413		137	337	318
Volume-to-Capacity Ratio (X)	0.800	0.560	0.136	0.343	0.930	0.935	0.428	0.305		0.747	0.875	0.884
Available Capacity (c _a), veh/h	239	2040	1118	385	956	949	402	414		207	338	318
Back of Queue (Q), veh/ln (95 th percentile)	11.4	10.4	2.8	3.2	32.0	32.8	5.1	6.7		7.0	17.9	17.4
Queue Storage Ratio (RQ) (95 th percentile)	0.82	0.00	0.00	0.54	0.00	0.00	0.37	0.49		0.89	0.00	0.99
Uniform Delay (d ₁), s/veh	37.7	11.7	7.2	17.1	21.6	20.0	57.8	43.6		63.2	55.0	54.2
Incremental Delay (d ₂), s/veh	16.2	1.1	0.3	0.6	16.5	17.1	0.7	0.6		7.9	22.1	24.5
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	53.9	12.8	7.5	17.8	38.0	37.2	58.5	44.2		71.1	77.1	78.7
Level of Service (LOS)	D	B	A	B	D	D	E	D		E	E	E
Approach Delay, s/veh / LOS	17.1		B	36.5		D	52.4		D	76.9		E
Intersection Delay, s/veh / LOS	37.3						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS												
Bicycle LOS Score / LOS	2.8		C	3.1		C	2.0		B	2.7		B

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 20, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	PM Peak Hour	PHF	0.93
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 16:00
Intersection	Northgate Parkway	File Name	Northgate PM Total Dual with IDOT Recs wo Nb...		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	163	1062	141	102	1619	33	155	71	46	95	93	493

Signal Information												
Cycle, s	140.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	6.3	1.2	67.6	9.9	0.6	26.5		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	3.5	4.5	3.5	3.5	4.5		
				Red	0.0	0.0	1.5	1.0	1.0	1.5		

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.993			0.934			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3603		3412	1045		1757	1659	
Proportion of Vehicles Arriving on Green (P)	0.09	0.74	0.55	0.05	0.69	0.52	0.11	0.24	0.24	0.08	0.20	0.20
Incremental Delay Factor (k)	0.29	0.50	0.50	0.11	0.50	0.50	0.11	0.15		0.11	0.41	0.42

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.59	0.55	0.54	0.52	0.31	0.24	0.08	0.20
Permitted Saturation Flow Rate (s_p), veh/h/ln	264	0	486	0	1277	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	70.6	0.0	68.6	0.0	27.0	0.0	0.0	0.0
Permitted Service Time (g_u), s	9.1	0.0	50.8	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	9.1		5.2					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610						0
Protected Right Effective Green Time (g_R), s		20.0						0.0

Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v								
Pedestrian F_s / F_{delay}								
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	1032.02	16.40	965.06	18.74	450.90	42.00	378.30	46.02
Bicycle F_w / F_v	-2.57	1.21	-2.57	1.56	-2.57	0.48	-2.57	1.12

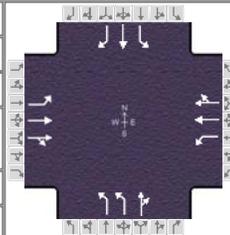
--- Messages ---

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

--- Comments ---

HCS 2010 Signalized Intersection Input Data

General Information				Intersection Information			
Agency	Eriksson Engineering			Duration, h	0.25		
Analyst	SBC	Analysis Date	Oct 8, 2015	Area Type	Other		
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97		
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30		
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual Lt Shared Th Rt.xus				
Project Description	Total Conditions						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

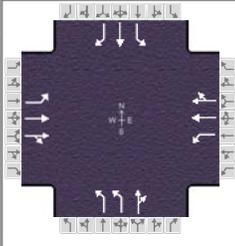
Signal Information				Signal Timing (s)								Signal Phases												
Cycle, s	120.0	Reference Phase	2	Green	6.3	1.4	57.6	8.5	2.0	21.7	Yellow	3.5	0.0	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	0.0	0.0	1.5
Offset, s	0	Reference Point	End	Green				Yellow				Red												
Uncoordinated	No	Simult. Gap E/W	On	Green				Yellow				Red												
Force Mode	Fixed	Simult. Gap N/S	On	Green				Yellow				Red												

Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h	None			None			None			None		
Heavy Vehicles (P _{HV}), %	3	3		3	3		3	3		3	3	3
Ped / Bike / RTOR, /h	0	0	0	0	0	0	0	0	0	0	0	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0		12.0	12.0		12.0	12.0		12.0	12.0	12.0
Turn Bay Length, ft	355	0		150	0		350	350		200	0	450
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	25	25	25	30	30	30

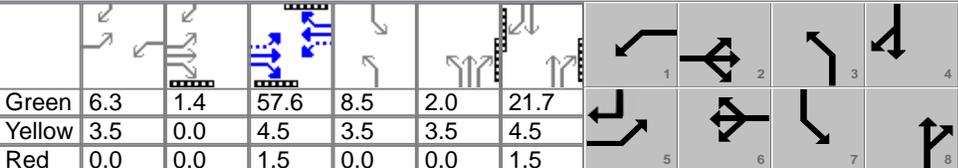
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	14.4	60.4	14.4	60.4	20.0	25.2	20.0	25.2
Yellow Change Interval (Y), s	3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5
Red Clearance Interval (R _c), s	0.0	1.5	0.0	1.5	0.0	1.5	0.0	1.5
Minimum Green (G _{min}), s	3	15	3	15	14	8	10	8
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Passage (PT), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s	0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0
Pedestrian Clearance Time (PC), s	0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft	9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65
Street Width / Island / Curb	0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking	No	0.00										

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information		
Agency	Eriksson Engineering			Duration, h	0.25	
Analyst	SBC	Analysis Date	Oct 8, 2015	Area Type	Other	
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97	
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30	
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual Lt Shared Th Rt.xus			
Project Description	Total Conditions					

Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

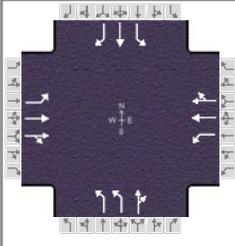
Signal Information																								
Cycle, s	120.0	Reference Phase	2	Green	6.3	1.4	57.6	8.5	2.0	21.7	Yellow	3.5	0.0	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	0.0	0.0	1.5
Offset, s	0	Reference Point	End	Uncoordinated	No	Simult. Gap E/W	On	Force Mode	Fixed	Simult. Gap N/S	On													

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	11.2	65.1	9.8	63.6	17.5	33.2	12.0	27.7
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	3.5	6.0	3.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	7.5		6.2		10.1	31.2	5.7	12.7
Green Extension Time (g _e), s	0.2	0.0	0.1	0.0	0.5	0.0	0.1	1.8
Phase Call Probability	1.00		0.99		1.00	1.00	0.85	1.00
Max Out Probability	1.00		0.61		0.24	1.00	0.00	0.36

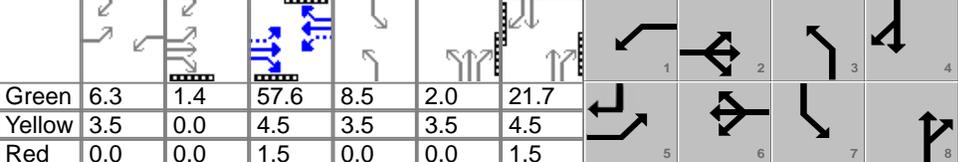
Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	170	855	842	127	771	762	244	174		57	108	172
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1845	1780	1757	1845	1816	1706	1713		1757	1845	1563
Queue Service Time (g _s), s	5.5	42.0	45.8	4.2	34.4	35.6	8.1	10.3		3.7	6.0	10.7
Cycle Queue Clearance Time (g _c), s	5.5	42.0	45.8	4.2	34.4	35.6	8.1	10.3		3.7	6.0	10.7
Green Ratio (g/C)	0.57	0.53	0.53	0.55	0.52	0.52	0.29	0.24		0.08	0.20	0.28
Capacity (c), veh/h	251	985	950	193	963	948	426	417		139	364	435
Volume-to-Capacity Ratio (X)	0.676	0.869	0.886	0.657	0.800	0.804	0.573	0.418		0.408	0.297	0.396
Available Capacity (c _a), veh/h	299	985	950	261	963	948	498	417		256	364	435
Back of Queue (Q), veh/ln (95 th percentile)	4.7	21.4	24.4	3.4	18.0	18.9	6.4	8.0		3.1	5.1	7.6
Queue Storage Ratio (RQ) (95 th percentile)	0.34	0.00	0.00	0.58	0.00	0.00	0.47	0.58		0.39	0.00	0.43
Uniform Delay (d ₁), s/veh	22.8	15.8	16.5	26.3	15.4	14.8	49.5	38.2		52.6	41.9	35.1
Incremental Delay (d ₂), s/veh	4.7	10.3	11.9	3.8	7.0	7.2	1.2	1.0		1.9	0.6	0.8
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	27.5	26.1	28.4	30.0	22.3	22.1	50.7	39.2		54.5	42.5	36.0
Level of Service (LOS)	C	C	C	C	C	C	D	D		D	D	D
Approach Delay, s/veh / LOS	27.3	C		22.8	C		45.9	D		41.2	D	
Intersection Delay, s/veh / LOS	28.4						C					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS								
Bicycle LOS Score / LOS	3.1	C	2.9	C	2.3	B	2.1	B

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information		
Agency	Eriksson Engineering			Duration, h	0.25	
Analyst	SBC	Analysis Date	Oct 8, 2015	Area Type	Other	
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97	
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30	
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual Lt Shared Th Rt.xus			
Project Description	Total Conditions					

Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information																								
Cycle, s	120.0	Reference Phase	2	Green	6.3	1.4	57.6	8.5	2.0	21.7	Yellow	3.5	0.0	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	0.0	0.0	1.5
Offset, s	0	Reference Point	End	Uncoordinated	No	Simult. Gap E/W	On	Force Mode	Fixed	Simult. Gap N/S	On													

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.965			0.984			0.929			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3267		1757	3501		3412	983		1757	1845	
Proportion of Vehicles Arriving on Green (P)	0.07	0.71	0.53	0.06	0.70	0.52	0.12	0.24	0.24	0.08	0.20	0.20
Incremental Delay Factor (k)	0.16	0.50	0.50	0.11	0.50	0.50	0.11	0.15		0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	2.5	4.0	2.5	4.0
Green Ratio (g/C)	0.57	0.53	0.55	0.52	0.29	0.24	0.08	0.20
Permitted Saturation Flow Rate (s_p), veh/h/ln	334	0	285	285	1268	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	59.6	0.0	58.6	55.4	20.2	0.0	0.0	0.0
Permitted Service Time (g_u), s	23.1	0.0	11.3	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	23.1		11.3					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln								1563
Protected Right Effective Green Time (g_R), s								9.7

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v												
Pedestrian F_s / F_{delay}												
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	984.21	15.48	960.42	16.21	453.45	35.88	361.67	40.26				
Bicycle F_w / F_v	-2.57	1.54	-2.57	1.37	-2.57	0.69	-2.57	0.56				

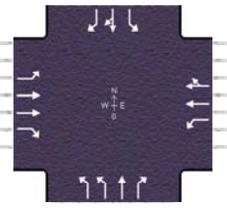
--- **Messages** ---

No errors or warnings exist.

--- **Comments** ---

HCS 2010 Signalized Intersection Input Data

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

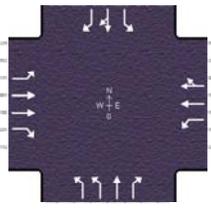
Signal Information				Signal Timing (s)																				
Cycle, s	120.0	Reference Phase	2	Green	6.2	1.4	59.4	8.5	1.0	19.0	Yellow	3.5	0.0	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	1.0	1.0	1.5
Offset, s	0	Reference Point	End																					
Uncoordinated	No	Simult. Gap E/W	On																					
Force Mode	Fixed	Simult. Gap N/S	On																					

Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h	None			None			None			None		
Heavy Vehicles (P _{HV}), %	3	3	0	3	3		3	3	0	3	3	3
Ped / Bike / RTOR, /h	0	0	0	0	0	0	0	0	0	0	0	50
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0	12.0	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0
Turn Bay Length, ft	355	0	0	150	0		350	350	150	200	0	450
Grade (P _g), %	0			0			0			0		
Speed Limit, mi/h	35	35	35	35	35	35	25	25	25	30	30	30

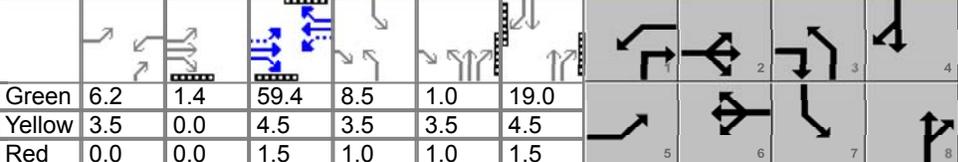
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	14.4	60.4	14.4	60.4	20.0	25.2	20.0	25.2
Yellow Change Interval (Y), s	3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5
Red Clearance Interval (R _c), s	0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5
Minimum Green (G _{min}), s	3	15	3	15	14	8	10	19
Start-Up Lost Time (lt), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Passage (PT), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s	0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0
Pedestrian Clearance Time (PC), s	0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft	9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65
Street Width / Island / Curb	0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking	No	0.00										

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information		
Agency	Eriksson Engineering			Duration, h	0.25	
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other	
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97	
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30	
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs.xus			
Project Description	IDOT Geometrics					

Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information																								
Cycle, s	120.0	Reference Phase	2	Green	6.2	1.4	59.4	8.5	1.0	19.0	Yellow	3.5	0.0	4.5	3.5	3.5	4.5	Red	0.0	0.0	1.5	1.0	1.0	1.5
Offset, s	0	Reference Point	End	Uncoordinated	No	Simult. Gap E/W	On	Force Mode	Fixed	Simult. Gap N/S	On													

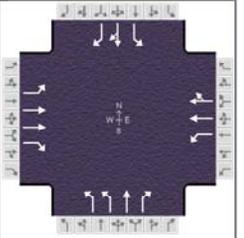
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	2.0	3.0	2.0	3.0
Phase Duration, s	11.1	66.8	9.7	65.4	18.5	30.5	13.0	25.0
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	7.4		6.1		10.1	7.4	5.7	9.4
Green Extension Time (g _e), s	0.2	0.0	0.2	0.0	0.5	2.2	0.1	1.7
Phase Call Probability	1.00		0.99		1.00	1.00	0.85	1.00
Max Out Probability	0.43		0.12		0.47	0.01	0.00	0.15

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	170	1528	169	127	771	762	244	100	74	57	120	109
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1816	1706	1845	1610	1757	1816	1563
Queue Service Time (g _s), s	5.4	29.6	4.1	4.1	32.1	33.4	8.1	5.4	4.1	3.7	7.0	7.4
Cycle Queue Clearance Time (g _c), s	5.4	29.6	4.1	4.1	32.1	33.4	8.1	5.4	4.1	3.7	7.0	7.4
Green Ratio (g/C)	0.58	0.55	0.71	0.56	0.54	0.54	0.12	0.22	0.29	0.08	0.17	0.17
Capacity (c), veh/h	261	2029	1138	247	990	975	426	407	465	139	318	274
Volume-to-Capacity Ratio (X)	0.652	0.753	0.149	0.514	0.778	0.782	0.573	0.245	0.160	0.408	0.378	0.397
Available Capacity (c _a), veh/h	335	2029	1138	341	990	975	469	434	488	242	344	296
Back of Queue (Q), veh/ln (95 th percentile)	4.4	13.8	2.4	3.0	16.3	17.3	6.4	4.5	3.0	3.1	5.9	5.3
Queue Storage Ratio (RQ) (95 th percentile)	0.32	0.00	0.00	0.51	0.00	0.00	0.47	0.33	0.50	0.39	0.00	0.30
Uniform Delay (d ₁), s/veh	21.3	12.4	5.8	18.9	13.7	13.3	49.5	39.3	31.8	52.6	44.6	43.9
Incremental Delay (d ₂), s/veh	2.9	2.6	0.3	1.7	6.0	6.2	1.4	0.4	0.2	1.9	1.1	1.3
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	24.2	15.0	6.0	20.5	19.7	19.6	50.9	39.7	32.0	54.5	45.6	45.2
Level of Service (LOS)	C	B	A	C	B	B	D	D	C	D	D	D
Approach Delay, s/veh / LOS	15.0	B		19.7	B		44.9	D			47.2	D
Intersection Delay, s/veh / LOS	22.0						C					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS								
Bicycle LOS Score / LOS	3.1	C	2.9	C	2.3	B	2.0	B

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs.xus		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
		Green		6.2	1.4	59.4	8.5	1.0	19.0						
		Yellow		3.5	0.0	4.5	3.5	3.5	4.5						
		Red		0.0	0.0	1.5	1.0	1.0	1.5						

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.984			0.000			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3501		3412	1845		1757	1816	
Proportion of Vehicles Arriving on Green (P)	0.07	0.73	0.55	0.06	0.72	0.54	0.12	0.22	0.22	0.08	0.17	0.17
Incremental Delay Factor (k)	0.11	0.50	0.50	0.11	0.50	0.50	0.12	0.15	0.15	0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.58	0.55	0.56	0.54	0.12	0.22	0.08	0.17
Permitted Saturation Flow Rate (s_p), veh/h/ln	334	0	336	0	0	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	61.3	0.0	60.4	0.0	0.0	0.0	0.0	0.0
Permitted Service Time (g_u), s	27.0	0.0	29.2	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	27.0		18.9					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610				1610		0
Protected Right Effective Green Time (g_R), s		19.0				8.2		0.0

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v												
Pedestrian F_s / F_{delay}												
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	1014.05	14.58	990.57	15.28	408.44	38.00	316.67	42.50				
Bicycle F_w / F_v	-2.57	1.54	-2.57	1.37	-2.57	0.69	-2.57	0.47				

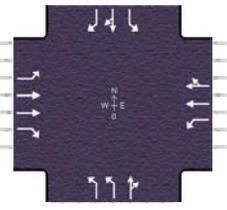
--- Messages ---

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

--- Comments ---

HCS 2010 Signalized Intersection Input Data

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs wo Nb...		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information				Signal Timing (s)																				
Cycle, s	120.0	Reference Phase	2	Green	6.0	1.4	61.6	8.5	3.5	19.0	Yellow	3.5	0.0	4.5	3.5	0.0	4.5	Red	0.0	0.0	1.5	1.0	0.0	1.5
Offset, s	0	Reference Point	End																					
Uncoordinated	No	Simult. Gap E/W	On																					
Force Mode	Fixed	Simult. Gap N/S	On																					

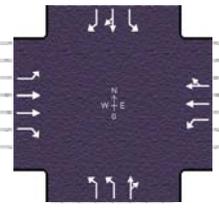
Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	2000	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h	None			None			None			None		
Heavy Vehicles (P _{HV}), %	3	3	0	3	3		3	3		3	3	3
Ped / Bike / RTOR, /h	0	0	0	0	0	0	0	0	0	0	0	50
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	4	3	3	4	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	12.0
Turn Bay Length, ft	355	0	0	150	0		350	350		200	0	450
Grade (P _g), %	0			0			0			0		
Speed Limit, mi/h	35	35	35	35	35	35	25	25	25	30	30	30

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	14.4	60.4	14.4	60.4	20.0	25.2	20.0	25.2
Yellow Change Interval (Y), s	3.5	4.5	3.5	4.5	3.5	4.5	3.5	4.5
Red Clearance Interval (R _c), s	0.0	1.5	0.0	1.5	1.0	1.5	1.0	1.5
Minimum Green (G _{min}), s	3	15	3	15	12	8	10	19
Start-Up Lost Time (lt), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Passage (PT), s	3.0	7.0	3.0	7.0	3.0	4.0	3.0	4.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s	0.0	7.0	0.0	7.0	0.0	7.0	0.0	7.0
Pedestrian Clearance Time (PC), s	0.0	23.0	0.0	23.0	0.0	29.0	0.0	29.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	35	No	50	35	No	50	30	No	50	30	No	50
Walkway / Crosswalk Width / Length, ft	9.0	8	75	9.0	8	75	9.0	8	55	9.0	8	65
Street Width / Island / Curb	0	1	Yes	0	1	Yes	0	0	Yes	0	0	Yes
Width Outside / Bike Lane / Shoulder, ft	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0	12	0.0	0.0
Pedestrian Signal / Occupied Parking	No	0.00										

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs wo Nb...		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information													
Cycle, s	120.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	6.0	1.4	61.6	8.5	3.5	19.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	0.0	4.5	3.5	0.0	4.5			
				Red	0.0	0.0	1.5	1.0	0.0	1.5			

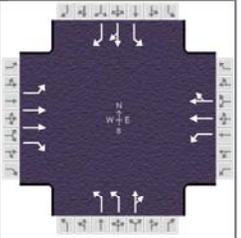
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	10.9	69.0	9.5	67.6	16.5	28.5	13.0	25.0
Change Period, (Y+R _c), s	3.5	6.0	3.5	6.0	4.5	6.0	4.5	6.0
Max Allow Headway (MAH), s	4.1	0.0	4.1	0.0	4.3	5.3	4.2	5.3
Queue Clearance Time (g _s), s	7.2		5.9		10.3	12.8	5.7	9.4
Green Extension Time (g _e), s	0.3	0.0	0.2	0.0	0.4	1.9	0.1	1.9
Phase Call Probability	1.00		0.99		1.00	1.00	0.85	1.00
Max Out Probability	0.08		0.02		0.52	0.08	0.00	0.08

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	170	1528	169	127	771	762	244	174		57	120	109
Adjusted Saturation Flow Rate (s), veh/h/ln	1757	1849	1610	1757	1845	1816	1706	1713		1757	1816	1563
Queue Service Time (g _s), s	5.2	27.1	4.1	3.9	29.4	30.8	8.3	10.8		3.7	7.0	7.4
Cycle Queue Clearance Time (g _c), s	5.2	27.1	4.1	3.9	29.4	30.8	8.3	10.8		3.7	7.0	7.4
Green Ratio (g/C)	0.60	0.57	0.71	0.58	0.55	0.55	0.28	0.20		0.08	0.17	0.17
Capacity (c), veh/h	272	2094	1140	258	1023	1007	370	350		139	318	274
Volume-to-Capacity Ratio (X)	0.626	0.730	0.148	0.491	0.753	0.757	0.661	0.498		0.408	0.378	0.397
Available Capacity (c _a), veh/h	377	2094	1140	383	1023	1007	469	403		242	374	322
Back of Queue (Q), veh/ln (95 th percentile)	3.9	12.0	2.4	2.9	14.4	15.4	6.6	8.4		3.1	5.9	5.3
Queue Storage Ratio (RQ) (95 th percentile)	0.28	0.00	0.00	0.49	0.00	0.00	0.48	0.61		0.39	0.00	0.30
Uniform Delay (d ₁), s/veh	19.2	10.7	5.7	16.9	11.9	11.6	51.4	42.3		52.6	44.6	43.9
Incremental Delay (d ₂), s/veh	2.4	2.3	0.3	1.4	5.1	5.3	2.3	1.6		1.9	1.1	1.3
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	21.5	12.9	6.0	18.4	17.0	16.9	53.7	43.9		54.5	45.6	45.2
Level of Service (LOS)	C	B	A	B	B	B	D	D		D	D	D
Approach Delay, s/veh / LOS	13.1	B		17.1	B		49.6	D		47.2	D	
Intersection Delay, s/veh / LOS	20.6						C					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS												
Bicycle LOS Score / LOS	3.1	C		2.9	C		2.3	B		2.0	B	

HCS 2010 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	Eriksson Engineering			Duration, h	0.25
Analyst	SBC	Analysis Date	Dec 21, 2015	Area Type	Other
Jurisdiction	IDOT/Wheeling	Time Period	Saturday Peak Hour	PHF	0.97
Urban Street	Dundee Road (IL 68)	Analysis Year	2026	Analysis Period	1 > 11:30
Intersection	Northgate Parkway	File Name	Northgate SAT Total Dual with IDOT Recs wo Nb...		
Project Description	IDOT Geometrics				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	165	1482	164	123	1422	65	237	97	72	55	105	167

Signal Information													
Cycle, s	120.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	6.0	1.4	61.6	8.5	3.5	19.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.5	0.0	4.5	3.5	0.0	4.5			
				Red	0.0	0.0	1.5	1.0	0.0	1.5			

Saturation Flow / Delay	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicle Adjustment Factor (f_{HV})	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	1.000	0.971	0.971	0.971
Approach Grade Adjustment Factor (f_g)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	0.952	1.000	1.000	1.000	1.000	0.971	1.000	1.000	1.000	1.000	1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.000			0.984			0.929			0.000	
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Movement Saturation Flow Rate (s), veh/h	1757	3697		1757	3501		3412	983		1757	1816	
Proportion of Vehicles Arriving on Green (P)	0.07	0.76	0.57	0.06	0.74	0.55	0.11	0.20	0.20	0.08	0.17	0.17
Incremental Delay Factor (k)	0.11	0.50	0.50	0.11	0.50	0.50	0.12	0.15		0.11	0.15	0.15

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	2.5	1.0	2.5	1.0	3.5	4.0	3.5	4.0
Green Ratio (g/C)	0.60	0.57	0.58	0.55	0.28	0.20	0.08	0.17
Permitted Saturation Flow Rate (s_p), veh/h/ln	334	0	336	0	1268	0	0	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	63.5	0.0	62.6	0.0	20.2	0.0	0.0	0.0
Permitted Service Time (g_u), s	31.8	0.0	33.9	0.0	0.0	0.0	0.0	0.0
Permitted Queue Service Time (g_{ps}), s	31.8		17.4					
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln		1610						0
Protected Right Effective Green Time (g_R), s		17.0						0.0

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v												
Pedestrian F_s / F_{delay}												
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	1049.28	13.56	1026.26	14.22	375.12	39.60	316.67	42.50				
Bicycle F_w / F_v	-2.57	1.54	-2.57	1.37	-2.57	0.69	-2.57	0.47				

--- Messages ---

WARNING: The shared-plus-exclusive turn lane solution is an approximation of the HCM method, because more than three lane groups cannot be accommodated. Input data for Percent Turns in Shared Lane are used to specify proportion of turning vehicles in the shared lane.

--- Comments ---

GREMLEY & BIEDERMANN

A DIVISION OF
PLCS Corporation
LICENSE NO. 181-005322
PROFESSIONAL LAND SURVEYORS
4505 NORTH ELSTON AVENUE, CHICAGO, IL 60630
TELEPHONE: (773) 685-5102 FAX: (773) 286-4184 EMAIL: INFO@PLCS-SURVEY.COM

ALTA / ACSM Land Title Survey

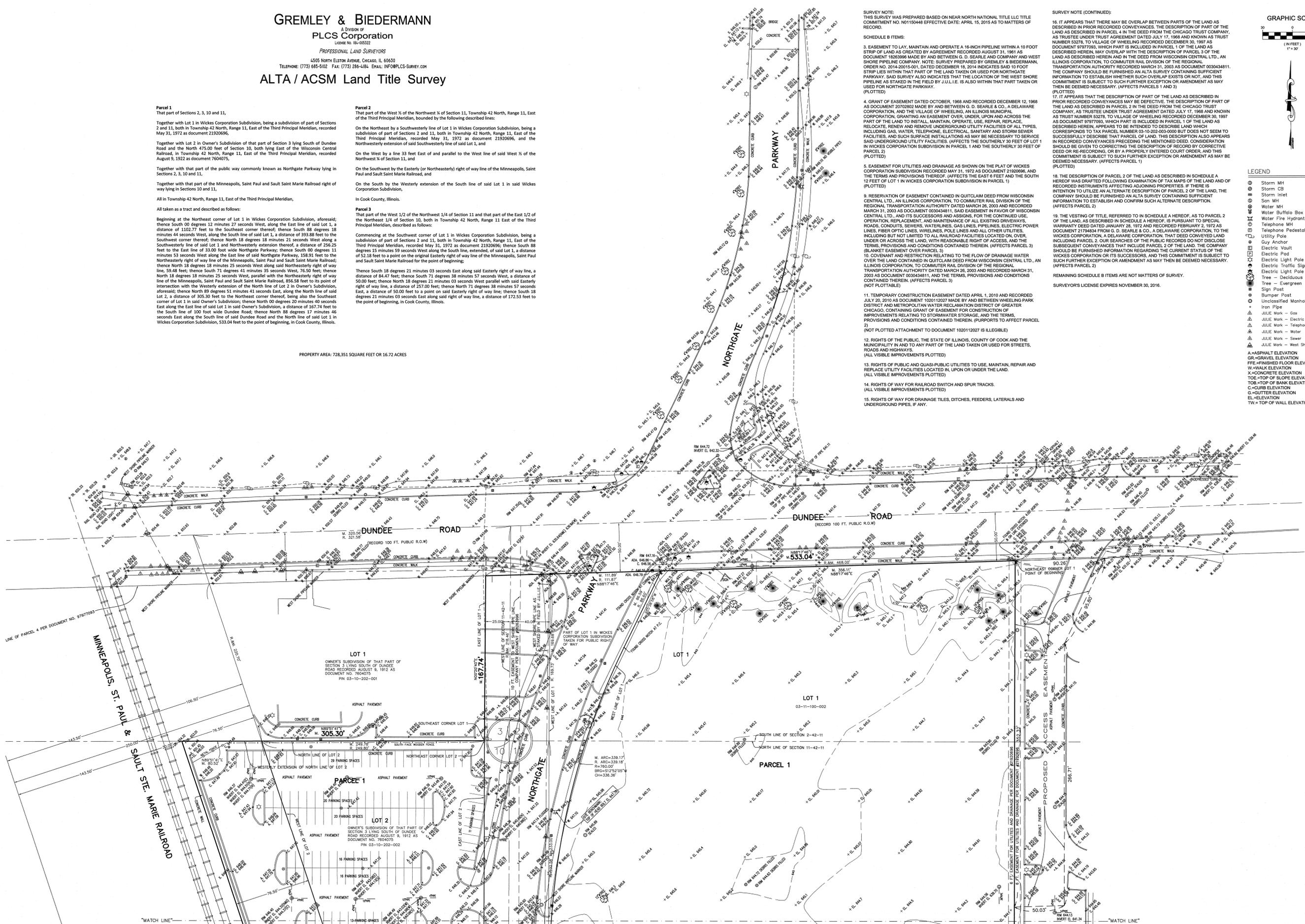
Parcel 1
That part of Sections 2, 3, 10 and 11,
Together with Lot 1 in Wickes Corporation Subdivision, being a subdivision of part of Sections 2 and 11, both in Township 42 North, Range 11, East of the Third Principal Meridian, recorded May 31, 1972 as document 21920696.
Together with Lot 2 in Owner's Subdivision of that part of Section 3 lying South of Dundee Road and the North 475.00 feet of Section 10, both lying East of the Wisconsin Central Railroad, in Township 42 North, Range 11, East of the Third Principal Meridian, recorded August 9, 1922 as document 7604075.
Together with that part of the public way commonly known as Northgate Parkway lying in Sections 2, 3, 10 and 11,
Together with that part of the Minneapolis, Saint Paul and Sault Saint Marie Railroad right of way lying in Sections 10 and 11,
All in Township 42 North, Range 11, East of the Third Principal Meridian,
All taken as a tract and described as follows:

Beginning at the Northeast corner of Lot 1 in Wickes Corporation Subdivision, aforesaid, thence South 00 degrees 12 minutes 27 seconds West, along the East line of said Lot 1, a distance of 1102.77 feet to the Southeast corner thereof; thence South 88 degrees 18 minutes 44 seconds West, along the South line of said Lot 1, a distance of 393.88 feet to the Southwest corner thereof; thence North 18 degrees 18 minutes 21 seconds West along a Southwesterly line of said Lot 1 and Northwesterly extension thereof, a distance of 256.25 feet to the East line of 33.00 foot wide Northgate Parkway; thence South 00 degrees 11 minutes 53 seconds West along the East line of said Northgate Parkway, 158.93 feet to the Northeast corner of the Minneapolis, Saint Paul and Sault Saint Marie Railroad; thence North 18 degrees 18 minutes 25 seconds West along said Northeastly right of way line, 59.48 feet; thence South 71 degrees 41 minutes 35 seconds West, 76.50 feet; thence North 18 degrees 18 minutes 25 seconds West, parallel with the Northeastly right of way line of the Minneapolis, Saint Paul and Sault Saint Marie Railroad, 856.58 feet to its point of intersection with the Westerly extension of the North line of Lot 2 in Owner's Subdivision, aforesaid; thence North 89 degrees 54 minutes 41 seconds East, along the North line of said Lot 2, a distance of 305.30 feet to the Northeast corner thereof, being also the Southeast corner of Lot 2 in said Owner's Subdivision; thence North 00 degrees 20 minutes 40 seconds East along the East line of said Lot 1 in said Owner's Subdivision, a distance of 167.74 feet to the South line of 100 foot wide Dundee Road; thence North 88 degrees 17 minutes 46 seconds East along the South line of said Dundee Road and the North line of said Lot 1 in Wickes Corporation Subdivision, 533.04 feet to the point of beginning, in Cook County, Illinois.

Parcel 2
That part of the West 1/4 of the Northwest 1/4 of Section 11, Township 42 North, Range 11, East of the Third Principal Meridian, bounded by the following described lines:
On the Northeast by a Southwesterly line of Lot 1 in Wickes Corporation Subdivision, being a subdivision of part of Sections 2 and 11, both in Township 42 North, Range 11, East of the Third Principal Meridian, recorded May 31, 1972 as document 21920696, and the Northwesterly extension of said Southwesterly line of said Lot 1, and
On the West by a line 33 feet East of and parallel to the West line of said West 1/4 of the Northwest 1/4 of Section 11, and
On the Southwest by the Easterly (or Northeastly) right of way line of the Minneapolis, Saint Paul and Sault Saint Marie Railroad, and
On the South by the Westerly extension of the South line of said Lot 1 in said Wickes Corporation Subdivision,
In Cook County, Illinois.

Parcel 3
That part of the West 1/2 of the Northwest 1/4 of Section 11 and that part of the East 1/2 of the Northeast 1/4 of Section 10, both in Township 42 North, Range 11 East of the Third Principal Meridian, described as follows:
Commencing at the Southwest corner of Lot 1 in Wickes Corporation Subdivision, being a subdivision of part of Sections 2 and 11, both in Township 42 North, Range 11, East of the Third Principal Meridian, recorded May 31, 1972 as document 21920696; thence South 18 degrees 21 minutes 03 seconds East along said Easterly right of way line, a distance of 52.18 feet to a point on the original Easterly right of way line of the Minneapolis, Saint Paul and Sault Saint Marie Railroad for the point of beginning;
Thence South 18 degrees 21 minutes 03 seconds East along said Easterly right of way line, a distance of 84.47 feet; thence South 71 degrees 38 minutes 57 seconds West, a distance of 50.00 feet; thence North 28 degrees 21 minutes 03 seconds West parallel with said Easterly right of way line, a distance of 257.00 feet; thence North 71 degrees 38 minutes 57 seconds East, a distance of 50.00 feet to a point on said Easterly right of way line; thence South 18 degrees 21 minutes 03 seconds East along said right of way line, a distance of 172.53 feet to the point of beginning, in Cook County, Illinois.

PROPERTY AREA: 728,351 SQUARE FEET OR 16.72 ACRES



SURVEY NOTE:
THIS SURVEY WAS PREPARED BASED ON NEAR NORTH NATIONAL TITLE LLC TITLE COMMITMENT NO. M0116044 EFFECTIVE DATE: APRIL 15, 2015 AS TO MATTERS OF RECORD.

SCHEDULE B ITEMS:

3. EASEMENT TO LAY, MAINTAIN AND OPERATE A 16-INCH PIPELINE WITHIN A 10 FOOT STRIP OF LAND AS CREATED BY AGREEMENT RECORDED AUGUST 31, 1981 AS DOCUMENT 1626998 MADE BY AND BETWEEN G. D. SEARLE AND COMPANY AND WEST SHORE PIPELINE COMPANY. NOTE: SURVEY PREPARED BY GREMLEY & BIEDERMANN, ORDER NO. 2014-20015-001, DATED DECEMBER 18, 2014 INDICATES SAID 10 FOOT STRIP LIES WITHIN THAT PART OF THE LAND TAKEN OR USED FOR NORTHGATE PARKWAY. SAID SURVEY ALSO INDICATES THAT THE LOCATION OF THE WEST SHORE PIPELINE AS STAKED IN THE FIELD BY J.U.L.I.E. IS ALSO WITHIN THAT PART TAKEN OR USED FOR NORTHGATE PARKWAY. (PLOTTED)

4. GRANT OF EASEMENT DATED OCTOBER, 1988 AND RECORDED DECEMBER 12, 1988 AS DOCUMENT 2070280 MADE BY AND BETWEEN G. D. SEARLE & CO., A DELAWARE CORPORATION, AND THE VILLAGE OF WHEELING RECORDED DECEMBER 30, 1997 AS DOCUMENT 9797798, WHICH PART IS INCLUDED IN PARCEL 1 OF THE LAND AS DESCRIBED HEREIN, MAY OVERLAP WITH THE DESCRIPTION OF PARCEL 3 OF THE LAND AS DESCRIBED HEREIN AND IN THE DEED FROM WISCONSIN CENTRAL LTD., AN ILLINOIS CORPORATION, TO COMMUTER RAIL DIVISION OF THE REGIONAL TRANSPORTATION AUTHORITY RECORDED MARCH 31, 2003 AS DOCUMENT 0030434811. THE COMPANY SHOULD BE FURNISHED AN ALTA SURVEY CONTAINING SUFFICIENT INFORMATION TO ESTABLISH WHETHER SUCH OVERLAP EXISTS OR NOT, AND THIS COMMITMENT IS SUBJECT TO SUCH FURTHER EXCEPTION OR AMENDMENT AS MAY THEN BE DEEMED NECESSARY. (AFFECTS PARCELS 1 AND 3) (PLOTTED)

17. IT APPEARS THAT THE DESCRIPTION OF PART OF THE LAND AS DESCRIBED IN PRIOR RECORDED CONVEYANCES MAY BE DEFECTIVE. THE DESCRIPTION OF PART OF THE LAND AS DESCRIBED IN PARCEL 2 IN THE DEED FROM THE CHICAGO TRUST COMPANY, AS TRUSTEE UNDER TRUST AGREEMENT DATED JULY 17, 1968 AND KNOWN AS TRUST NUMBER 53278, TO VILLAGE OF WHEELING RECORDED DECEMBER 30, 1997 AS DOCUMENT 9797798, WHICH PART IS INCLUDED IN PARCEL 1 OF THE LAND AS DESCRIBED HEREIN, APPEARS TO BE INTENDED TO DESCRIBE LAND WHICH CORRESPONDS TO TAX PARCEL NUMBER 03-10-202-003-0000 BUT DOES NOT SEEM TO SUCCESSFULLY DESCRIBE THAT PARCEL OF LAND. THIS DESCRIPTION ALSO APPEARS IN RECORDED CONVEYANCES PRECEDING THE MENTIONED DEED. CONSIDERATION SHOULD BE GIVEN TO CORRECTING THE DESCRIPTION OF RECORD BY CORRECTIVE DEED OR RE-RECORDING, OR BY A PROPERLY ENTERED COURT ORDER, AND THIS COMMITMENT IS SUBJECT TO SUCH FURTHER EXCEPTION OR AMENDMENT AS MAY THEN BE DEEMED NECESSARY. (AFFECTS PARCEL 1) (PLOTTED)

18. THE DESCRIPTION OF PARCEL 2 OF THE LAND AS DESCRIBED IN SCHEDULE A HEREOF WAS DRAFTED FOLLOWING EXAMINATION OF TAX MAPS OF THE LAND AND OF RECORDED INSTRUMENTS AFFECTING ADJOINING PROPERTIES. IF THERE IS INTENTION TO UTILIZE AN ALTERNATE DESCRIPTION OF PARCEL 2 OF THE LAND, THE COMPANY SHOULD BE FURNISHED AN ALTA SURVEY CONTAINING SUFFICIENT INFORMATION TO ESTABLISH AND CONFIRM SUCH ALTERNATE DESCRIPTION. (AFFECTS PARCEL 2)

19. THE VESTING OF TITLE, REFERRED TO IN SCHEDULE A HEREOF, AS TO PARCEL 2 OF THE LAND AS DESCRIBED IN SCHEDULE A HEREOF, IS PURSUANT TO SPECIAL WARRANTY DEED DATED JANUARY 28, 1972 AND RECORDED FEBRUARY 2, 1972 AS DOCUMENT 2179484 FROM G. D. SEARLE & CO., A DELAWARE CORPORATION, TO THE WICKES CORPORATION, A DELAWARE CORPORATION, THAT DEED COVERED LAND INCLUDING PARCEL 2. OUR SEARCHES OF THE PUBLIC RECORDS DO NOT DISCLOSE SUBSEQUENT CONVEYANCES THAT INCLUDE PARCEL 2 OF THE LAND. THE COMPANY SHOULD BE FURNISHED INFORMATION REGARDING THE CURRENT STATUS OF THE WICKES CORPORATION OR ITS SUCCESSORS, AND THIS COMMITMENT IS SUBJECT TO SUCH FURTHER EXCEPTION OR AMENDMENT AS MAY THEN BE DEEMED NECESSARY. (AFFECTS PARCEL 2)

REMAINING SCHEDULE B ITEMS ARE NOT MATTERS OF SURVEY.
SURVEYOR'S LICENSE EXPIRES NOVEMBER 30, 2016.

11. TEMPORARY CONSTRUCTION EASEMENT DATED APRIL 1, 2010 AND RECORDED JULY 29, 2010 AS DOCUMENT 1020112027 MADE BY AND BETWEEN WHEELING PARK DISTRICT AND METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO, CONTAINING GRANT OF EASEMENT FOR CONSTRUCTION OF IMPROVEMENTS RELATING TO STORMWATER STORAGE, AND THE TERMS, PROVISIONS AND CONDITIONS CONTAINED THEREIN, (PURPORTS TO AFFECT PARCEL 2) (NOT PLOTTED ATTACHMENT TO DOCUMENT 1020112027 IS ILLEGIBLE)

12. RIGHTS OF THE PUBLIC, THE STATE OF ILLINOIS, COUNTY OF COOK AND THE MUNICIPALITY IN AND TO ANY PART OF THE LAND TAKEN OR USED FOR STREETS, ROADS AND HIGHWAYS. (ALL VISIBLE IMPROVEMENTS PLOTTED)

13. RIGHTS OF PUBLIC AND QUASI-PUBLIC UTILITIES TO USE, MAINTAIN, REPAIR AND REPLACE UTILITY FACILITIES LOCATED IN, UPON OR UNDER THE LAND. (ALL VISIBLE IMPROVEMENTS PLOTTED)

14. RIGHTS OF WAY FOR RAILROAD SWITCH AND SPUR TRACKS. (ALL VISIBLE IMPROVEMENTS PLOTTED)

15. RIGHTS OF WAY FOR DRAINAGE TRENCHES, DITCHES, FEEDERS, LATERALS AND UNDERGROUND PIPES, IF ANY.

GRAPHIC SCALE
0 10 20
(IN FEET)
1" = 30'

LEGEND

- Storm Flood
- Storm CB
- Storm Inlet
- Son MH
- Water MH
- Water Buffalo Box
- Water Fire Hydrant
- Telephone MH
- Telephone Pedestal
- Utility Pole
- Guy Anchor
- Electric Vault
- Electric Pad
- Electric Light Pole
- Electric Light Pole with Traffic Signal
- Tree - Deciduous
- Tree - Evergreen
- Sign Post
- Bumper Post
- Unclassified Manhole
- Iron Pipe
- JULIE Mark - Gas
- JULIE Mark - Electric
- JULIE Mark - Telephone
- JULIE Mark - Water
- JULIE Mark - Sewer
- JULIE Mark - West Shore Pipeline
- A=ASPHALT ELEVATION
- GR=GRAVEL ELEVATION
- FR=FINISHED FLOOR ELEVATION
- W=WALK ELEVATION
- X=CONCRETE ELEVATION
- TOE=TOP OF SLOPE ELEVATION
- TOP=TOP OF BANK ELEVATION
- C=CURB ELEVATION
- G=GUTTER ELEVATION
- EL=ELEVATION
- TM=TOP OF WALL ELEVATION

REVISED: 5/18/2015 PER ORDER NO. 2015-20840 [RJT]
REVISED: 12/12/2014 ADDITIONAL WORK PER ORDER #2014-20259 [RJT]

ORDERED BY: WTE LLC
ADDRESS: WHEELING TOWN CENTER
DRAWN BY: GREMLEY & BIEDERMANN
DATE: OCTOBER 29, 2014
SCALE: 1" = 30 FEET
PAGE: 1 OF 2

SURVEY NOTES:
Surveyor's license expires November 30, 2016.
Note: R. & M. denotes Record and Measured distances respectively.
Distances are marked in feet and decimal parts thereof. Compare all points BEFORE building by same and at once report any differences BEFORE commencing to build.
For easements, building lines and other restrictions shown on survey plat refer to your abstract, deed, contract, title plat and local building regulations.
NO dimensions shall be assumed by scale measurement upon this plat.
Unless otherwise noted herein the Bearing, Backs, Elevation Datum and Coordinate Datum if used is ASSUMED.
COPYRIGHT GREMLEY & BIEDERMANN, INC. 2014 "All Rights Reserved"

SURVEY NOTES:
UTILITY WARNING
The underground utilities shown have been located from field survey information and existing drawings. The surveyor makes NO guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated although he does certify that they are located as accurately as possible from information available. This surveyor has no physical location of the underground utilities.
Call DIGGER - (312) 744-7000 within the City of Chicago.
Outside of the City of Chicago call J.U.L.I.E. (800) 892-0123 prior to construction or excavation.

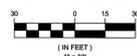
SUBTRACT 0.31 FEET TO CONVERT ELEVATIONS DEPICTED ON THIS PLAT TO NORTH AMERICAN VERTICAL DATUM OF 1988 (NAV88). THIS CONVERSION FACTOR WAS ESTABLISHED USING THE FOLLOWING BENCHMARK:
BENCHMARK DESIGNATION: 06 NO 3301
BENCHMARK P.I.D.: N601229
ELEVATION: 639.73 FEET (NAV88)
LOCATION: 0.6 MI W FROM WILMETTE, 0.55 MILES WEST ALONG LAKE AVENUE FROM THE VILLAGE MALL ANNEX, AT THE INTERSECTION OF LAKE AVENUE AND RIDGE ROAD, SET IN THE NORTH LAIN OF ST. JOSEPH CHURCH, 82.2 FEET EAST OF THE EAST CURB OF RIDGE ROAD, 73.3 FEET NORTH OF THE CENTER LINE OF THE NORTHWEST MAIN ENTRANCE TO THE CHURCH 23.9 FEET SOUTH OF THE SOUTH CURB OF LAKE AVENUE. A COOK COUNTY HIGHWAY DEPARTMENT DISK IN THE TOP OF A CONCRETE POST FLUSH WITH THE GROUND AND ABOUT LEVEL WITH THE STREET.

ELEVATIONS DEPICTED ON THIS PLAT ARE PER NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29) USING THE FOLLOWING BENCHMARK:
BENCHMARK # RM 68-2
ELEVATION = 647.210 FEET (NGVD29)
LOCATION: CHISELED SQUARE ON TOP OF RIGHT DOWNSTREAM WING WALL OF WHEELING DRAINAGE DITCH AND STATE HIGHWAY 68 (DUNDEE ROAD).

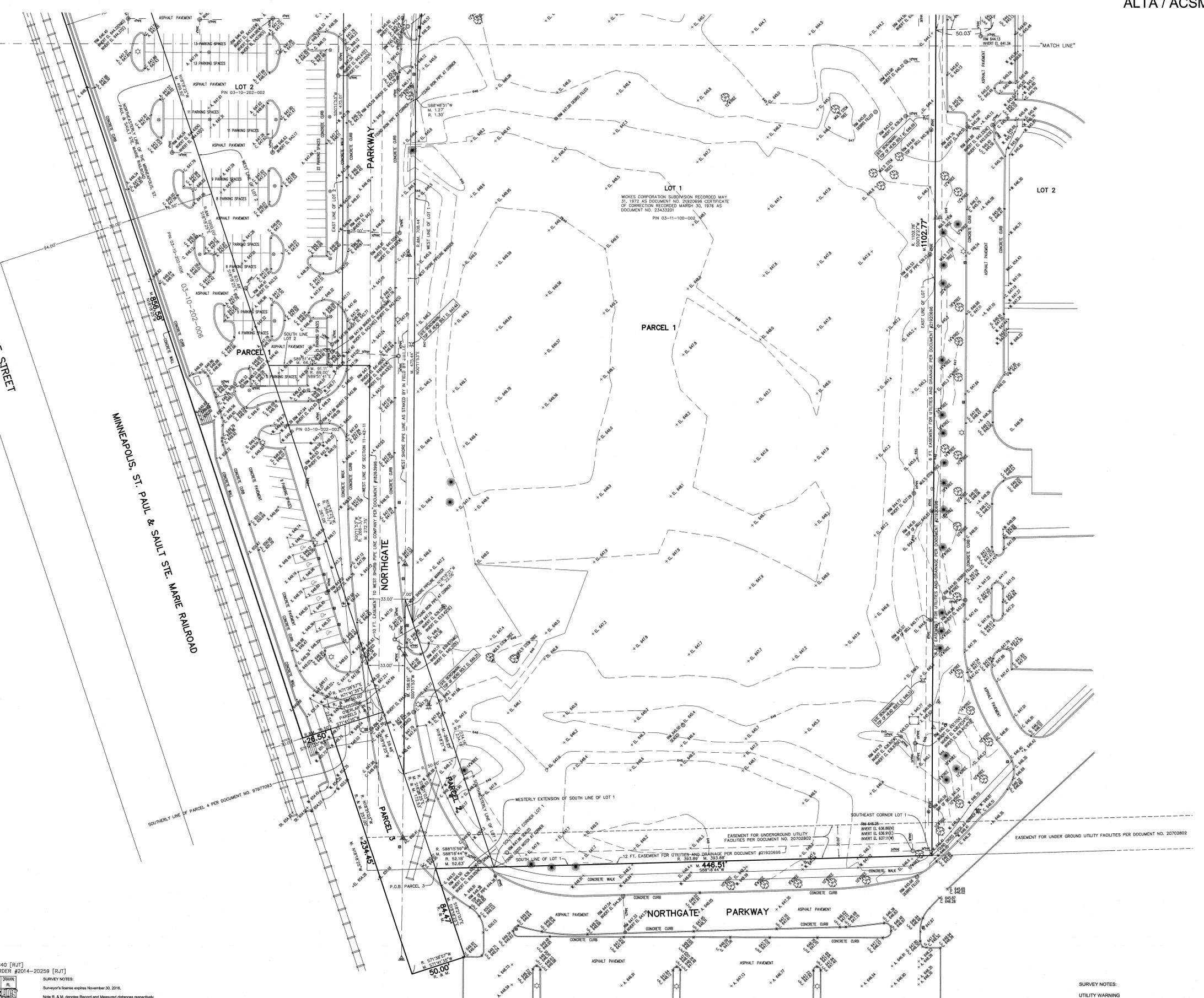
SURVEY NOTE:
THIS SURVEY WAS PREPARED WITHOUT THE BENEFIT OF, AND IS SUBJECT TO, ALL RESTRICTIONS AND MATTERS APPEARING OF RECORD AS MIGHT BE DISCLOSED BY A CURRENT TITLE COMMITMENT.
PROPERTY APPEARS IN "OTHER AREAS" ZONE X, AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, PER FLOOR INSURANCE RATE MAP COOK COUNTY, ILLINOIS, MAP NO. 170310 0068J, EFFECTIVE DATE AUGUST 19, 2008.
REGARDING TABLE A ITEM 21 PROFESSIONAL LIABILITY INSURANCE POLICY OBTAINED BY THE SURVEYOR IN THE MINIMUM AMOUNT OF \$2,000,000 TO BE IN EFFECT THROUGHOUT THE CONTRACT TERM. CERTIFICATE OF INSURANCE TO BE FURNISHED UPON REQUEST.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 8, 9, 11(a) and 21 of Table A thereto.
The field work was completed on December 12, 2014.
Date of Plat: May 19, 2015
By: Robert G. Biedermann
Professional Illinois Land Surveyor No. 2802





- LEGEND**
- Storm MH
 - Storm CB
 - Storm Inlet
 - Son MH
 - Water MH
 - Water Buffalo Box
 - Water Fire Hydrant
 - Telephone MH
 - Telephone Pedestal
 - Utility Pole
 - Coy Anchor
 - Electric Vault
 - Electric Pad
 - Electric Light Pole
 - Electric Traffic Signal
 - Electric Light Pole with Traffic Sign
 - Tree - Deciduous
 - Tree - Evergreen
 - Sign Post
 - Bumper Post
 - Unclassified Manhole
 - Iron Pipe
 - JULIE Mark - Gas
 - JULIE Mark - Electric
 - JULIE Mark - Telephone
 - JULIE Mark - Water
 - JULIE Mark - Sewer
 - JULIE Mark - West Shore Pipeline
 - A=ASPHALT ELEVATION
 - GR=GRAVEL ELEVATION
 - FFE=FINISHED FLOOR ELEVATION
 - W=WALK ELEVATION
 - X=CONCRETE ELEVATION
 - TOE=TOP OF SLOPE ELEVATION
 - TOSB=TOP OF BANK ELEVATION
 - C=CURB ELEVATION
 - G=GUTTER ELEVATION
 - EL=ELEVATION
 - TW=TOP OF WALL ELEVATION



REVISED: 5/18/2015 PER ORDER NO. 2015-20840 [RJT]
 REVISED: 12/12/2014 ADDITIONAL WORK PER ORDER #2014-20259 [RJT]

ORDERED BY: WTC LLC	CHECKED: [Signature]
ADDRESS: WHEELING TOWN CENTER	
GREMLEY & BIEDERMANN	
PLCS CORPORATION	
4505 NORTH ELSTON AVENUE, CHICAGO, IL 60630	
TELEPHONE: (773) 685-5102 FAX: (773) 286-4184 EMAIL: INFO@PLCS-SURVEY.COM	
DATE: OCTOBER 29, 2014	PAGE NO. 2 OF 2

SURVEY NOTES:

Surveyor's license expires November 30, 2016.

Note R & M denotes Record and Measured distances respectively.

Distances are marked in feet and decimal parts thereof. Compare all points BEFORE building by same and at once report any differences BEFORE damage is done.

For easements, building lines and other restrictions not shown on survey plat refer to your abstract, deed, contract, the policy and local building line regulations.

NO dimensions shall be assumed by scale measurement upon this plat.

Unless otherwise noted herein the Bearing, Elevation Datum and Coordinate Datum if used is ASSUMED.

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SURVEY NOTES:

UTILITY WARNING

The underground utilities shown have been located from field survey information and existing drawings. The surveyor makes NO guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated although he does certify that they are located as accurately as possible from information available. The surveyor has not physically located the underground utilities.

Call DIGGER - (312) 744-7000 within the City of Chicago.

Outside of the City of Chicago call J.U.L.I.E. (800) 892-0123 prior to construction or excavation.

Dundee Road Looking Southwest



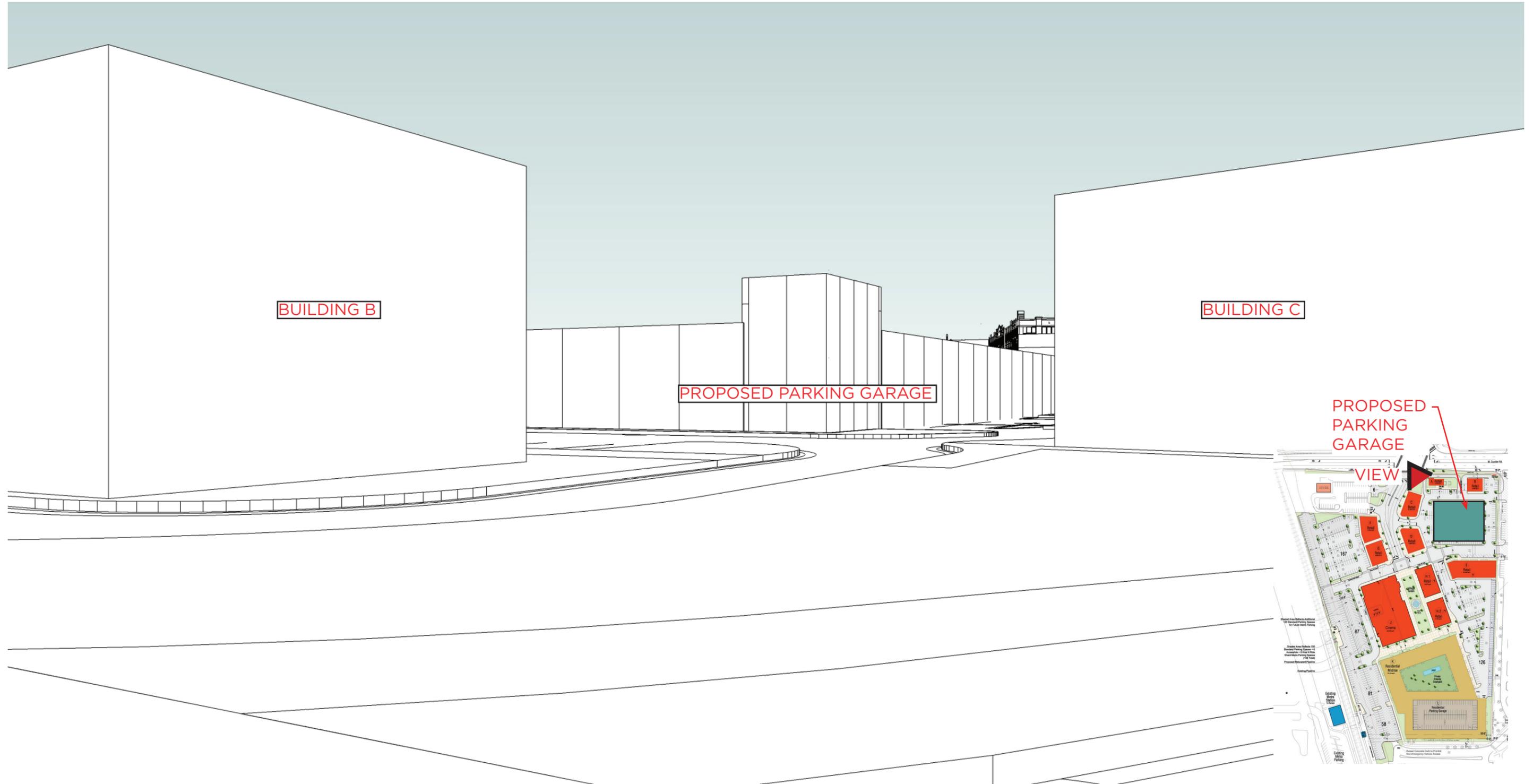
Dundee Road Looking Southwest



Main Entrance Looking Southeast



Main Entrance Looking Southeast



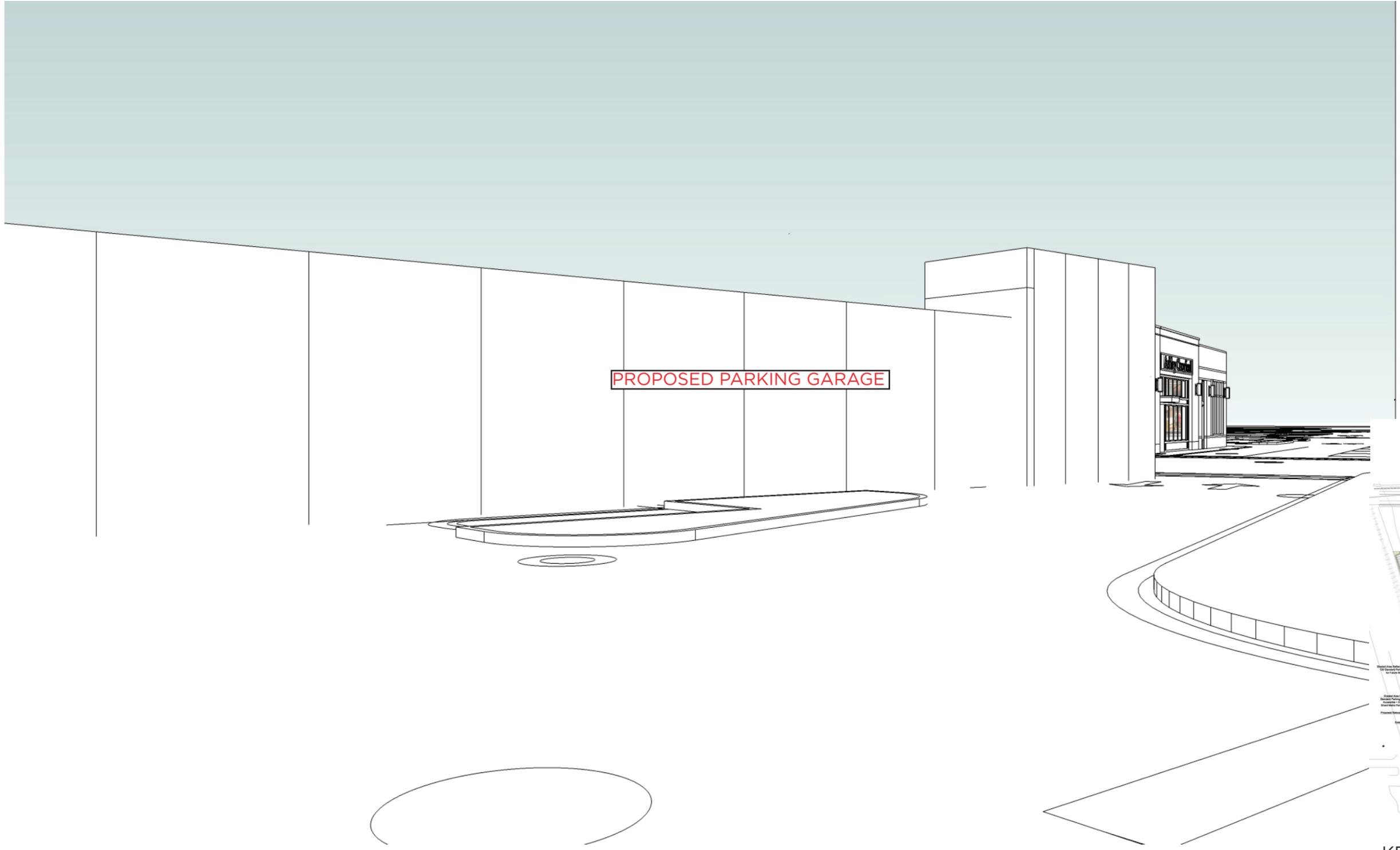
East Parking Lot Entrance Looking South-East



BUILDING D



KEY PLAN



BUILDING D

PROPOSED PARKING GARAGE



PROPOSED
PARKING
GARAGE

VIEW

KEY PLAN



FROM 700FT.

ELEVATION VIEW

Scale - NTS

DESCRIPTION

Visual Representation of Signage

Approved:	 2135 S. Frontage Rd. Des Plaines, IL 60018 847-390-0350 - Fax 847-390-9231 sgareason@designgroupsignage.com	Job # 5758 WTC Development	Names:	Date:	Notes:
			SG	December 29, 2015	Initial Sign Drawing
Drawn By: SG Checked By:	These plans are exclusive property of DeSignGroup Signage Corp. and are the result of the original work of its employees. They are submitted to your company for the sole purpose of your consideration of whether to purchase these plans or to purchase from DeSignGroup Signage Corp. a sign manufactured according to these plans. Discussion or exhibition of these plans to construct a sign similar to the one herein is strictly forbidden. In the event that such exhibition occurs, DeSignGroup Signage Corp. expects to be reimbursed \$500.00 in compensation for time and effort entailed in creating these plans.	Page 4 of 4			





FROM 500FT.

ELEVATION VIEW

Scale - NTS

DESCRIPTION

Visual Representation of Signage

Approved:	 2135 S. Frontage Rd. Des Plaines, IL 60018 847-390-0350 - Fax 847-390-9231 sg@relson@designgroupsignage.com	Job # 5758 WTC Development	Names:	Date:	Notes:
			SG	December 29, 2015	Initial Sign Drawing
Drawn By: SG Checked By:	These plans are exclusive property of DeSignGroup Signage Corp. and are the result of the original work of its employees. They are submitted to your company for the sole purpose of your consideration of whether to purchase these plans or to purchase from DeSignGroup Signage Corp. a sign manufactured according to these plans. Discussion or exhibition of these plans to construct a sign similar to the one herein is strictly forbidden. In the event that such exhibition occurs, DeSignGroup Signage Corp. expects to be reimbursed \$500.00 in compensation for time and effort entailed in creating these plans.	Page 3 of 4			





FROM 400FT.

ELEVATION VIEW

Scale - 1" = 1'

DESCRIPTION

Visual Representation of Signage

Approved:		2135 S. Frontage Rd. Des Plaines, IL 60018 847-390-0350 - Fax 847-390-9231 sgareason@designgroupsignage.com	Job # 5758 WTC Development	Names:	Date:	Notes:
				SG	December 29, 2015	Initial Sign Drawing
Drawn By: SG Checked By:	These plans are exclusive property of DeSignGroup Signage Corp. and are the result of the original work of its employees. They are submitted to your company for the sole purpose of your consideration of whether to purchase these plans or to purchase from DeSignGroup Signage Corp. a sign manufactured according to these plans. Discussion or exhibition of these plans to construct a sign similar to the one herein is strictly forbidden. In the event that such exhibition occurs, DeSignGroup Signage Corp. expects to be reimbursed \$500.00 in compensation for time and effort entailed in creating these plans.			Drawn By: SG	Checked By:	
				Page 2 of 4		





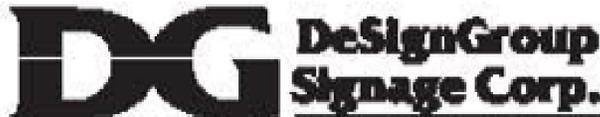
FROM 300FT.

ELEVATION VIEW

Scale - NTS

DESCRIPTION

Visual Representation of Signage

Approved:	 <p>2135 S. Frontage Rd. Deer Park, IL 60018 847-390-0350 - Fax 847-390-9231 signage@designgroup.com</p>	Job # 5758 WTC Development	Name:	Date:	Notes:
			SG	December 29, 2015	Initial Sign Drawing
Drawn By: SG Checked By:	<p>These plans are exclusive property of DeSignGroup Signage Corp. and are the result of the original work of its employees. They are submitted to your company for the sole purpose of your consideration of whether to purchase these plans or to purchase from DeSignGroup Signage Corp. a sign manufactured according to these plans. Distribution or exhibition of these plans to construct a sign similar to the one herein is strictly forbidden. In the event that such exhibition occurs, DeSignGroup Signage Corp. reserves the right to be reimbursed \$500.00 in compensation for time and effort entailed in creating these plans.</p>	Page 1 of 4			

SCHEDULE A
TITLE 19, CHAPTER 9
USE AND DESIGN STANDARDS FOR PLANNED UNIT DEVELOPMENTS

(A) Uses. Permitted, special and accessory uses for each PUD shall be specified in the preliminary PUD application. Residential use may be proposed for any planned unit development. Nonresidential uses shall be consistent with the uses permitted in the underlying zoning district (see Appendix A, Use Table). With the exception of planned unit developments in an R-4 district, a mix of different uses within a PUD may be permitted if the plan commission and the board of trustees determine that the mix of uses is compatible and necessary to achieve the objectives of the PUD. Any additions or exceptions to the specified uses in those districts shall be specified at the time of preliminary PUD review and approval.

Response: The proposed development will be consistent with the uses permitted in the underlying Mixed Use (B-3 and R-4) zoning districts.

(B) Number of Buildings on a Lot. The PUD may allow more than one building on a lot.

Response: Acknowledged.

(C) Density. The PUD may permit the grouping of dwelling units in one or more locations within the total site, however residential density for the site as a whole shall be consistent with that specified in the village's comprehensive plan.

Response: The proposed residential density is consistent with the Village's comprehensive plan, which contemplates transit-oriented, mixed-use development in the Dundee Road Subarea.

(D) Minimum Area. The minimum lot area for a PUD is one acre, which may consist of one or more contiguous parcels.

Response: The proposed development site area is approximately 16.25 acres.

(E) Space Between Structures. Spaces between structures shall not be less than required by the building code.

Response: The structure spacing will comply with the building code.

(F) Setbacks. The dimension of setbacks and limitations on their use in each PUD project shall be based on the regulations for the underlying zoning district. Variations to zoning regulations are often required to facilitate the construction of a planned development and will be established during the development review and approval process. When establishing the appropriate setbacks, explicit consideration will be given to existing conditions and proposed building heights. The plan commission may recommend and the village board may require that setback areas be landscaped and used only for recreation, utility rights-of-way, sidewalks, ponds, water detention basins and drainage channels. It may also be specified that setback areas are not to be used for parking or driveways except as they are used to provide direct access to the development.

Response: The proposed development will comply with the underlying zoning district, except with respect to those items for which the Applicant is seeking relief as indicated in the enclosed Form A4.

(G) Circulation, Parking and Loading. Adequate pedestrian and vehicular access shall be provided into and throughout the development. Parking and loading facilities shall be located near the uses they support and shall be adequately screened and landscaped in a manner which meets or exceeds the requirements of this Title. Private streets are not permitted.

Response: The proposed development will be served by one full-access curb cut and one restricted curb cut on West Dundee Road to the north. Additional access points are provided along the east and south boundaries of the site, providing sufficient access to the development.

Parking is provided both in an enclosed 6-story parking garage and in surface parking lots, all of which are conveniently located throughout the site to serve the various users they support. The Parking Study prepared by Eriksson Engineering Associates, Ltd. confirms that the proposed parking will be sufficient to support the proposed uses on the site.

Additionally, carefully planned sidewalks run throughout the development, creating a pedestrian-friendly environment for residents and visitors to the site.

(H) Landscaping. All required vegetation shall be of a quality consistent with the standards of the American Association of Nurserymen (ANSI 260.1). All required vegetation shall be maintained on an ongoing basis, including seasonal tree and plant replacement.

Response: Acknowledged.

(I) Lighting. See Section 19.11.040 of this Title for lighting regulations.

Response: The development will comply with the Village's lighting regulations.

(J) Utilities and Mechanicals.

(i) Mechanical, electrical, communications and service equipment, including meters, shall be located inside the building whenever possible. Any roof- or wall-mounted equipment, including piping, shall be screened on all sides from public view by parapets, walls, or other approved means. Utility meters, gas regulator valves and the like shall not be placed on the street side of any building.

(ii) All ground level mechanical, electrical and transformers, communications and service equipment shall be screened with either plantings or a durable noncombustible enclosure which are unified and harmonious with the overall architectural theme of the building while meeting utility provider standards for location and maintenance.

(iii) All utility lines shall be installed underground. Transformers and substations shall be installed within buildings or otherwise screened from view.

Response: The development will comply with the Village's requirements related to the location and screening of mechanical and utility lines and equipment.

(K) Signs. All signs in the PUD shall be included as part of the PUD application and are subject to the provisions of Title 21

Response: Please see the enclosed signage package.

(L) Establishment of an Owners' Association. Should a PUD include multi-family residential property where property will be held in common ownership, the petitioner shall indicate the manner in which a single owners' association will be established. This shall include, but is not limited to, the proposed declaration of covenants and restrictions, articles of incorporation, and association by-laws.

Response: The development will be governed by all necessary and appropriate reciprocal easements and on-going maintenance/association agreements. The documentation will be provided at a later date.

Wheeling Town Center
Zoning Variation – Landscaping & Tree Replacement
Variation Standards

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: The subject property is a large and significant parcel in the Village which the Applicant seeks to develop in a comprehensive and integrated manner that is consistent with the Village Comprehensive Plan. As such, relief is needed from the strict letter of the Village Code with respect to landscaping, including the landscaping requirements for the parking lots and buildings, in order to accommodate a thoughtful and convenient parking layout and allow for a pedestrian-friendly development..

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The conditions giving rise to the requested variation include the applicant's and the Village's desire to accommodate an integrated, pedestrian-friendly, mixed-use development. This is a unique opportunity, which exists due to the size, layout and location of the subject property. These conditions are not recurrent elsewhere in the district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. The conditions arise as a result of the desire to develop the property in a manner consistent with the Village Comprehensive Plan. As such, the difficulty or hardship were not caused by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: If the property were only permitted to be used as permitted under the zoning ordinance, the proposed integrated development would not be permitted. This is not a reasonable or desirable use of the subject property, which presents a unique development opportunity for the Village.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: The proposed development will maintain a significant amount of landscaping, including a Village Green area in the middle of the development that fosters a sense of

community. Therefore, granting the variation will have a positive impact on – but will not alter – the essential character of the locality.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land.

Wheeling Town Center
Zoning Variation – Minimum Floor Area
Variation Standards

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: The proposed development intends to provide a variety of unit types and options for future residents, including efficiency units that are slightly smaller than the minimum floor area required for efficiency units in the R4 District. The smaller floor area is needed in order to achieve the desired floor plans and building footprint that works within the overall proposed development.

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The conditions giving rise to the requested variation include the proximity to the Metra train, size and overall location of the subject property. This presents the opportunity to build a transit-oriented development on the site, which often entails smaller floor plans and higher density. Further, the building has been designed to fit within the development as a whole, which is unique to this property and not likely to be duplicated elsewhere in the district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. The conditions arise as a result of the desire to develop the property in a manner consistent with the Village Comprehensive Plan. As such, the difficulty or hardship were not caused by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: If the efficiency units were made bigger, the property could not be developed with the mixed-use development that offers a variety of residential housing types consistent with the Comprehensive Plan.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: Granting the variation will have no impact on the essential character of the locality.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land.

Wheeling Town Center
Zoning Variation – Open Space
Variation Standards

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: The subject property is a large and significant parcel in the Village, which the Applicant seeks to develop in a comprehensive and integrated manner that is consistent with the Village Comprehensive Plan. As such, relief is needed from the strict letter of the Village Code with respect to open space in order to allow for an integrated mixed-use development on the property that continues to provide parking for Metra commuters, which is not possible under conventional zoning.

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The subject property is unique in the Village in its size, layout, and proximity to the Metra and other Village services. These conditions do not exist in other properties in the Village or zoning district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. As such, the difficulty or hardship were not caused by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: If the property were only permitted to be used as permitted under the zoning ordinance, the proposed integrated development would not be permitted. This is not a reasonable or desirable use of the subject property, which presents a unique development opportunity for the Village.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: Granting the variation will allow for the proposed development, which will bring life and activity to this area of Wheeling and be an amenity to the Village, its residents and visitors. This includes providing unique retail opportunities, a “Village Green” for community gatherings and events, and providing a housing option that is underrepresented in the area. As

such, granting the variation will benefit the locality as opposed to altering its essential character.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land.

Wheeling Town Center
Zoning Variation – Parking Stall Size
Variation Standards

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: The Fire Department requires additional width for certain drive aisles to ensure adequate access to the southernmost portion of the property due to the property's size and configuration. Further, the relocated gas pipeline along the western boundary of the property requires a wider drive aisle in that portion of the property. Therefore, the 1.5' parking stall size variation is required in order to accommodate these additional drive aisle width requirements while maintaining an appropriate amount of parking for the development.

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The conditions giving rise to the requested variation include the size and configuration of the property and the presence of the gas pipeline. The conditions are unique to this site and unlikely to recur elsewhere in the zoning district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. The conditions arise as a result of physical conditions related to the property. As such, the difficulty or hardship were not caused by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: If required to provide the additional 1.5' for the parking stalls, the site would need to be reconfigured and a significant amount of parking would be eliminated. This would reduce the feasibility of developing a transit-oriented, mixed-use development on the property consistent with the Comprehensive Plan.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: Granting the variation will have no impact on the essential character of the locality. The additional 1.5' will be accommodated by means of vehicle overhang space over the adjacent landscaped islands. Therefore, the requested variation will have no impact on the property or its users.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land.

**Wheeling Town Center
Zoning Variation – Parking
Variation Standards**

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: The proposed development utilizes high-quality architecture and site planning for a mixed-use development that incorporates a variety of complementary uses. Adding parking would require increased height in the residential building and more surface parking throughout the site, which would compromise the site plan and undermine the transit-oriented nature of the development that the Applicant is trying to achieve. Further, as the shared parking study demonstrates, the parking needs of both the commercial and residential components of the proposed development are well-served by the proposed parking.

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The conditions giving rise to the requested variation include the proximity to the Metra train, size and overall location of the subject property. This presents the opportunity to build a transit-oriented development on the site, which by definition require less parking. The conditions are unique to this site and unlikely to recur elsewhere in the zoning district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. The conditions arise as a result of the desire to develop the property in a manner consistent with the Village Comprehensive Plan and best practices for transit-oriented developments. As such, the difficulty or hardship were not caused by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: If required to provide parking as required by the Village Code, the property could not yield a reasonable return while being developed with a transit-oriented, mixed-use development consistent with the Comprehensive Plan.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: Granting the variation will have no impact on the essential character of the locality.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land. Further, reduced parking will encourage alternative modes of transportation and decrease the traffic impact of the proposed development on the surrounding area.

Wheeling Town Center
Zoning Variation – Sign Location
Variation Standards

1. Physical Conditions vs. Convenience. That there are present actual physical conditions applying to the lot, parcel, building, structure, use or intended use on that premises which are creating the practical difficulty or unnecessary hardship in the application of this Title, as distinguished from a mere inconvenience to the owner if the strict letter of the regulations are required.

Response: This subject property is unique with respect to its size, location, irregular shape and limited frontage along the Dundee Road right-of-way relative to its overall depth. The proposed development intends to utilize high-quality and creative site design with ample landscaping and open space while providing visual interest along the public right-of-way.

2. Unique vs. General Conditions. That the conditions are unique, exceptional, extraordinary or unusual circumstances applying only or primarily to the property under appeal and are not of such a general or recurrent nature elsewhere in the same zoning district as to suggest or establish the basis for future variations, ordinance changes or amendments.

Response: The conditions giving rise to the requested variation include the size and configuration of the subject property, including its relatively limited frontage along Dundee Road. These conditions are unique to this site and unlikely to be recurrent elsewhere in the zoning district.

3. Conditions Not Created by Appellant. That the alleged conditions creating the difficulty or hardship were not caused by the appellant nor by any person still having an interest in the property. A self-imposed hardship is not a basis for granting a variation, nor are financial concerns.

Response: The applicant is the contract purchaser and the property is currently vacant. The conditions arise as a result of size and configuration of the property and were not created by the applicant.

4. Reasonable Return. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

Response: Development of a high-quality, mixed use development that is consistent with the Village Comprehensive Plan is appropriate for the subject property, which is not possible under conventional zoning regulations.

5. Essential Character. The granting of the variation will not alter the essential character of the locality.

Response: Granting the variation will have no impact on the essential character of the locality.

6. Environmental Quality. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood.

Response: The proposed variation will not have any detrimental effects on the environmental quality of the surrounding properties and will enhance the neighborhood and Village as a whole by making productive use of a currently-vacant and significant parcel of land.



WHEELING TOWN CENTER

E DUNDEE RD & NORTHGATE PKWY
WHEELING, ILLINOIS



AN ARCADIS COMPANY

PRELIMINARY PUD REVIEW RESUBMITTAL 12.31.2015

DEVELOPER/OWNER

WTC, LLC
500 LAKE COOK RD.
SUITE 270
DEERFIELD, IL 60015
PH: (847) 943-0704
CONTACT: BRAD FRIEDMAN

GENERAL CONTRACTOR

WM. A. RANDOLPH, INC.
820 LAKESIDE DR.
SUITE 3
GURNEE, IL 60031
PH: (847) 856-0123
CONTACT: PETER FARQUHAR

CIVIL ENGINEER

ERIKSSON ENGINEERING ASSOCIATES
145 COMMERCE DR.
SUITE A
GRAYSLAKE, IL 60030
PH: (847) 223-4804
CONTACT: GEORGE DREGER

SHEET INDEX

ARCHITECTURE	
A-000	COVER SHEET
A-010	CONSTRUCTION PHASING DIAGRAMS
A-011	CONSTRUCTION PHASING DIAGRAMS
A-012	SIGNAGE LOCATION PLAN
A-100	ARCHITECTURAL SITE PLAN
A-111	RESIDENTIAL LEVEL 1 FLOOR PLAN
A-112	RESIDENTIAL LEVEL 2 FLOOR PLAN
A-113	RESIDENTIAL LEVEL 3-5 FLOOR PLAN
A-116	RESIDENTIAL ROOF PLAN
A-201	RESIDENTIAL EXTERIOR ELEVATIONS
A-202	RESIDENTIAL EXTERIOR ELEVATIONS
A-210	FLIX EXTERIOR ELEVATIONS
A-211	FLIX EXTERIOR ELEVATIONS
CIVIL	
C-0.1	NOTES & LEGENDS
C-1.1	SITE DEMOLITION PLAN (NORTH)
C-1.2	SITE DEMOLITION PLAN (SOUTH)
C-2.1	SITE GEOMETRY PLAN (NORTH)
C-2.2	SITE GEOMETRY PLAN (SOUTH)
C-3.1	SITE UTILITY PLAN (NORTH)
C-3.2	SITE UTILITY PLAN (SOUTH)
C-4.1	SITE GRADING/PAVING PLAN (NORTH)
C-4.2	SITE GRADING/PAVING PLAN (SOUTH)
C-5.1	SITWORK DETAILS
C-5.2	SITWORK DETAILS
CX-1.1	FIRE TRUCK MOVEMENT

LANDSCAPE	
L0.0	EXISTING CONDITIONS AND TREE SURVEY
L1.1	GENERAL LANDSCAPE PLAN
L1.2	SAMPLE LANDSCAPE PLAN
L2.1	VILLAGE GREEN ENLARGEMENT
L2.2	NORTHGATE PARKWAY SECTION
L2.3	PLANT PALETTE
SITE ELECTRICAL	
E-100.C	SITE LIGHTING PLAN
E-100.D	SITE PHOTOMETRIC PLAN

LOCATION MAP



ARCHITECT

RTKL ASSOCIATES, INC
200 SOUTH MICHIGAN AVE.
SUITE 1800
CHICAGO, IL 60604
PH: (312) 542-5900
CONTACT: ANTHONY FASOLO

LANDSCAPE ARCHITECT

UPLAND DESIGN, LTD.
24042 LOCKPORT ST.
PLAINFIELD, IL 60544
PH: (815) 254-0091
CONTACT: MICHELLE KELLY

WHEELING TOWN CENTER



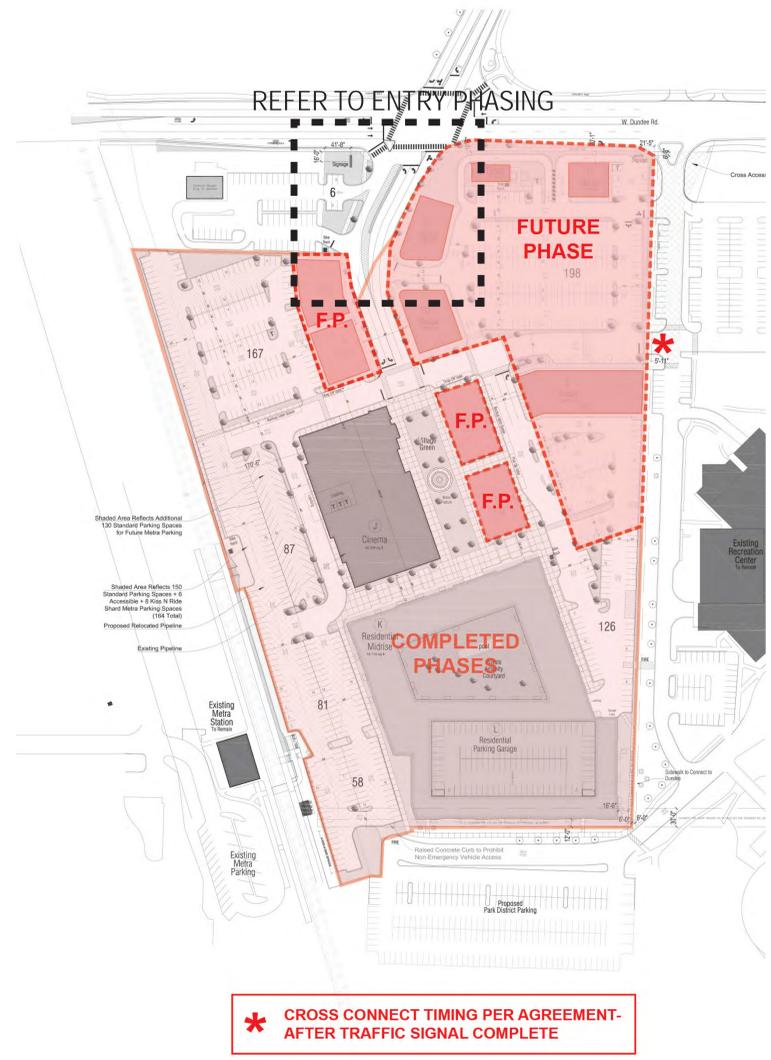
CLIENT
WTC, LLC
500 LAKE COOK RD
SUITE #270
DEERFIELD, IL 60015

ISSUE DRAWING LOG

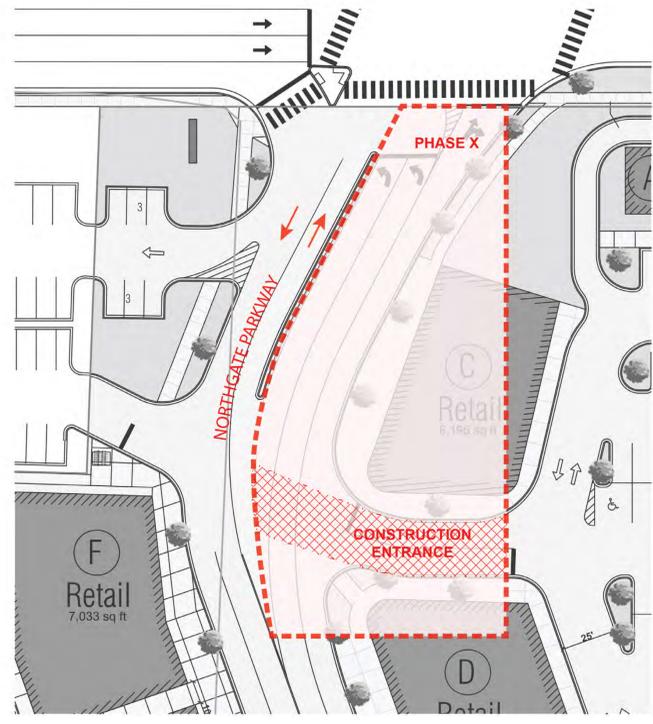
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12/31/15	PRELIMINARY PUD RESUB.

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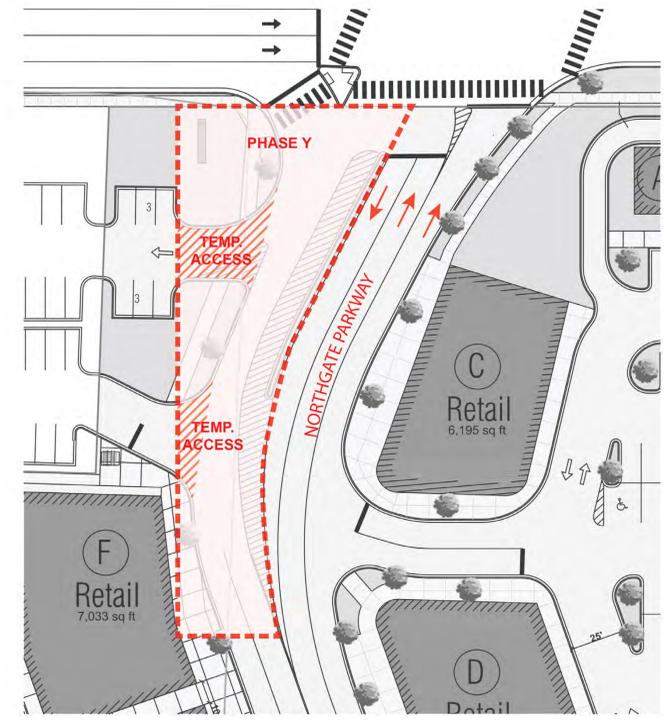
SHEET IDENTIFICATION
TITLE
CONSTRUCTION PHASING DIAGRAMS
NUMBER
A-011
2015 RTKL ASSOCIATES INC.



CONSTRUCTION PHASE 3 (FUTURE) DIAGRAM



ENTRY PHASE DIAGRAMS



NOT FOR CONSTRUCTION

WHEELING TOWN CENTER



CLIENT
WTC, LLC
500 LAKE COOK RD
SUITE 4270
DEERFIELD, IL 60015

ISSUE DRAWING LOG

DATE	DESCRIPTION
12/31/15	PRELIMINARY PUD RESUB.

SEAL

SHEET IDENTIFICATION

TITLE
RESIDENTIAL EXTERIOR ELEVATIONS
NUMBER

A-201
2015 RTKL ASSOCIATES INC.



1 NORTH ELEVATION
1" = 10'-0"



2 EAST ELEVATION (NORTH END)
1" = 10'-0"



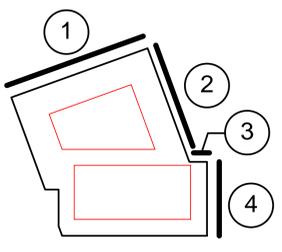
4 EAST ELEVATION (SOUTH END)
1" = 10'-0"



3 EAST ELEVATION (MIDDLE)
1" = 10'-0"

MATERIAL LEGEND

- CAST STONE
- FACE BRICK (FB-1)
- FACE BRICK (FB-2)
- FIBER CEMENT PANEL
- METAL PANEL AT PROJECTED BAYS
- CONCRETE MASONRY UNIT
- ARCHITECTURAL WALL PANEL



ELEVATION KEY

NOT FOR CONSTRUCTION

DEMOLITION NOTES

- All Signs to Be Removed Shall Be Salvaged and Stored in the Owner's Facility for Future Use as Applicable.
- Keep All Village Streets Free and Clear of Construction Related Dirt/Dust/Debris.
- Coordinate Existing Utility Removal with Local Authorities and Utility Companies Having Jurisdiction.
- Coordinate Removal of Overhead Wires And Utility Poles With Authorities Having Jurisdiction And Respective Utility Providers.
- All Sawcutting Shall be Full Depth to Provide a Clean Edge to Match New Construction. Match Existing Elevations of Points of Connection for New and Existing Pavement, Curb, Sidewalks, etc. All Sawcut Locations Shown Are Approximate and May Be Field Adjusted to Accommodate Conditions, Joints, Material Types, etc. Remove Minimum Amount Necessary for Installation of Proposed Improvements.
- Provide and Maintain All Necessary Traffic Control and Safety Measures Required During Demolition and Construction Operations Within or Near the Public Roadway.
- Perform Tree Pruning in All Locations Where Proposed Pavement And/or Utility Installation Encroach Within The Existing Drip Line Of Trees To Remain. All Trenching Within The Drip Line Of Existing Trees To Remain Shall Be Done Radially Away From Trunk If Roots in Excess Of 1" Diameter Are Exposed. Roots Must Be Cut By Reputable Tree Pruning Service Prior To Any Traverse Trenching. Obtain Approval Of The Architect Prior To Operations For A Variance From This Procedure.
- Coordinate Tree Removal with Landscape Architect. All Trees To Be Removed Shall Be Removed in Their Entirety and Stumps Shall Be Ground to Proposed Subgrade. Use As Much for Proposed Landscaping Where Applicable and Acceptable to Architect.
- Provide Tree Protection Fencing Prior To Construction Operations. Maintain Throughout Construction.

GEOMETRY NOTES

- All Dimensions Contained Herein Reference Back Of Curb, Face Of Retaining Wall, Edge Of Pavement, Center of Structure And Outside Face Of Building Foundation Unless Otherwise Noted.
- All Pavement Striping Shall Be 4" Wide Yellow Paint Per Specifications. All Cross Hatch Striping Shall Be 45° At 2'-0" Centers.
- All Accessible Parking Signs (R7-8) Must Be Placed at the Center of the Space and Within 5 Feet of the Space.
- Refer to Architectural Drawings for Exact Locations of All Buildings.
- Refer to Architectural Drawings for Locations and Details of All Permanent Site Fencing.

SOIL EROSION & SEDIMENTATION CONTROL NOTES

- Illinois Urban Manual Shall Govern All Soil Erosion and Sediment Control, and Related Work.
- Contractor Shall Be Responsible for Compliance With IEPA NPDES and ILRI0 Permit Requirements for Project.
- Soil Disturbance Shall Be Conducted in Such a Manner as To Minimize Erosion. Soil Stabilization Measures Shall Consider the Time of Year, Site Conditions, and the Use of Temporary or Permanent Measures.
- Soil Erosion and Sediment Control Features Shall Be Constructed Prior to the Commencement of Upland Disturbance.
- Temporary Soil Stabilization Shall Be Applied to Topsoil Stockpiles and Disturbed Areas, Where Construction Activity Will Not Occur For A Period of More Than 14 Calendar Days. Temporary Measures Shall Be Applied Within 7 Calendar Days of the End of Active Hydrologic Disturbance. The Sediment Control Measures Shall Be Maintained On A Continuing Basis Until the Site is Permanently Stabilized And All Inspections Are Complete. Permanent Stabilization Shall Be Completed Within 14 Days after Completion of Final Grading of Soil.
- All Temporary and Permanent Erosion Control Measures Shall Be Removed Within 30 Days After Final Site Stabilization is Achieved or After The Temporary Measures Are No Longer Needed. Trapped Sediment And Other Disturbed Soil Areas Shall Be Permanently Stabilized.
- Final Site Stabilization is Defined By The EPA General Permit as Meaning That All Soil Disturbing Activities At The Site Have Been Completed, And That A Uniform Perennial Vegetative Cover With A Density Of 70 Percent Of The Cover For Unpaved Areas Not Covered By Permanent Structures Has Been Established Or Equivalent Permanent Stabilization Measures (Such As The Use Of Riprap, Gabions, Or Geotextiles) Have Been Employed.
- All Storm Sewer Structures That Are, Or Will Be, Functioning During Construction Shall Be Protected, Filtered, Or Otherwise Treated To Remove Sediment. The General Contractor Shall Use "Catch-All" Inlet Protectors (or equal) and Filter Watties Around The Grate in Landscaped Areas And "Catch-All" Inlet Protectors (or equal) in Paved Areas To Prevent Siltation.
- All Temporary and Permanent Sediment And Erosion Control Measures Must Be Maintained, Repaired, and Inspected in Conformance With All Applicable IEPA-NPDES Phase I.
- Following The Termination Of Construction Activities And Issuance Of The Required "Notice Of Termination", The Permittees Must Keep A Copy Of The Storm Water Pollution Prevention Plan, Inspection Reports, and Records Of All The Data Used To Complete The Notice Of Intent For A Period Of At Least Three Years Following Final Stabilization.

- Install And Maintain Silt Fence At The Perimeter Of The Construction Zone And Wetland Areas And As Shown On The Plans. Maintain Silt Fence Throughout Construction And Until Vegetation Has Been Fully Established.
- Contractor Shall Provide Qualified Soil Erosion and Sediment Control Inspector Services in Accordance with NPDES and Governmental Requirements. Inspections Shall Occur at Every Seven Calendar Days Or Within 24 Hours of a 0.5" or Greater Rainfall Event. Engineer Shall Be Copied on Inspection Logs.
- The Erosion Control Measures Indicated On The Drawings Are The Minimum Requirements. Additional Measures May Be Required As Directed By The Qualified Soil and Erosion Sediment and Control Inspector Or Governing Agency.
- Unless Otherwise Indicated on the Drawings, Stabilize All Disturbed Ground Areas Where Slopes Exceed 6:1 or Within Swales with North American Green SiltNet SC150BN Erosion Control Blanket, or Approved Equal.
- Report Releases of Reportable Quantities of Oil or Hazardous Materials If They Occur in Accordance with IEPA NPDES Requirements.
- All Concrete Washout Shall Conform To The "Temporary Concrete Washout Facility" Standards (Code 954) of the Illinois Urban Manual, Latest Edition.
- If Necessary, The SWPPP Shall Be Modified To Reflect Changes Required During The Effective Period Of The IEPA NPDES General Permit, No. ILRI0 and Local and County Permits.
- Dewatering Excavations Shall Be Performed in a Manner Such as Through the use of Filter Bags or Polymer Treated Dewatering Swales, so as to Not Discharge Sediment Laden Water Into Storm Sewers Tributary to Open Water.

UTILITY NOTES

- Utility Service Lines as Shown Hereon are Approximate. Coordinate The Exact Locations With The Plumbing Drawings. Coordinate The Locations With The Plumbing Contractor and/or the Owner's Construction Representative Prior to Installation of Any New Utilities.
- Refer to Plumbing Drawings for Continuation of All Utilities Within 5 Feet of Building Face.
- Field Verify Invert & Locations of Existing Utility Mains Prior to Installing Any On-Site Utilities or Structures. All Elevations and Inverts Referencing Said Utility Shall Be Field Verified Prior to Installation Of Any New Structures Or Utilities, and Adjustments Shall Be Made as Necessary. Contact Engineer Prior to Installation if Discrepancy Exists With These Drawings.
- Coordinate the Relocation Of Any Utilities Encountered And Replacement Of Any Utilities Damaged Within Influence Zone Of New Construction. Contact Engineer if The Existing Utilities Vary Appreciably From The Plans.
- All Water Main and Services Shall Be Installed at a Minimum Depth of 5.5' From Top of Finished Ground Elevation to Top of Main.
- Protection of water supplies shall be as described in Section 370.350 of the Illinois Recommended Standards for Sewage Works or Section 41-2.01 of the Standard Specifications for Water and Sewer Main Construction in Illinois, latest edition.
- Clean Out All Existing and Proposed Storm Inlets and Catch Basins at the Completion of Construction.
- Provide Adequate Coupling Device and/or Oversized Concrete Flared-End Section to Accommodate iDPE Storm Sewer.
- The "Standard Specifications for Water and Sewer Main Construction in Illinois", Current Edition Shall Govern Work Where Applicable.

STRUCTURE NOTES

- All Catch Basins to Be Installed in Paved Areas Shall Have Neenah R2504-D Frame & Grate or Approved Equal.
- All Catch Basins to Be Installed in Landscaped Areas Shall Have Neenah R4349-B Frame & Grate or Approved Equal. For Cone Sections Install a Minimum of 4" Grate Rings For Topsoil Respread. For Flat Slab Slabs Install the Following Minimum Height of Grate Rings:
4" Diameter Structures - 4"
5" Diameter Structures - 6"
6" Diameter Structures - 6"
- All Catch Basins to Be Installed Along Curb and Gutter (B-6.12) Shall Have Neenah R3281-A Frame & Grate or Approved Equal.
- All Catch Basins to Be Installed Along Depressed Curb and Gutter (Dep B-6.12) Shall Have East Jordan Iron Works 5120 Catch Basin Inlet Frame and Grate, or Approved Equal.
- Where Structures are Shown Along the Curbside, Unless Specifically Stated Otherwise, It is Intended That the Frame of the Structure is To Fall Within the Flowline of the Gutter or at the Pavement Edge Where No Gutter Exists.
- All Manholes Shall Have Neenah R1713-B Frame & Closed Lid or Approved Equal, with "Storm" or "Sanitary" Imprinted as Appropriate.
- For All Manhole Structures to Be Adjusted, Install or Remove Adjusting Rings, New Cone Section or New Barrel Section As Necessary.
- All Flared End Sections 12" and Larger Shall Include an IDOT Standard Grate.
- All Sanitary Manholes Shall Include a Chimney Seal.

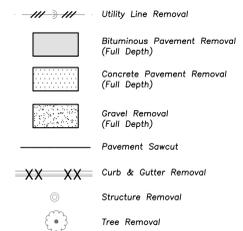
GRADING NOTES

- Install And Maintain Silt Fence at the Perimeter of the Construction Zone. Install Hay Bale Erosion Control Around All Proposed and Existing Structures Receiving Drainage From Disturbed Areas. Silt Fabric Under the Lid is Not an Acceptable Alternative in Landscaped Areas.
- The Grading and Construction of Proposed Improvements Shall Be Done in a Manner Which Will Allow For Future Drainage, and Not Cause Flooding of Stormwater on the Surface of Proposed Improvements.
- All Landscaped Areas Disturbed by Construction Shall Be Respread With 6 Inches (Min.) to 12 Inches (Max.) Topsoil and Hydroseded Unless Noted Otherwise On The Landscape Drawings.
- Refer to Architectural Drawings for Locations and Patterns of Expansion and Control Joints in Concrete Pavement and Sidewalks.
- Accessible Parking Spaces and Loading Spaces Shall Be Sloped at Maximum 2.0% in Any Direction. Maximum Sidewalk Cross Slopes Shall be 2.0%. Maximum Longitudinal Sidewalk Slope Shall Be 4.9%. Contact Engineer if Conflicts Exist.
- Conditions May Require Contractor to Excavate Clay Borrow Pit in Order to Produce Suitable Fill Material for Building Pad Preparation or Site Paving. Borrow Pit Shall Not be Located in Proposed Landscape Areas. Contractor Shall Backfill Borrow Pit to Meet Proposed Grades, or Existing Grades if Otherwise Undisturbed Areas, and May Use Topsoil and Unsuitable Material to Backfill. Review Soil Studies Prior to Excavation.

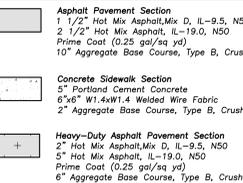
MAJOR SEDIMENT AND EROSION CONTROL MEASURES

- Install Stabilized Construction Entrance
- Install All Downslope and Sidewalk Perimeter Controls Before Commencement of Any Ground Disturbing Activity.
- Do Not Disturb An Area Until It is Necessary For Construction To Proceed.
- Cover and Stabilize Disturbed Areas As Soon As Possible.
- When Practical, Time Construction Activities To Limit Impact From Seasonal Climate Changes or Weather Events.
- Construct Sedimentation Basins and Structures.
- Perform Grading Operations and Installation of Site Infrastructure and Pavement.
- Install Permanent Seeding and Plantings.
- Remove Accumulated Sediment From Basins and Along Silt Fence.
- Construction of Infiltration Measures Shall Take Place Following Stabilization of Upstream Drainage Areas.
- Remove Temporary Sediment and Erosion Control Measures Following Final Stabilization of All Disturbed Areas.

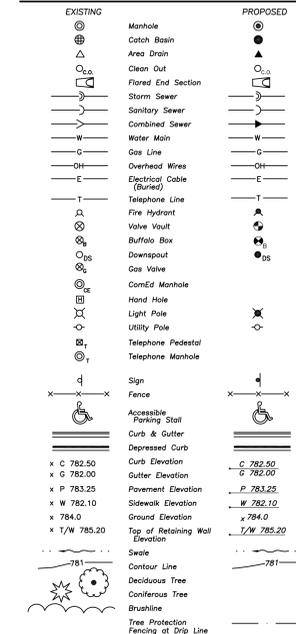
DEMOLITION LEGEND



PAVING & SURFACE LEGEND



LEGEND



GENERAL NOTES

- The Location of Existing Underground Utilities, Such As Watermains, Sewers, Gas Lines, Etc., As Shown On The Plans, Has Been Determined From The Best Available Information and is Given For The Convenience of The Contractor. However, The Owner and The Engineer Do Not Assume Responsibility in The Event That During Construction, Utilities Other Than Those Shown May Be Encountered, and That The Actual Location of Those Which Are Shown May Be Different From The Location As Shown On The Drawings. Contact Engineer Immediately if Surface and/or Subsurface Features Are Different Than Shown On The Drawings.
- Notify The Engineer Without Delay of Any Discrepancies Between the Drawings and Existing Field Conditions.
- Notify The Owner, Engineer and The Village of Wheeling A Minimum of 48 Hours in Advance of Performing Any Work.
- All Areas, On or Off Site, Disturbed During Construction Operations and Not Part of the Work As Shown Hereon Shall Be Restored to Original Condition to the Satisfaction of the Owner at No Additional Cost to the Owner; it is Incumbent Upon Contractor to Show That Damaged Areas Were Not Disturbed by Construction Operations.
- These Drawings Assume That The Contractor Will Utilize An Electronic Drawing File (DWG) and Stake All Site Improvements Accordingly.
- No Person May Utilize The Information Contained Within These Drawings Without Written Approval From Eriksson Engineering Associates, Ltd.
- The Engineer is Furnishing These Drawings For Construction Purposes As a Convenience To The Designer, Architect, Surveyor, or Contractor. Prior To The Use Of These Drawings For Construction Purposes, The User Of This Media Shall Verify All Dimensions And Locations Of Buildings With The Foundation Drawings And Architectural Site Plan, and Coordinate All Dimensions and Locations of All Site Items. If Conflicts Exist The User Of This Information Shall Contact The Engineer Immediately.
- Provide An As-Built Survey Prepared By A Licensed Professional Land Surveyor in Accordance With The Authorities Having Jurisdiction Which May Include All Detention Basins, Including All Storm and Sanitary Sewers and Structure Locations, Sizes, Rim and Invert Elevations, Final Detention Volume Calculations For The Basin(s) and Watermain and Valve and Appurtenance Locations.
- The Illinois Department Of Transportation Standard Specifications For Road And Bridge Construction Latest Edition, And All Addenda Thereto, Shall Govern The Earthwork And Paving Work Under This Contract Unless Noted Otherwise.

J.U.L.I.E.

Note: The exact location of all utilities shall be verified by the contractor prior to construction activities. For utility locations call: J.U.L.I.E. 1 (800) 892-0123

SURVEY PROVIDED BY

GREMLEY & BIEDERMANN
PLCS CORPORATION
LICENSE NO. ILL-05332
PROFESSIONAL LAND SURVEYOR
4535 NORTH ELSTON AVENUE, CHICAGO, IL 60630
TELEPHONE: (773) 685-5102 FAX: (773) 288-4184 EMAIL: INFO@PLCS-SURVEY.COM

BENCHMARK DESIGNATION: 05 NO 3301
BENCHMARK PID: NG0129
ELEVATION = 633.73 FEET (NAVD88)

LOCATION:
0.6 MI W FROM WILMETTE, 0.55 MILES WEST ALONG LAKE AVENUE FROM THE VILLAGE HALL ANNEX, AT THE INTERSECTION OF LAKE AVENUE AND RIDGE ROAD, SET IN THE NORTH LAWN OF ST. JOSEPH CHURCH, 82.2 FEET EAST OF THE EAST CURB OF RIDGE ROAD, 73.3 FEET NORTH OF THE CENTER LINE OF THE NORTHWEST MAIN ENTRANCE TO THE CHURCH 23.9 FEET SOUTH OF THE SOUTH CURB OF LAKE AVENUE. A COOK COUNTY HIGHWAY DEPARTMENT DESK IN THE TOP OF A CONCRETE POST FLUSH WITH THE GROUND AND ABOUT LEVEL WITH THE STREET.

ERIKSSON ENGINEERING ASSOCIATES, LTD.
145 COMMERCE DRIVE, SUITE A
GRAYSLAKE, ILLINOIS 60030
PHONE: (847) 223-4804
FAX: (847) 223-4864
EMAIL: INFO@EEA-LTD.COM
PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-003220
EXPIRES: 04/30/2017

WHEELING TOWN CENTER

Reserved for Seal:

Expiration Date: _____

No.	Date	Description
	10/08/15	PRELIMINARY PUD REVIEW
	12/31/15	PRELIMINARY PUD RESUB.

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Design By: **GD/JIC** Date: **10/08/15**
Checked By: **GD** Project No.: **0000.00**

Sheet Title:

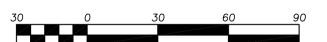
NOTES & LEGENDS

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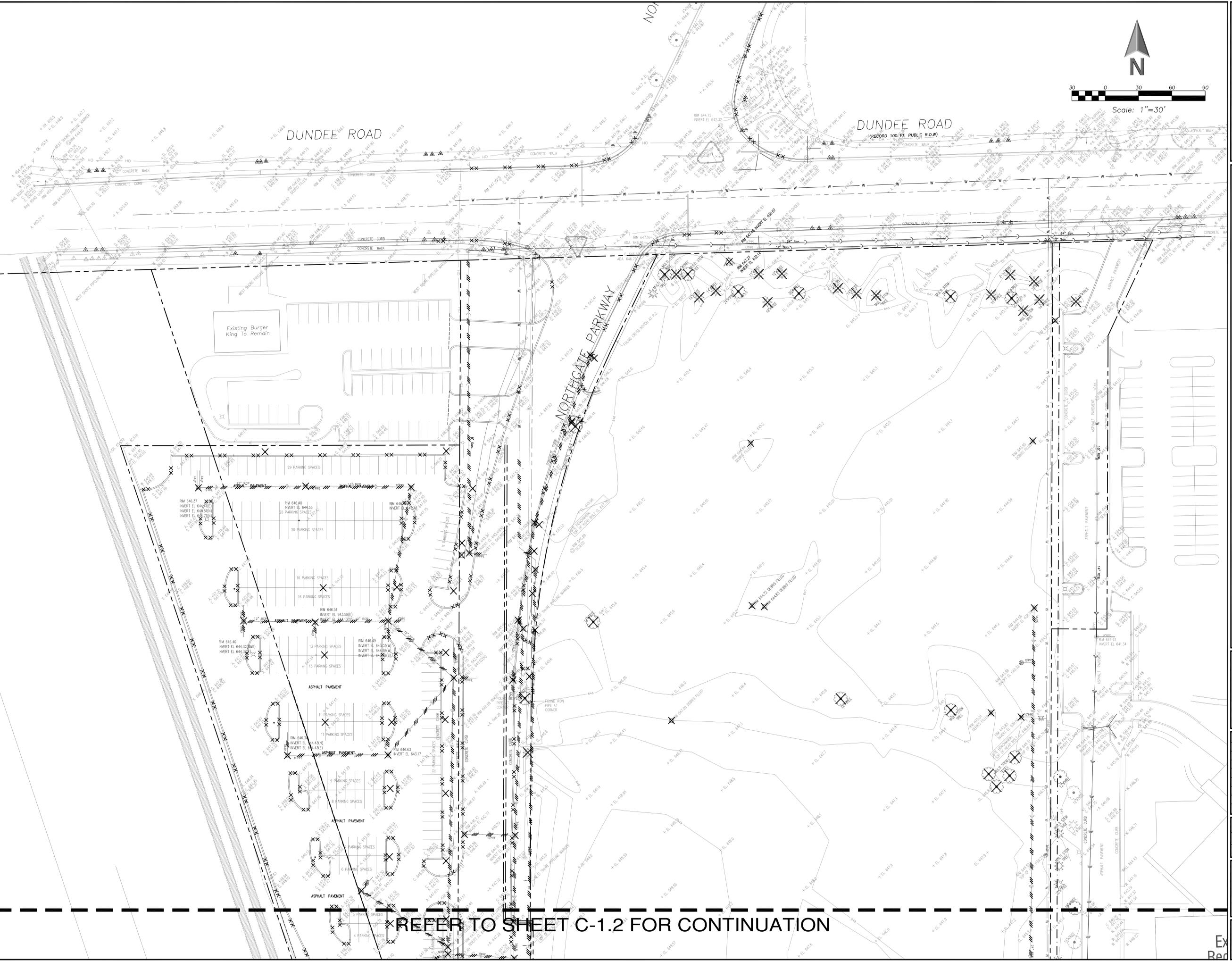
C-0.1



**ERIKSSON
ENGINEERING
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145 COMMERCE DRIVE, SUITE A
GRAYSLAKE, ILLINOIS 60030
PHONE: (847) 223-4804
FAX: (847) 223-4864
EMAIL: INFO@EEA-LTD.COM
PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-003220
EXPIRES: 04/30/2017



Scale: 1"=30'



REFER TO SHEET C-1.2 FOR CONTINUATION

WHEELING TOWN CENTER

Reserved for Seal:

Expiration Date:

No.	Date	Description
10/08/15	10/08/15	PRELIMINARY PUD REVIEW
12/31/15	12/31/15	PRELIMINARY PUD RESUB.

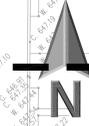
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Checked By:	GD
Date:	10/08/15
Project No.:	0000.00

Sheet Title:
**SITE DEMOLITION
PLAN
(NORTH)**

Sheet No:
C-1.1

EA - F:\George\Wheeling Town Center Urban R2\Drawing\WTC-Siteplan.dwg
Plotted: 12/29/15 @ 11:28am By: gregger

REFER TO SHEET C-1.1 FOR CONTINUATION



Scale: 1"=30'

MINNEAPOLIS, ST. PAUL & SAULT STE. MARIE RAILROAD

Existing
Metra
Station
To Remain

NORTHGATE

NORTHGATE PARKWAY

Existing
Metra
Parking

**ERIKSSON
ENGINEERING
ASSOCIATES, LTD.**
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10/08/15	10/08/15	PRELIMINARY PUD REVIEW
12/31/15	12/31/15	PRELIMINARY PUD RESUB.

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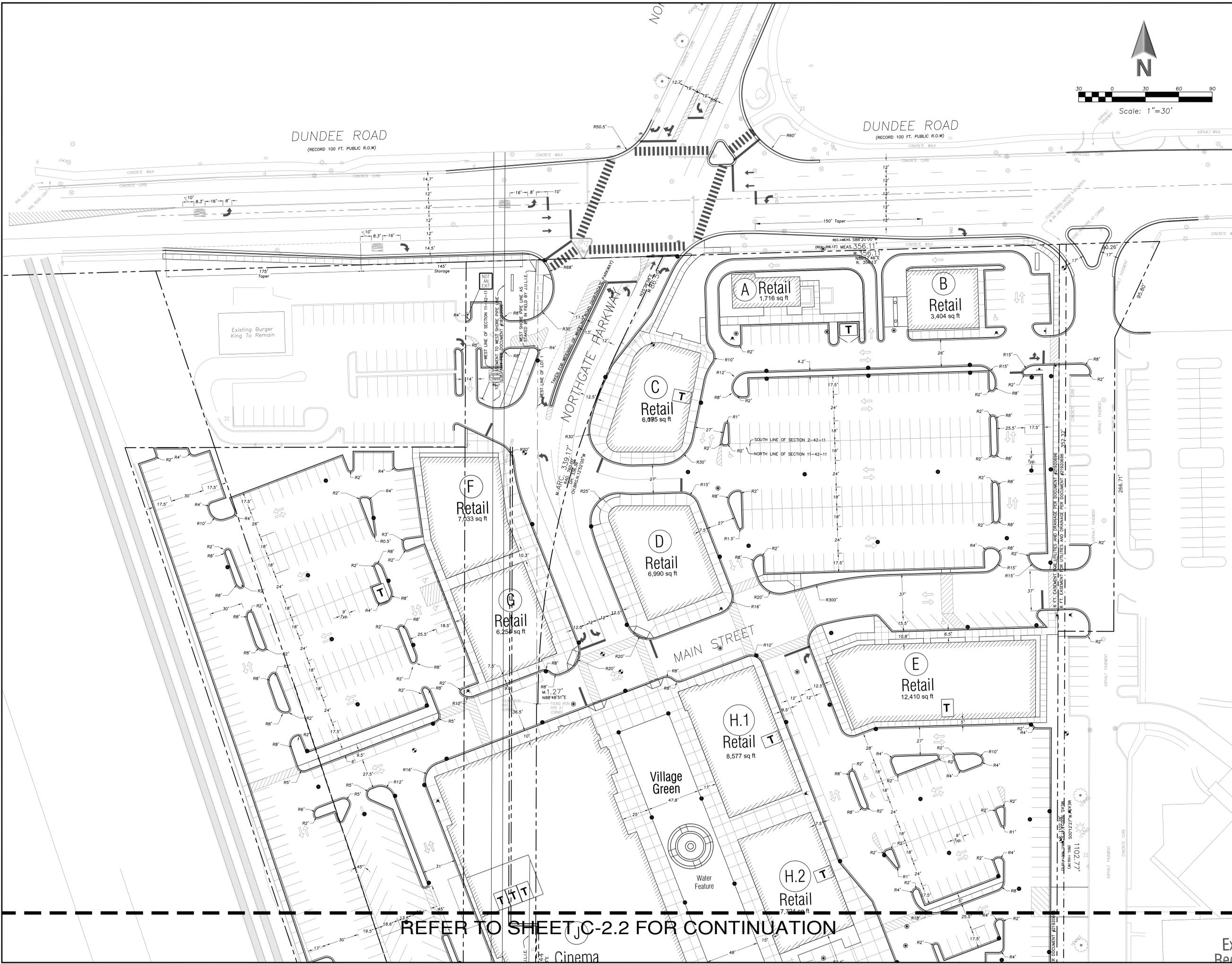
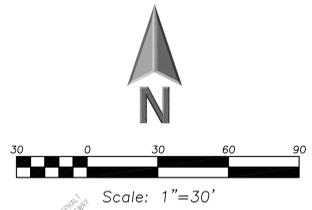
Design By: **GD/JC** Date: **10/08/15**
Checked By: **GD** Project No.: **0000.00**

Sheet Title:
**SITE DEMOLITION
PLAN
(SOUTH)**

Sheet No.:
C-1.2



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ENGINEERING
ASSOCIATES, LTD.**
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FAX: (847) 223-4864
EMAIL: INFO@EEA-LTD.COM
PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-003220
EXPIRES: 04/30/2017



WHEELING TOWN CENTER

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10/08/15	10/08/15	PRELIMINARY PUD REVIEW
12/31/15	12/31/15	PRELIMINARY PUD RESUB.

Design By:	GD/JC	Date:	10/08/15
Checked By:	GD	Project No.:	0000.00

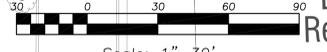
Sheet Title:
**SITE GEOMETRY
PLAN
(NORTH)**

Sheet No:
C-2.1

REFER TO SHEET C-2.2 FOR CONTINUATION

EEA - F:\George\Wheeling Town Center Urban R2 Drawing\WTC-Siteplan.dwg
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WHEELING TOWN CENTER

MINNEAPOLIS, ST. PAUL & SAULT STE. MARIE RAILROAD
Existing
Metra
Station
To Remain

Existing
Metra
Parking

J
Cinema
40,226 sq ft

K
Residential
Midrise
74,114 sq ft

pool
Private
Amenity
Courtyard

L
Residential
Parking Garage

NORTHGATE PARKWAY

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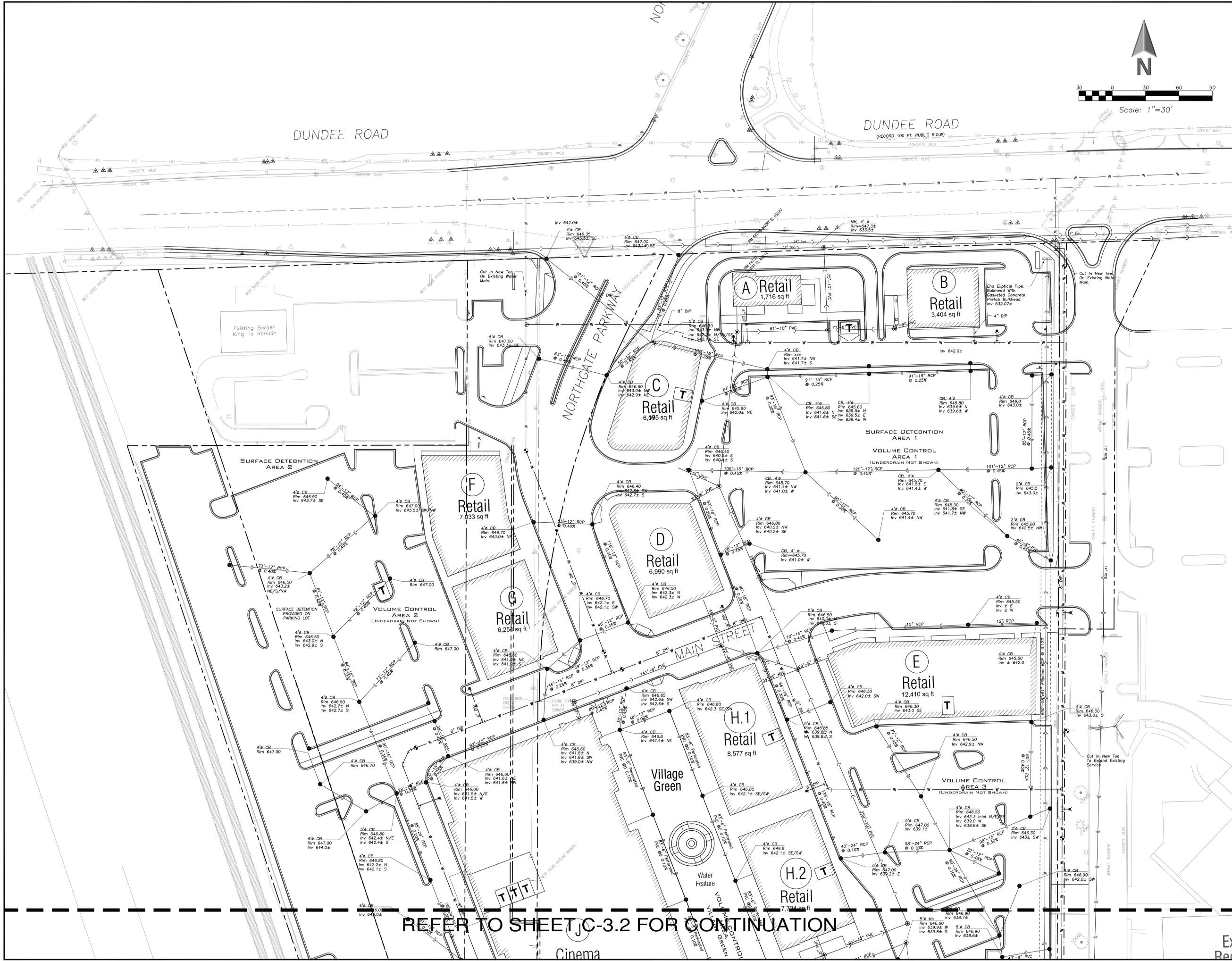
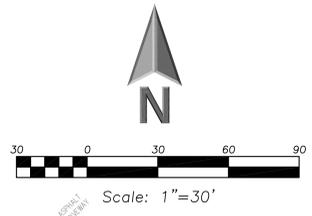
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Design By: GD/JC Date: 10/08/15
Checked By: GD Project No. 0000.00

Sheet Title:
**SITE GEOMETRY
PLAN
(SOUTH)**

Sheet No:
C-2.2



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12/31/15	12/31/15	PRELIMINARY PUD RESUB.

Design By:	GD/JC	Date:	10/08/15
Checked By:	GD	Project No.:	0000.00

Sheet Title:
**SITE UTILITY PLAN
(NORTH)**

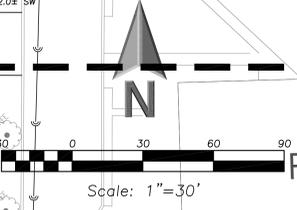
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EXPIRES: 04/30/2017



MINNEAPOLIS, ST. PAUL & SAULT STE. MARIE RAILROAD

Existing Metra Station To Remain

J
Cinema
40,226 sq ft

K
Residential Midrise
74,114 sq ft

pool

Private Amenity Courtyard

L
Residential Parking Garage

UNDERGROUND DETENTION
INV=639.5±
HWL=644.0±
0.92 AC.-FT. PROVIDED

UNDERGROUND DETENTION
HWL=644.3±
INV=638.3±
0.99 AC.-FT. PROVIDED

WORTHGATE PARKWAY

Existing Metra Parking

WHEELING TOWN CENTER

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 Checked By: **GD** Project No.: **0000.00**

Sheet Title:
**SITE UTILITY PLAN
 (SOUTH)**

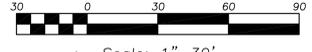
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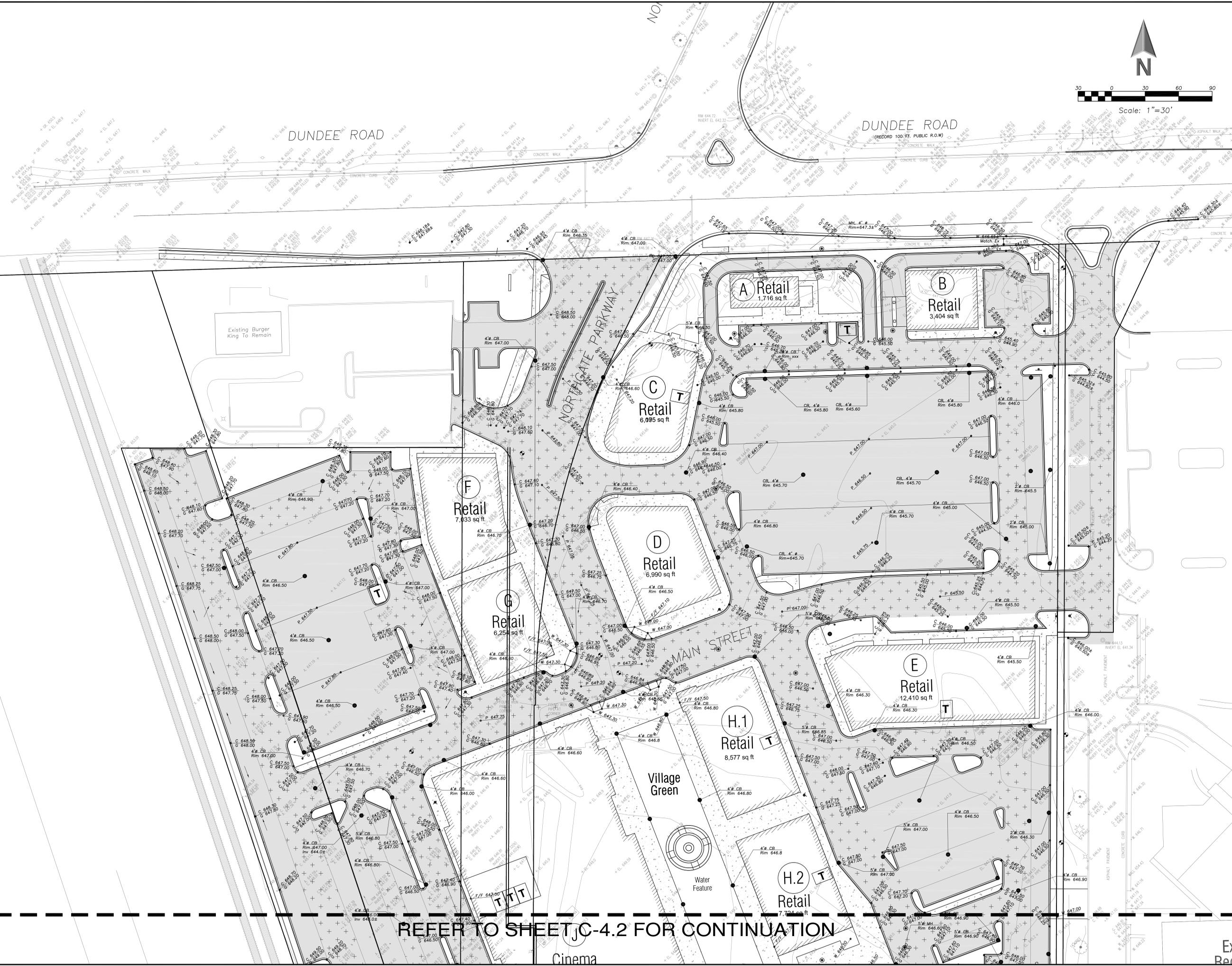


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WHEELING TOWN CENTER



Scale: 1"=30'



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12/31/15	12/31/15	PRELIMINARY PUD RESUB.

Design By:	GD/JC	Date:	10/08/15
Checked By:	GD	Project No.:	0000.00

Sheet Title:
**SITE
GRADING/PAVING
PLAN
(NORTH)**

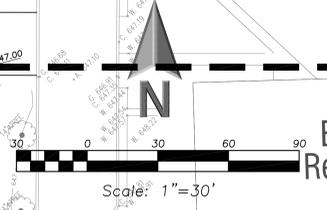
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C-4.1

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MNNEAPOLIS, ST. PAUL & SAULT STE. MARIE RAILROAD

Existing
Metra
Station
To Remain

Existing
Metra
Parking

J
Cinema
40,226 sq ft

K
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Courtyard

L
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Parking
Garage

NORTHGATE PARKWAY

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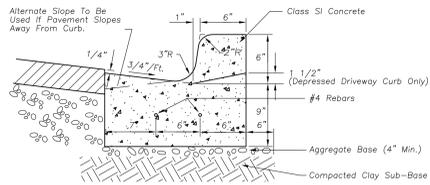
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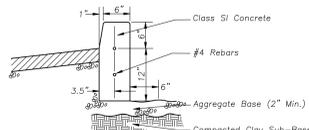
Sheet No.:
C-4.2



Notes:

1. A 1" expansion joint shall be installed at all points of curvature for short radius (under 25 ft.) curves. Maximum expansion joint spacing is 50'. Expansion joints shall be constructed with 1" thick preformed expansion joint filler conforming to the curb and gutter cross section and shall be provided with one 1-1/4" diameter, 18" long, coated smooth dowel bar. The dowel bar shall be fitted with a cap with a pinched stop which provides a minimum of 1" of expansion.
2. Maximum contraction/control joint spacing shall be 20'.

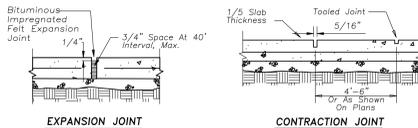
B-6.12 CURB & GUTTER



Notes:

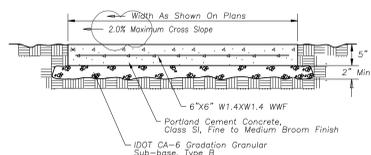
1. A 1" expansion joint shall be installed at all points of curvature for short radius (under 25 ft.) curves. Maximum expansion joint spacing is 50'. Expansion joints shall be constructed with 1" thick preformed expansion joint filler conforming to the curb and gutter cross section and shall be provided with one 1-1/4" diameter, 18" long, coated smooth dowel bar. The dowel bar shall be fitted with a cap with a pinched stop which provides a minimum of 1" of expansion.
2. Maximum contraction/control joint spacing shall be (20') twenty feet.

TYPE B CURB

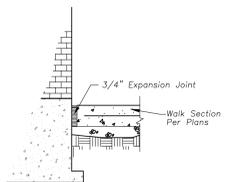


EXPANSION JOINT

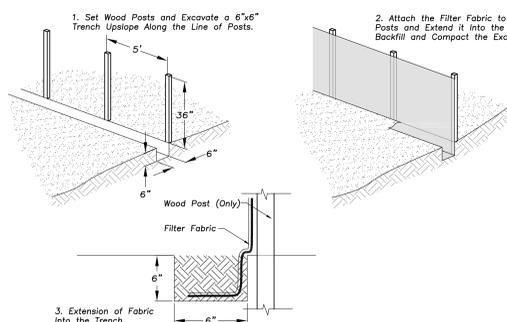
CONTRACTION JOINT



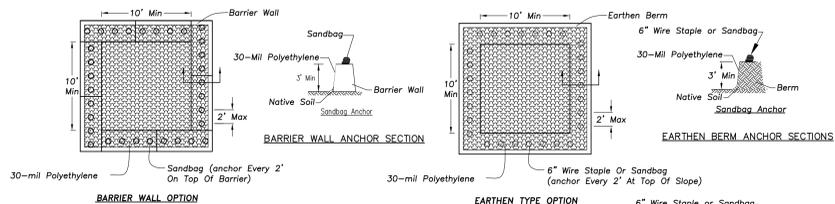
PORTLAND CEMENT CONC. SIDEWALK



WALK ADJACENT TO BUILDING

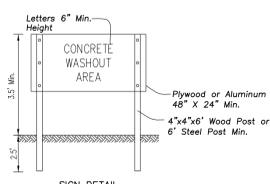


SILT FENCE CONSTRUCTION (AASHTO 288-00)



BARRIER WALL OPTION

EARTHEN TYPE OPTION

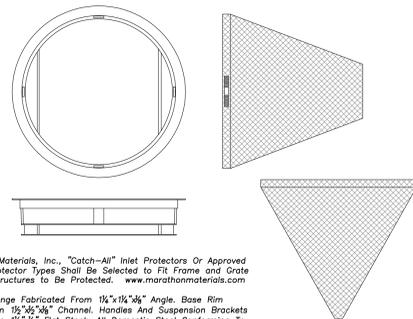


SIGN DETAIL

NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardened concrete and/or slurry and returning the facilities to a functional condition.
2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
3. Each straw bale is to be staked in place using (2) 2"x4" wooden stakes.

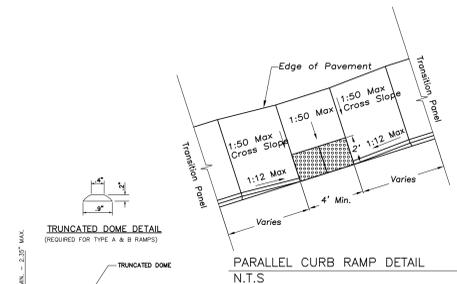
CONCRETE WASHOUT FACILITY



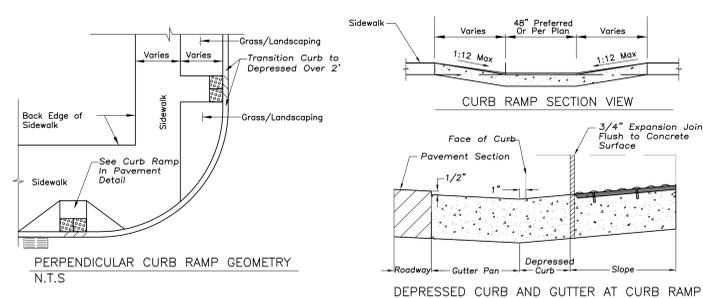
General Notes:

- Use Marathon Materials, Inc., "Catch-All" Inlet Protectors Or Approved Equal. Inlet Protector Types Shall Be Selected to Fit Frame and Grate of Open Lid Structures to Be Protected. www.marathonmaterials.com
- Frame: Top Flange Fabricated From 1/2"x1/2"x1/8" Angle. Base Rim Fabricated From 1/2"x3/4" Channel. Handles And Suspension Brackets Fabricated From 1/2"x1/2" Flat Stock. All Domestic Steel Conforming To ASTM-A36.
- Sediment Bag: Bag Fabricated From 4 Oz./sq.yd. Non-woven Polypropylene Geotextile Reinforced With Polyester Mesh. Bag Secured To Base Rim With A Stainless Steel Strap And Lock.
- Install Baskets In All Existing Storm Inlets Prior to Construction And All New Storm Inlets Immediately After Installation. Contractor Shall Maintain Throughout Construction Activities.

INLET PROTECTORS - SEDIMENT BAG

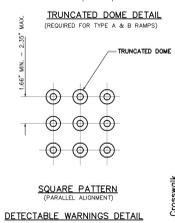


PARALLEL CURB RAMP DETAIL N.T.S.

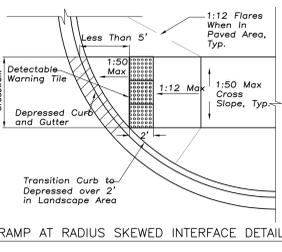


CURB RAMP SECTION VIEW

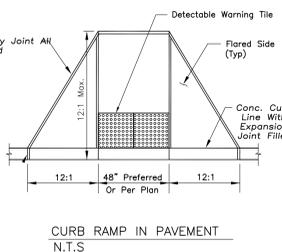
DEPRESSED CURB AND GUTTER AT CURB RAMP



DETECTABLE WARNINGS DETAIL



CURB RAMP AT RADIUS SKEWED INTERFACE DETAIL N.T.S.



CURB RAMP IN PAVEMENT N.T.S.

GENERAL NOTES

- This work shall consist of constructing sidewalk curb ramps with detectable warnings in compliance with the Americans with Disabilities Act, Accessibility Guidelines (ADAAG). The detectable warnings shall consist of an area of truncated domes that provide both visual and tactile cues to pedestrians who are about to enter into traffic.
- The detectable warnings shall present a contrast in color from the adjacent sidewalk.
- Construction Requirements:**
1. Ramps Shall Be Located As Shown On Plans In Alignment With Normal Sidewalk And/or Crosswalk And Shall Have Sufficient Curb Length At Corner Radius To Prevent Vehicular Encroachment.
 2. Curb Ramps At Marked Crossings Shall Be Wholly Contained Within The Markings Excluding Flared Sides.
 3. In No Case Shall Expansion Joints That Are Constructed In Curb And Gutters At Or Near Corner Radiuses Be Located Within The Accessible Ramp Areas.
 4. Where Preformed Joint Filler (p.j.f.) Is Installed Against A Curved Surface, The Contractor Shall Use A Flexible Filler In Accordance With The Illinois Department of Transportation (IDOT) Standard Specifications.
- Equipment:** Equipment for the detectable warning area of the curb ramps shall meet the requirements of the Authorities having jurisdiction.
- DETECTABLE WARNING SURFACE TILES**
- Provide Surface Applied Detectable/Tactile Warning Surface Tiles which comply with the detectable warnings on walking surface section of the Americans with Disabilities Act (Title III Regulations, 28 CFR Part 36 ADA STANDARDS FOR ACCESSIBLE DESIGN, Appendix A, Section 4.29.2 Dimensions: Cast In Place Detectable/Tactile Warning Surface Tiles shall be held within the following dimensions and tolerances:
- Length and Width in One of the Following as Necessary for the Dimensions of the Surface in Which the Detectable Warning is Proposed: 12x12 24x24 24x36 24x48 24x60 36x48 36x60 nominal. No Cutting of Tiles is Allowed.
- Installation of Detectable/Tactile Warning Surface Tiles Per Manufacturer's Instructions.
- Verify with the Local Authorities Having Jurisdiction that Detectable/Tactile Warning Surface Tiles are an Acceptable Method of Providing for the Detectable Warning Surface.
- Verify with the Local Authorities Having Jurisdiction that the Manufacturer's Products are Acceptable for the Detectable Warning Surface.

ACCESSIBLE RAMPS FOR SIDEWALKS



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12/31/15	12/31/15	PRELIMINARY PUD RESUB.

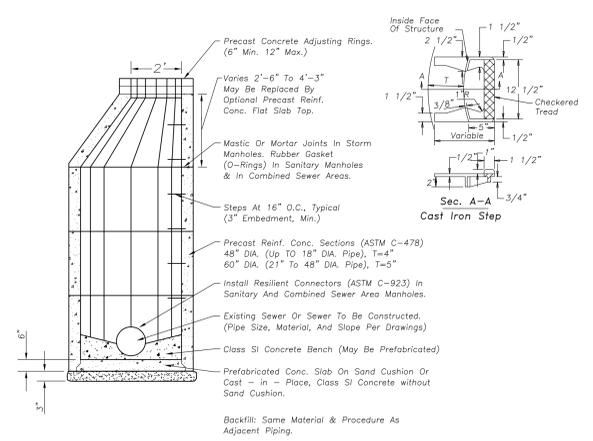
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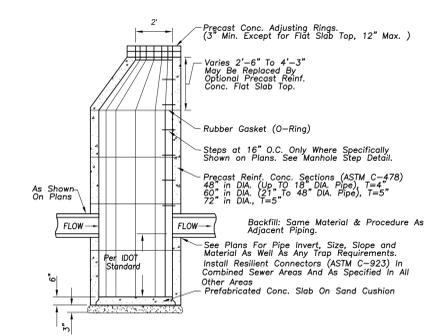
SITWORK DETAILS

Sheet No.:

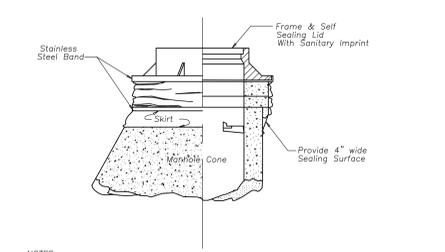
C-5.1



MANHOLE - 48"-60" DIA.

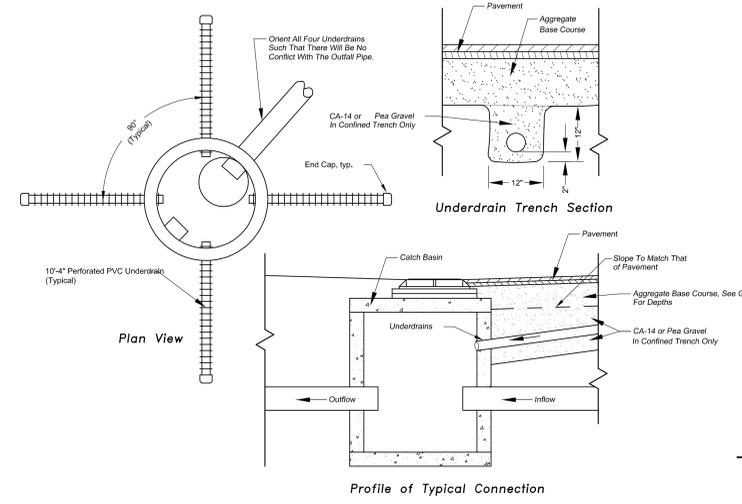


CATCH BASIN

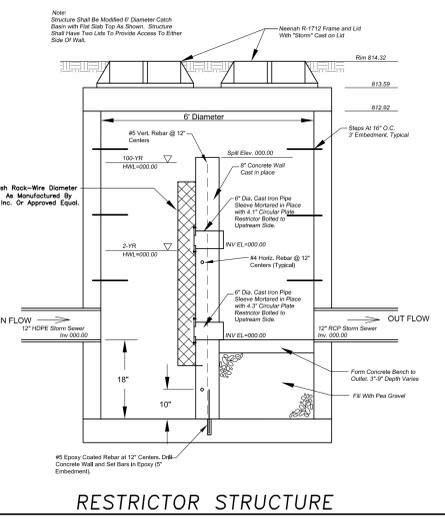
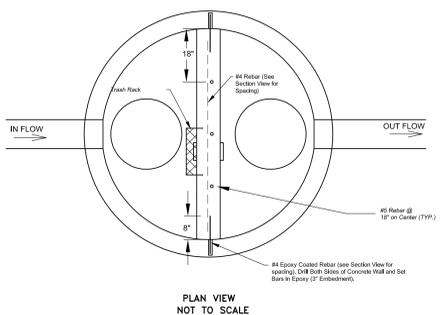


NOTES:
1. Chimney Seals shall be installed on all Sanitary Sewerage System Manholes.
2. "CRETEX" External Seals are required. Other products or other design solutions shall require the approval of the Village Engineer.
3. Chimney Seals shall be installed in accordance with the manufacturer's instructions.

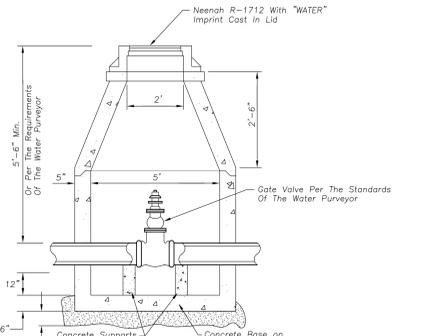
SANITARY MANHOLE CHIMNEY SEAL



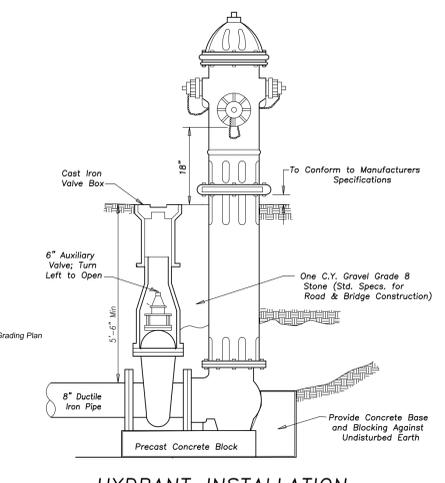
CATCH BASIN WITH FINGER DRAINS



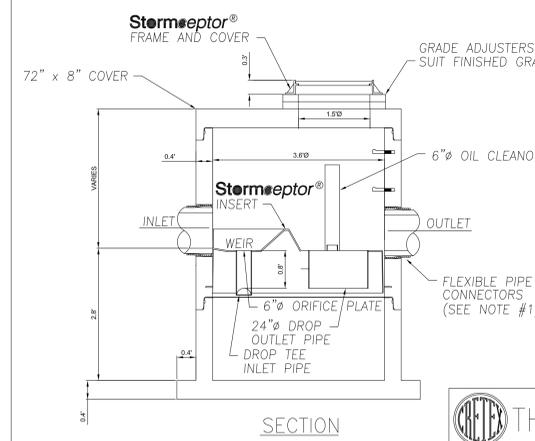
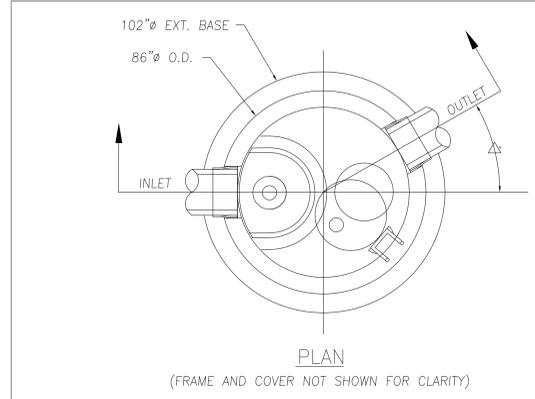
RESTRICTOR STRUCTURE



WATER VALVE VAULT



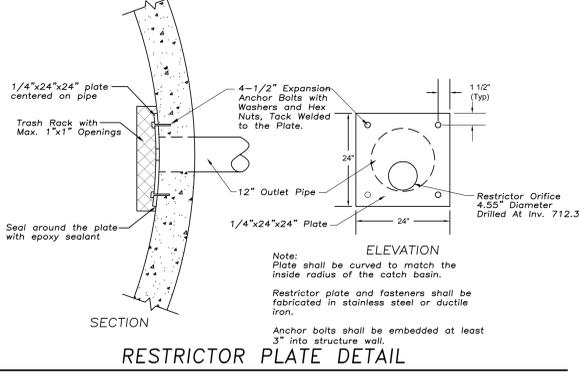
HYDRANT INSTALLATION



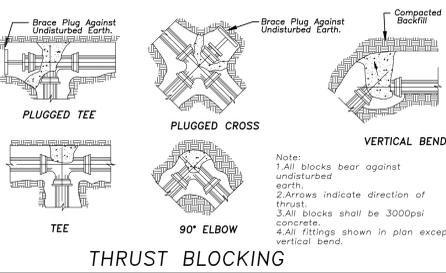
PROJECT		TITLE		STC 900	NUMBER	REV.
PRECAST CONCRETE		Stormceptor				

- NOTES
- THE USE OF FLEXIBLE PIPE CONNECTORS IS RECOMMENDED AT THE INLET AND OUTLET WHERE APPLICABLE.
 - THE COVER SHOULD BE POSITIONED OVER THE DROP OUTLET PIPE AND THE OIL CLEANOUT PIPE.
 - ALL JOINTS SHALL HAVE NEOPRENE GASKETS CONFORMING TO ASTM C443.
 - DESIGNED FOR AASHTO HS-20 LOADING.
 - THE Stormceptor SYSTEM IS PROTECTED BY ONE OR MORE OF THE FOLLOWING U.S. PATENTS: #4985148, #5498331, #5725760, #5753115, #5849181.
 - PRODUCT WEIGHT:
72"Ø BARREL = 1840 lbs/ft
INTEGRAL BASE SLAB = 5670 lbs
COVER SLAB = 3940 lbs

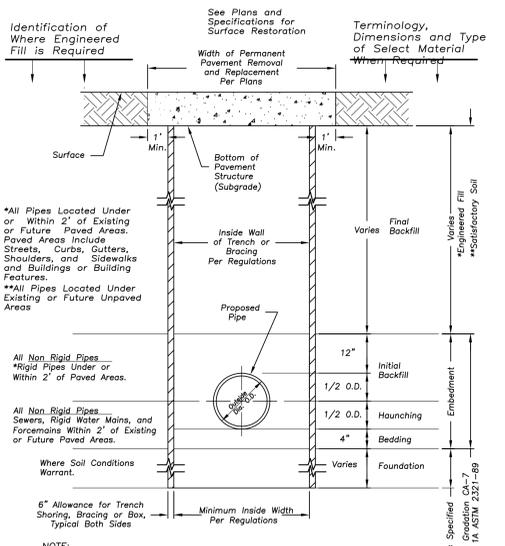
SCALE				REVISION		BY		DATE	
DATE		DR'N.		APVD					



RESTRICTOR PLATE DETAIL



THRUST BLOCKING



NOTES:
For Rigid Pipe the Minimum Depth From Subgrade to Top of Pipe Shall be 12".
For Non-rigid Pipe the Minimum Depth From Subgrade to Top of Pipe Shall be 12" for Pipe up to 48" Diameter and 18" For Pipe Over 48" Diameter. Non-rigid Pipe is Defined as Flexible Thermoplastic Pipe and/or Corrugated Metal Pipe.

STANDARD SEWER AND WATER TRENCH SECTION

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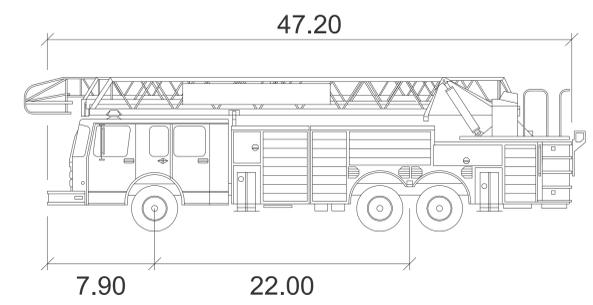
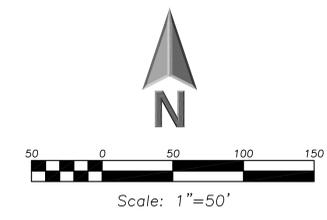
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Checked By:	GD	Project No.:	0000.00

Sheet Title:
SITWORK DETAILS

Sheet No:
C-5.2

EA - F:\George\Wheeling Town Center Urban R2 Drawing\WTC-Station.dwg
 Plotted: 12/29/15 @ 12:17pm By: gregor



Wheeling Fire Truck	feet
Width	: 7.90
Track	: 7.90
Lock to Lock Time	: 6.0
Steering Angle	: 39.2

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12/31/15	12/31/15	PRELIMINARY PUD RESUB.

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 Design By: GD/JC Date: 10/08/15
 Checked By: GD Project No. 0000.00

Sheet Title:
**Fire Truck
Movement**

Sheet No:
CX-1.1

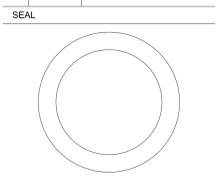
WHEELING TOWN CENTER



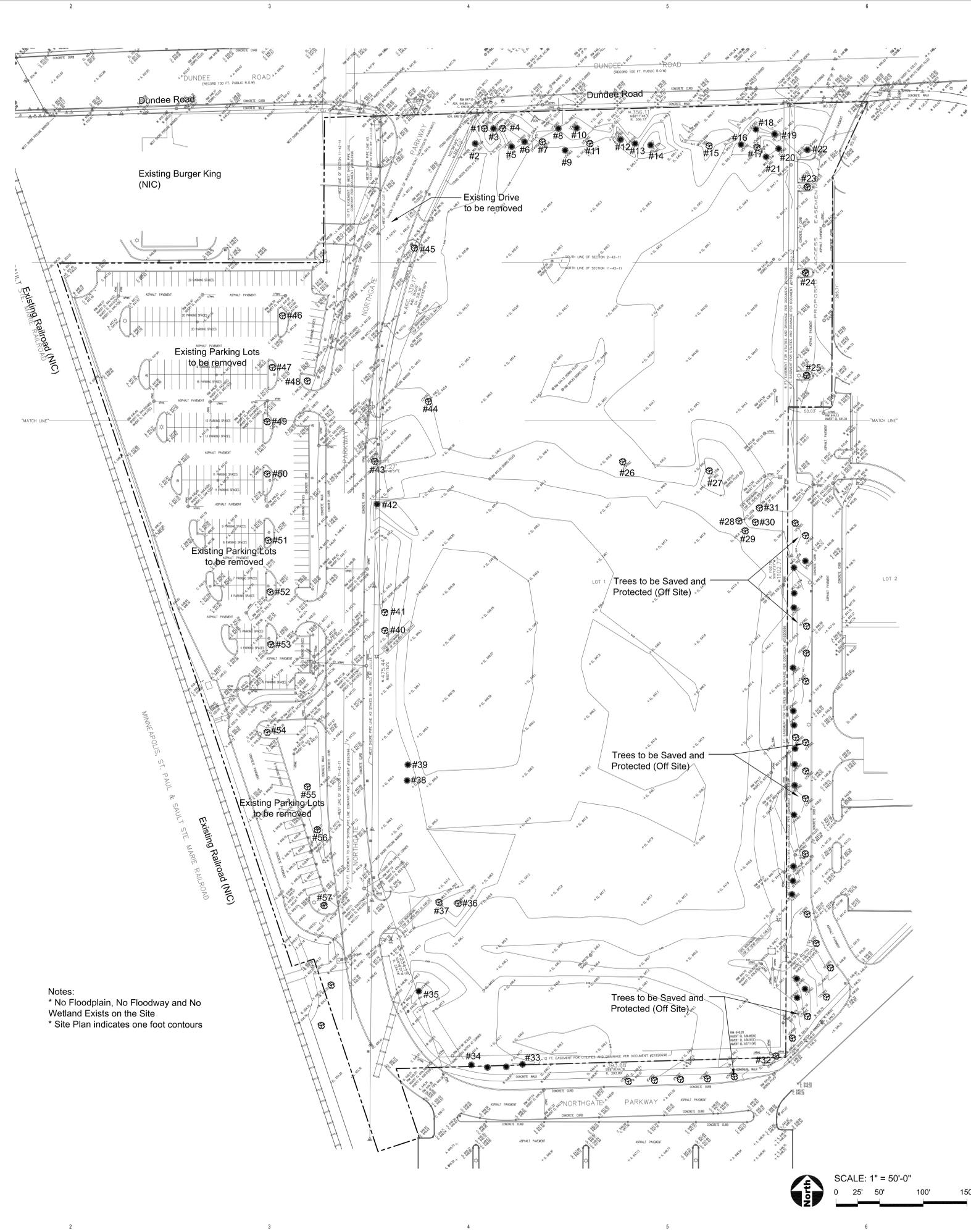
CLIENT
 WTC, LLC
 500 LAKE COOK RD
 SUITE 670
 DEERFIELD, IL 60015

ISSUE DRAWING LOG

12/31/2015	PRELIMINARY PUD RESUB
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SHEET IDENTIFICATION
 TITLE
EXISTING CONDITIONS & TREE SURVEY
 NUMBER
L1.0
 2015 RTKL ASSOCIATES INC.



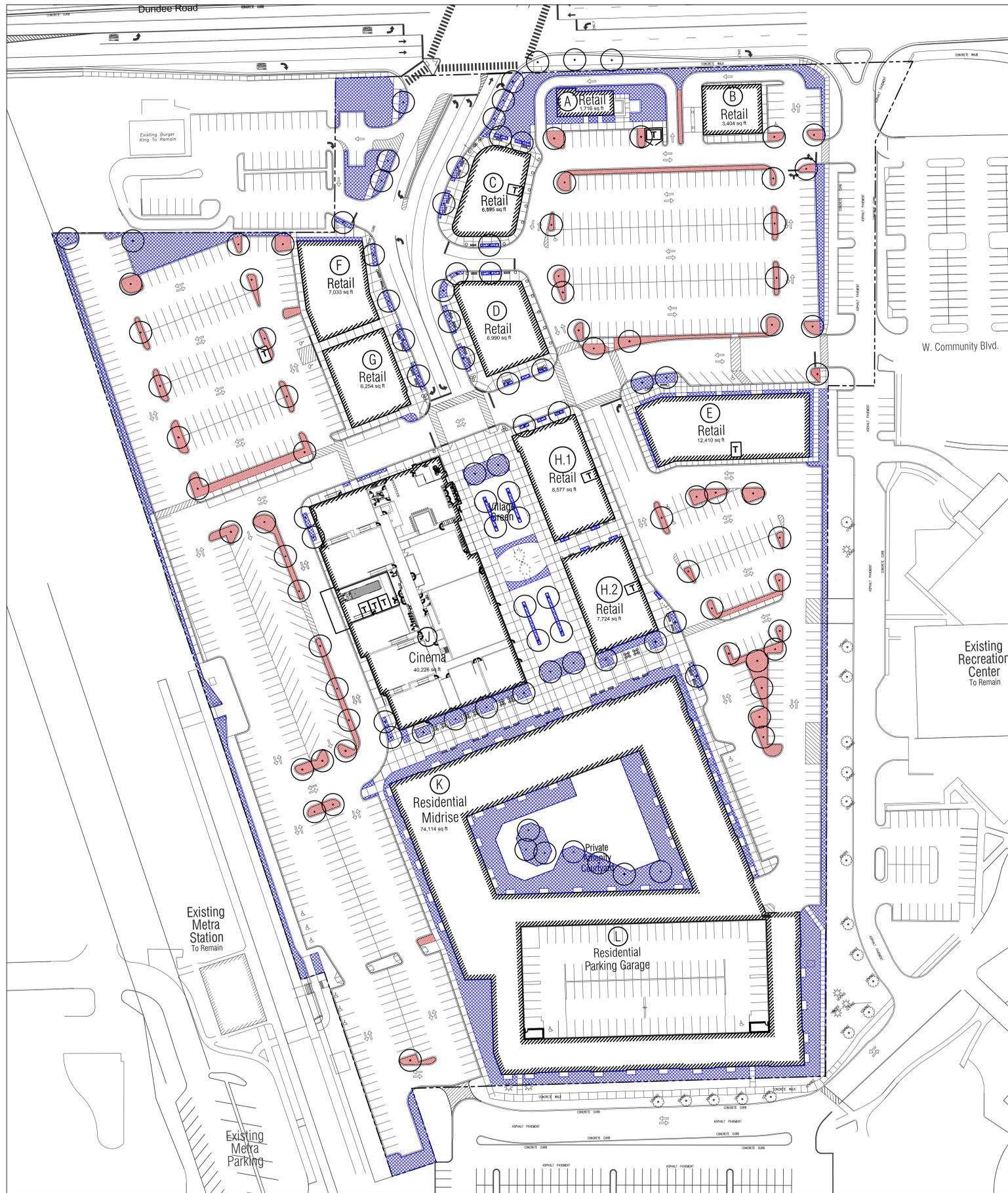
Notes:
 * No Floodplain, No Floodway and No Wetland Exists on the Site
 * Site Plan indicates one foot contours

EXISTING TREE LIST

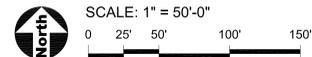
All existing trees to be removed.

KEY #	SIZE	COMMON NAME	BOTANICAL NAME	CONDITION
1	7", 5", 16" cal.	Austrian Pine	Pinus nigra	Good
2	7"	Crabapple	Malus sp.	Poor
3	14" cal.	Austrian Pine	Pinus nigra	Fair
4	9" cal.	Prairiefire Crabapple	Malus x 'Prairiefire'	Good
5	14" cal.	Austrian Pine	Pinus nigra	Good
6	14" cal.	Austrian Pine	Pinus nigra	Good
7	24" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
8	12" cal.	White Spruce	Picea glauca	Poor
9	12" cal.	White Spruce	Picea glauca	Good
10	12" cal.	Colorado Blue Spruce	Picea pungens	Good
11	30" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
12	2-12" cal.	Austrian Pine	Pinus nigra	Fair
13	14" cal.	Austrian Pine	Pinus nigra	Poor
14	16" cal.	Austrian Pine	Pinus nigra	Fair
15	10" cal.	Prairiefire Crabapple	Malus x 'Prairiefire'	Good
16	18" cal.	Austrian Pine	Pinus nigra	Fair
17	16" cal.	Austrian Pine	Pinus nigra	Poor
18	2-10" cal.	Austrian Pine	Pinus nigra	Good
19	20" cal.	Austrian Pine	Pinus nigra	Good
20	14" cal.	Austrian Pine	Pinus nigra	Poor
21	16" cal.	Austrian Pine	Pinus nigra	Poor
22	16" cal.	Austrian Pine	Pinus nigra	Good
23	8" cal.	Littleleaf Linden	Tilia cordata	Poor
24	10" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
25	12" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Fair
26	12" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
27	7" cal.	American Linden	Tilia americana	Poor
28	5", 5", 4" cal.	Boxelder	Acer negundo	Poor
29	4" cal.	Mulberry	Morus sp.	Poor
30	4" cal.	Mulberry	Morus sp.	Poor
31	4" cal.	Mulberry	Morus sp.	Poor
32	10" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
33	14" cal.	White Spruce	Picea glauca	Good
34	14" cal.	White Spruce	Picea glauca	Fair
35	14" cal.	White Spruce	Picea glauca	Good
36	9" cal.	Crabapple	Malus sp.	Fair
37	7", 8" cal.	Crabapple	Malus sp.	Fair
38	7", 5" cal.	Spruce	Picea sp.	Fair
39	9" cal.	Spruce	Picea sp.	Fair
40	6", 8", 7", 5" cal.	Maple	Acer sp.	Poor
41	9.5" cal.	Crabapple	Malus sp.	Good
42	8" cal.	Spruce	Picea sp.	Poor
43	20" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
44	12" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
45	14" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
46	7" cal.	Red Maple	Acer rubrum	Good
47	7" cal.	Red Maple	Acer rubrum	Good
48	9" cal.	Red Maple	Acer rubrum	Good
49	6.5" cal.	Red Maple	Acer rubrum	Good
50	7" cal.	Red Maple	Acer rubrum	Good
51	6" cal.	Red Maple	Acer rubrum	Good
52	3" cal.	Red Maple	Acer rubrum	Good
53	3" cal.	Red Maple	Acer rubrum	Good
54	7.5" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
55	12" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
56	11" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good
57	9" cal.	Thornless Honeylocust	Gleditsia triacanthos var. inermis	Good

NOT FOR CONSTRUCTION



- LEGEND**
-  Required Trees for Parking Lot Perimeter and Interior Applications
 -  Interior Landscaping for Off Street Parking Areas
 -  Open Space Landscape Bed



NOT FOR CONSTRUCTION



AN ARCADIS COMPANY

RTKL ASSOCIATES INC.
200 S. MICHIGAN AVENUE
CHICAGO, IL 60604
312-542-5800 MAIN
312-542-5901 FAX
WWW.RTKL.COM
PROJECT NUMBER 030-120019.01

CONSULTANT



PROJECT

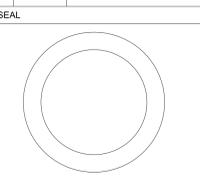
WHEELING TOWN CENTER



CLIENT
WTC, LLC
500 LAKE COOK RD
SUITE 670
DEERFIELD, IL 60015

ISSUE DRAWING LOG

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12/31/2015	PRELIMINARY PUD RESUB

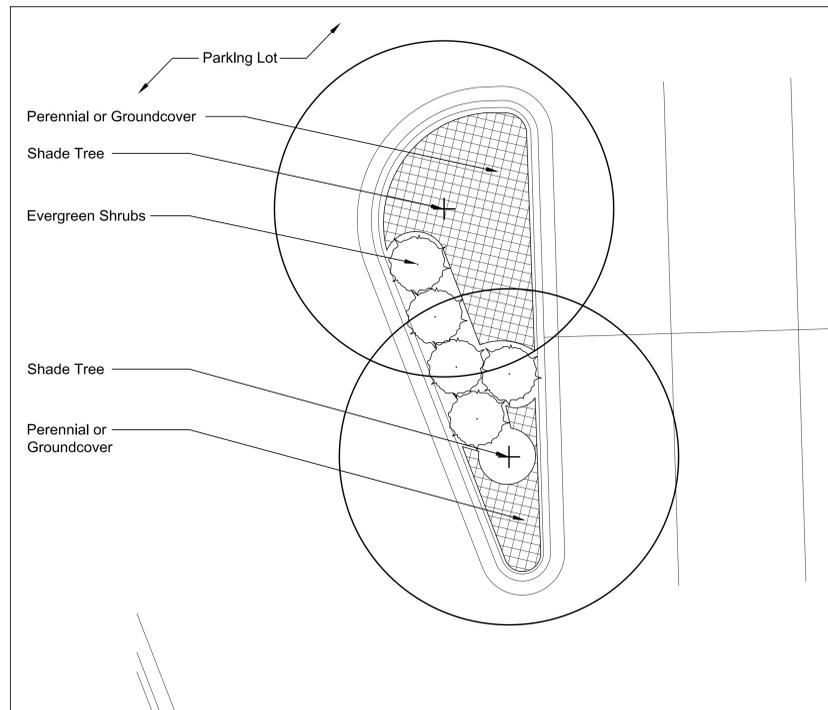


SHEET IDENTIFICATION
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GENERAL LANDSCAPE PLAN

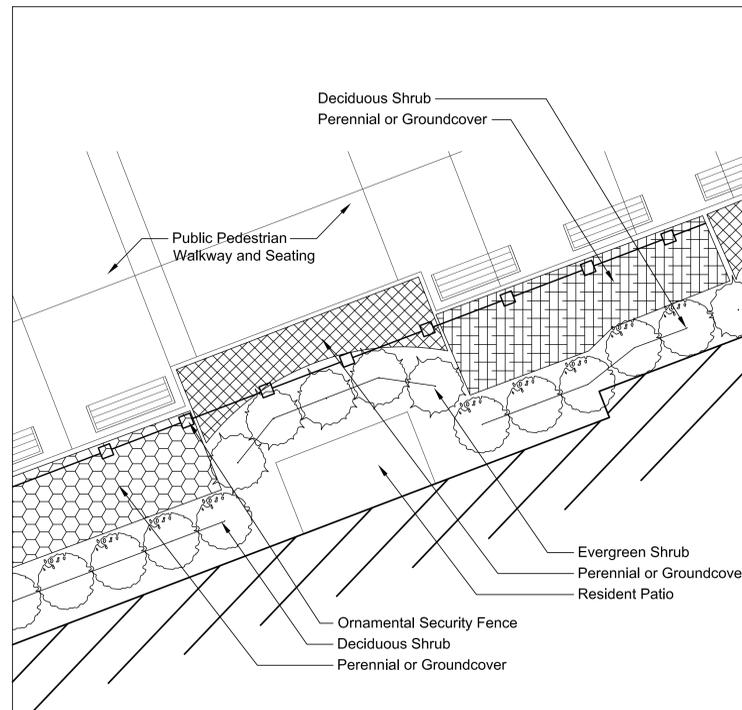
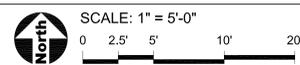
NUMBER
L1.1

2015 RTKL ASSOCIATES INC.

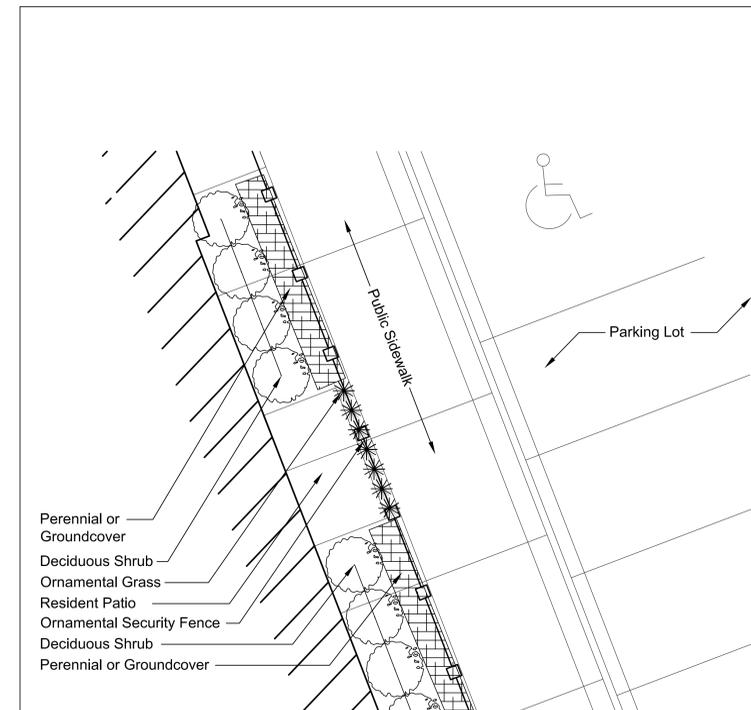
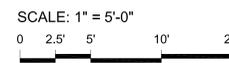
WHEELING TOWN CENTER



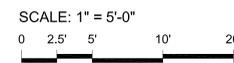
Sample Landscape Island



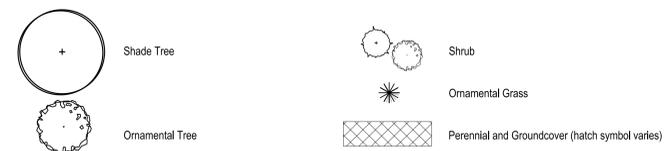
Sample Landscape at 1st Floor Patio - North of Building K



Sample Landscape at 1st Floor Patio - East and West of Building K

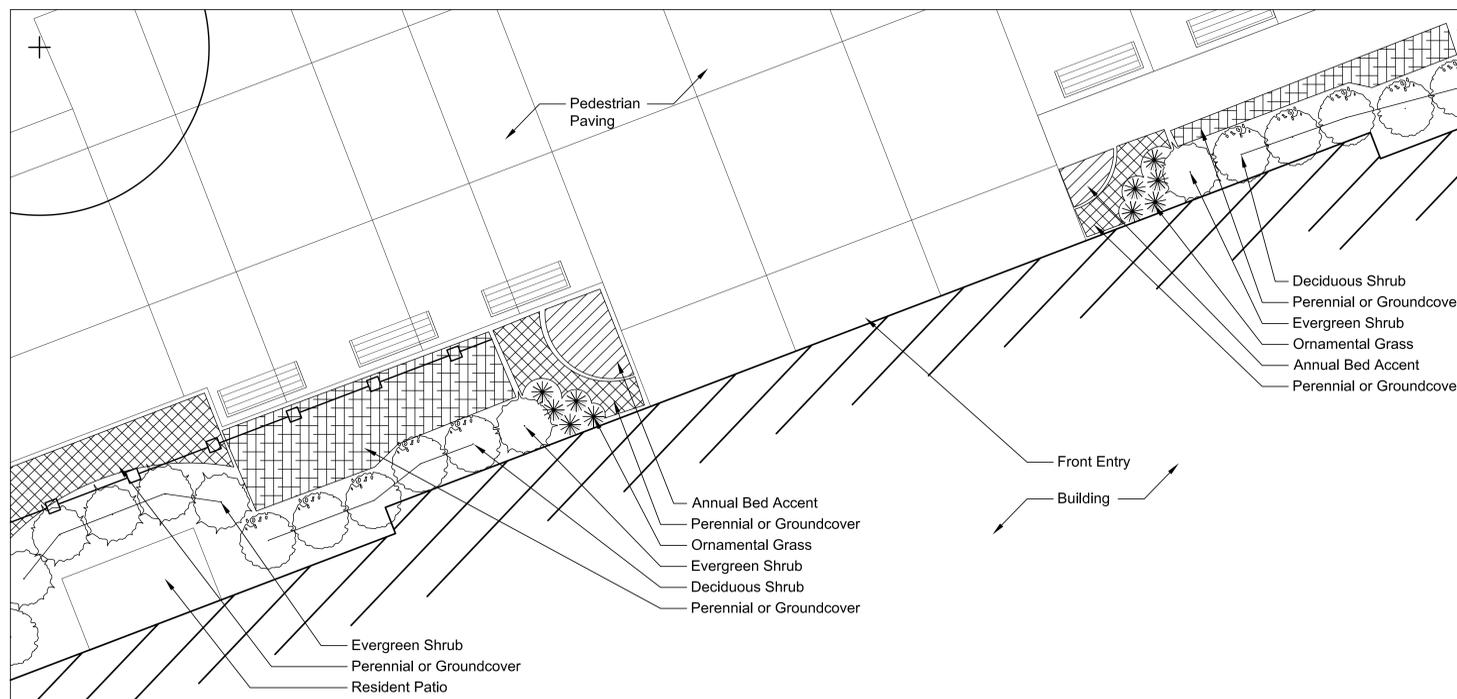


LEGEND

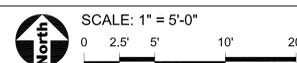


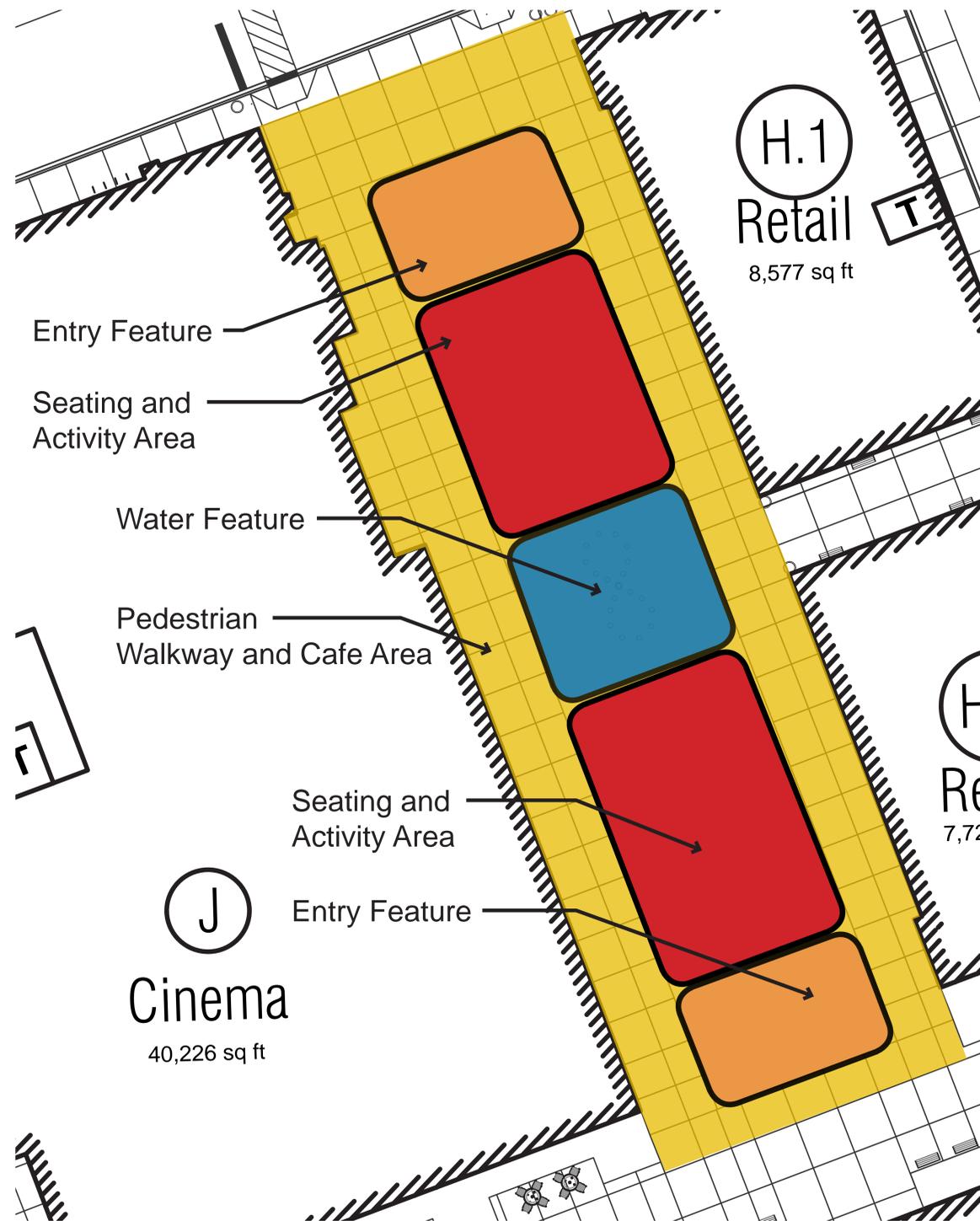
VILLAGE OF WHEELING LANDSCAPE ORDINANCE REQUIREMENTS

	Landscape Required	Landscape Proposed
Tree Preservation and Enhancement		
Replacement Trees		
1.5 times the tree trunk diameter of the removed tree	1,065 caliper inches = 354 trees	57 parking lot shade trees 63 site shade trees = 120 shade trees total
Landscaping Requirements for Off-street Parking Areas		
Required Trees for Parking Lot Perimeter and Interior Applications		
1 shade or ornamental tree for every fifteen parking spaces	744 total parking spaces / 15 spaces = 50 trees	57 shade trees
Interior Landscaping for Off-street Parking Areas		
20 SF of interior landscape island or landscaped peninsula per stall	740 total parking spaces x 20 SF = 14,800 SF	14,239 SF interior landscape
landscaped peninsulas min 200 SF; landscaped peninsulas min 100 SF		variance requested
25% - 70% evergreens		meets ordinance
30" max height within vision triangle		meets ordinance
Screening for Off-street Parking Areas		
street frontage 8' greenbelt in width; 18" high at planting and 30" high at maturity		variance requested
6' greenbelt along all interior lot lines		variance requested
Vision Triangle		
2' or less in height or have clearance of eight (8) feet beneath the lowest branch or projection		meets ordinance
Species Mix		
10-19: 50% of any one species		meets ordinance
20-39: 40% of any one species		
40-59: 30% of any one species		
60 or more: 20% of any one species		
Plant Sizes		
Deciduous Trees: 3" cal.		meets ordinance
Evergreen Trees: 6' ht.		
Ornamental Trees: 2" cal.		
Shrubs: 24" ht.		



Sample Landscape Bed





NOT FOR CONSTRUCTION

**WHEELING TOWN
CENTER**



CLIENT
WTC, LLC
500 LAKE COOK RD
SUITE #270
DEERFIELD, IL 60015

ISSUE DRAWING LOG

12/31/2015	PRELIMINARY PUD RESUB
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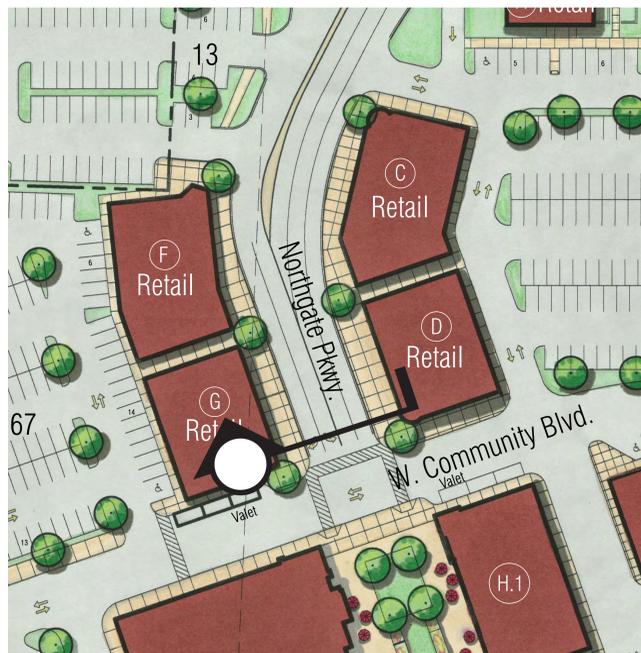
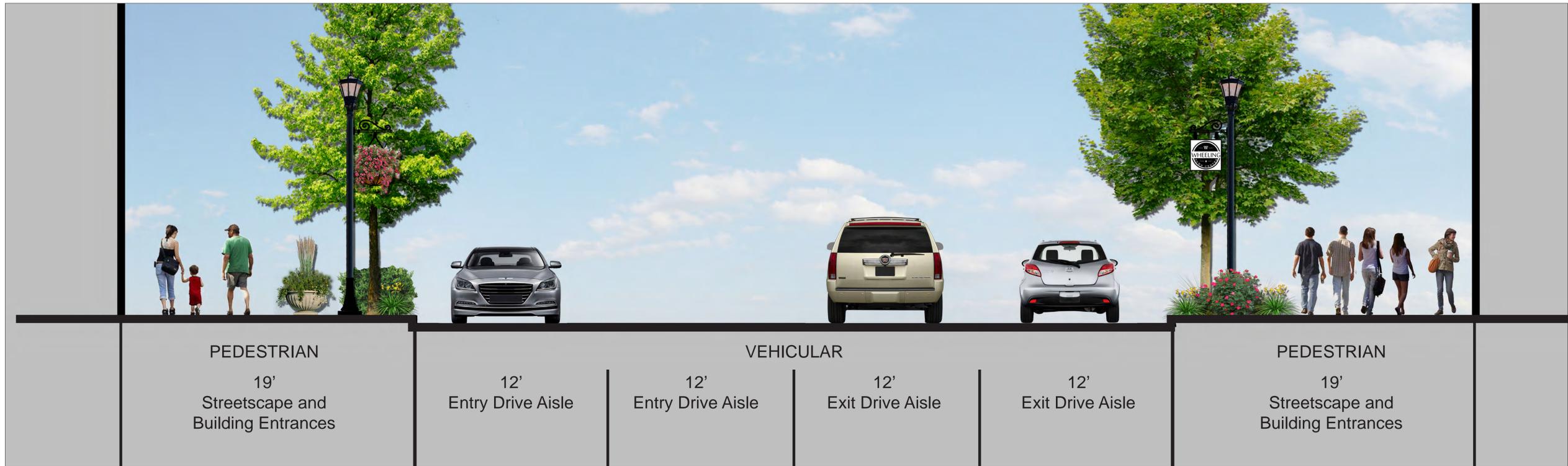
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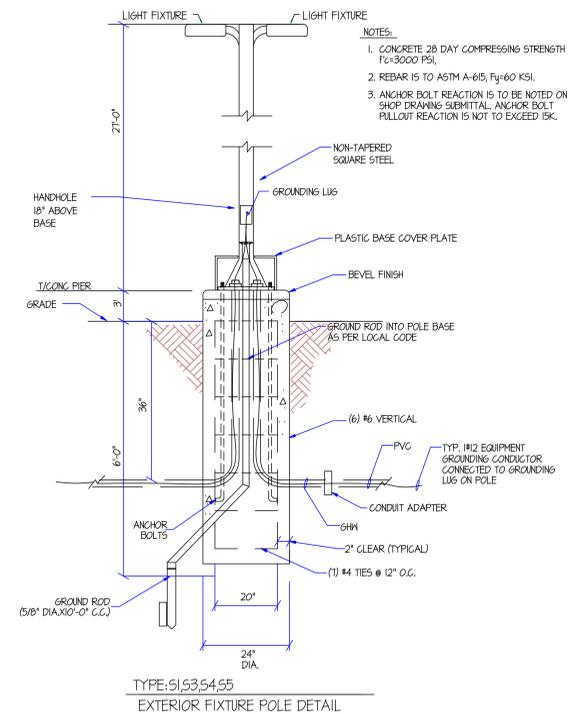
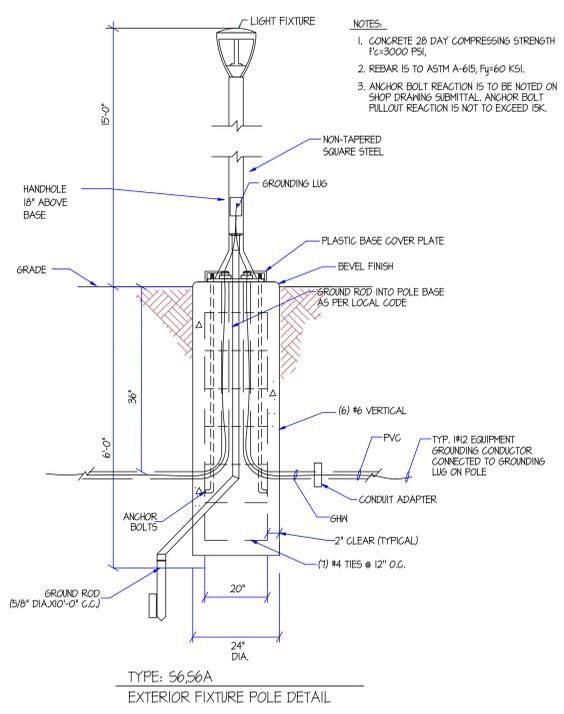
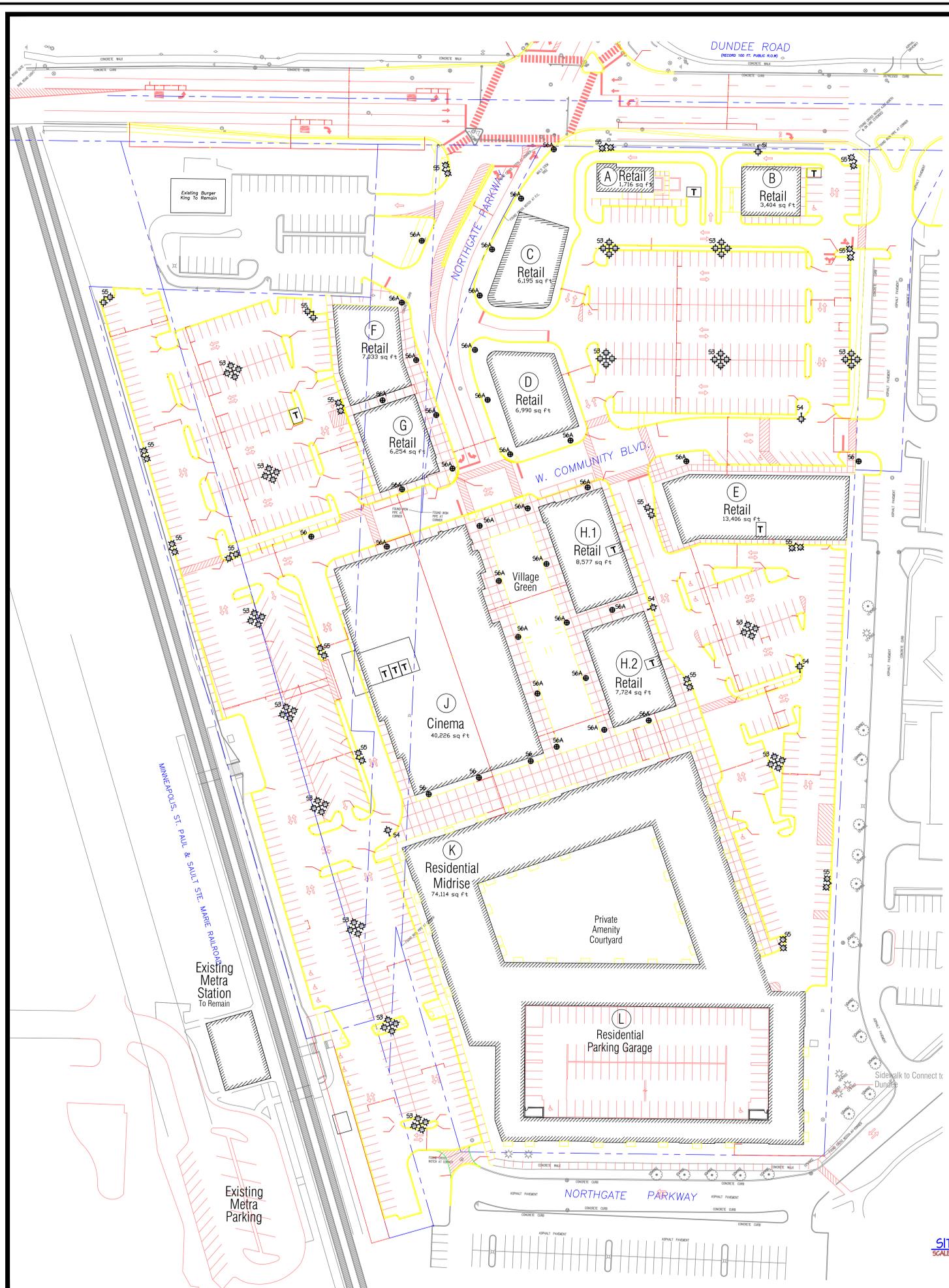
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**NORTHGATE
PARKWAY
SECTION**

NUMBER

L2.2

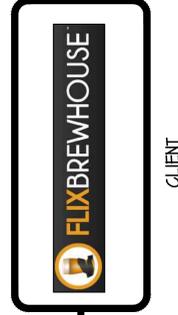


NOT FOR CONSTRUCTION



FIXTURE SCHEDULE									
TYPE	QTY	SYMBOL	DESCRIPTION	MANUFACTURER/MODEL	LAMPS		VOLTAGE	VA	NOTES
					QTY	TYPE			
S1	1	⊕	(1) LED SHIELDED FIXTURE MOUNTED 30' ABOVE GROUND	LITHONIA® DSX2 LED-800-700-40K-TSW-MVOLT-SPA-HS	(1)	LED	MVOLT	188	
S3	15	⊕	(4) LED SHIELDED FIXTURE MOUNTED 30' ABOVE GROUND	LITHONIA® DSX2 LED-1000-700-40K-TFTM-MVOLT-SPA-HS	(4)	LED	MVOLT	872	
S4	4	⊕	(1) LED SHIELDED FIXTURE MOUNTED 30' ABOVE GROUND	LITHONIA® DSX2 LED-1000-700-40K-TSW-MVOLT-SPA-HS	(1)	LED	MVOLT	218	
S5	17	⊕	(2) 90 DEGREES BACK TO BACK LED SHIELDED FIXTURE MOUNTED 30' ABOVE GROUND	LITHONIA® DSX2 LED-1000-700-40K-TSW-MVOLT-SPA-HS	(2)	LED	MVOLT	436	
S6	5	⊕	15' ABOVE GROUND MOUNTED LED TOPPER	FIXTURE MANUFACTURER TO BE DETERMINED	1	LED	MVOLT	52	
S6A	30	⊕	15' ABOVE GROUND MOUNTED LED TOPPER (2)	FIXTURE MANUFACTURER TO BE DETERMINED	2	LED	MVOLT	104	

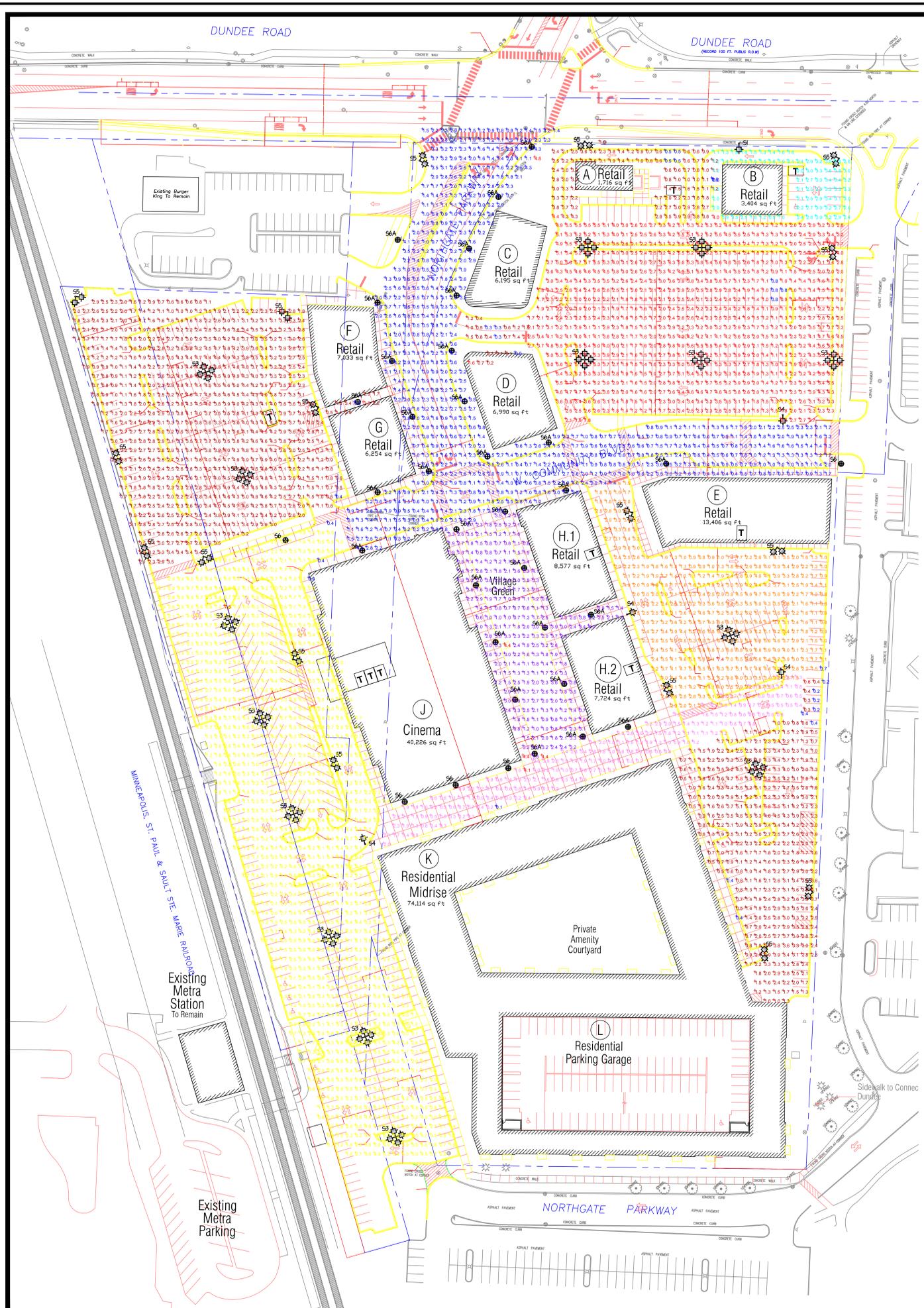
SITE LIGHTING PLAN-OPTION #4
SCALE: 1/8"=1'-0"



4-22-14 ISSUED FOR REVIEW
12-2-15 ISSUED FOR REVIEW
12-31-15 ISSUED FOR REVIEW

WHEELING TOWN CENTER
WHEELING, IL

Checked By: [Signature]
Drawn By: [Signature]
Designed By: [Signature]
E-100.C



TYPE: 56,56A

GT1615

The GT1615 is a Cast Aluminum Traditional Style 6-sided lantern. Pictured below with Clear Acrylic lenses and an Optional Glass Refractor.

FEATURES
The fixture mounts to a 3" finch with stainless steel screws. Fixture can be ordered with Textured Acrylic (TA), Prismatic Acrylic (PA) or Clear Acrylic (CA) Lenses.

ELECTRICAL		VOLTAGE
BALLAST AND WATTAGE	SOCKET	
50W/WH, 50W/HPS	MED	120V, 277V
70W/WH, 70W/HPS	MED	
100W/WH, 100W/HPS	MED	120V, 208V, 240V, 277V
150W/WH, 150W/HPS	MED	
52W LED	MED	120V, 277V
26W CFL, 32W CFL, 42W CFL	4-PIN	

OPTICS

HOUSE-SIDE SHIELD	LENS TYPE	LOUVERS	OTHER
L3 - Type 3 Glass Refractor	HSS 90	CA - Clear Acrylic	LVR1 - Small Louver
L5 - Type 5 Glass Refractor	HSS 180	PA - Prismatic Acrylic	PCL - PhotoCell
		TA - Textured Acrylic	

STANDARD FINISH
BK - Black
BT - Textured Black
SB - Statuary Bronze
GN - Green

PREMIUM FINISH
WH - White
CV - Copper Vein
GV - Green Vein
CF - Custom Finish

ORDERING SEQUENCE
GT1615 - CA / LED / 120V / L3 / BT

1605 N. Main St. Bldg. B | Pearland, TX 77581 | 37 | Ph: 281.412.2944 | www.giplco.com

TYPES: 91,93,94,95

D-Series Size 2 LED Area Luminaire

Specifications
EPA: 2.8 W/ft
Length: 40" (1016mm)
Width: 15" (381mm)
Height: 7-1/2" (190mm)
Weight (max): 39 lbs (17.5kg)

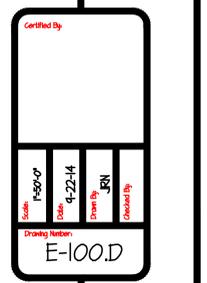
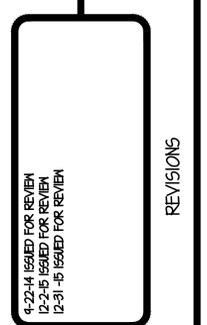
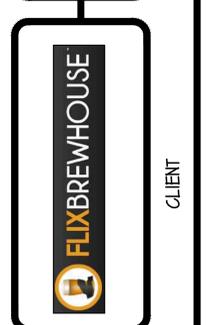
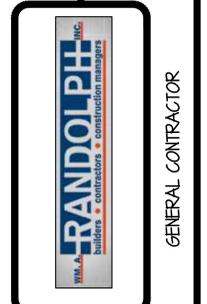
Ordering Information
EXAMPLE: DSK2 LED 80C 1000 40K T4M MVOLT SPA DBXD

Symbol	Label	Qty	Catalog Number	Description	Lamp	File	Lumens	LLF	Watts
⊕	S1	1	DSK2 LED 80C 700	DSK2 LED W/LARGE & 2 SMALL LIGHT ENGINES, (2) 700MA DRIVERS, 4000K LED, TYPE TSW OPTICS, WITH HOUSE SIDE SHIELD	LED	DSK2_LED_80C_700_40K_T5 W_MVOLT_HS	Absolute	0.95	188
⊕	S3	15	DSK2 LED 100C 700 40K T5M	DSK2 LED W/LARGE & 2 MEDIUM LIGHT ENGINES, (2) 700MA DRIVERS, 4000K LED, TYPE T5M OPTICS, WITH HOUSE SIDE SHIELD	LED	DSK2_LED_100C_700_40K_T5M	Absolute	0.95	872
⊕	S4	4	DSK2 LED 100C 700 40K T5M	DSK2 LED W/LARGE & 2 MEDIUM LIGHT ENGINES, (2) 700MA DRIVERS, 4000K LED, TYPE T5M OPTICS, WITH HOUSE SIDE SHIELD	LED	DSK2_LED_100C_700_40K_T5M	Absolute	0.95	218
⊕	S5	17	DSK2 LED 100C 700 40K T5M	DSK2 LED W/LARGE & 2 MEDIUM LIGHT ENGINES, (2) 700MA DRIVERS, 4000K LED, TYPE T5M OPTICS, WITH HOUSE SIDE SHIELD	LED	DSK2_LED_100C_700_40K_T5M	Absolute	0.95	436
⊕	S6	5	GT1615_60WLED T5	LED 16.5 LUXEON COB	LED	GT1615_60WLED-T5-IES	Absolute	0.95	52
⊕	S6A	30	GT1615_60WLED T5	LED 16.5 LUXEON COB	LED	GT1615_60WLED-T5-IES	Absolute	0.95	104

STATISTICS

Description	Symbol	Avg	Min	Avg/Min
MAIN ACCESS DRIVE-ROAD	+	1.7 fc	0.3 fc	8:1
NORTHERN PARKING LOT	+	2.9 fc	0.4 fc	7:1
RETAIL A ACCESS DRIVE	+	2.0 fc	0.5 fc	4:1
RETAIL A AND B PARKING	+	3.1 fc	0.8 fc	3:1
RETAIL B ACCESS DRIVE	+	2.4 fc	0.6 fc	4:1
RETAIL E PARKING LOT	+	3.3 fc	0.7 fc	4:1
SE PARKING	+	2.5 fc	0.4 fc	4:1
SOUTHERN PARKING	+	3.5 fc	0.4 fc	5:1
TRAFFIC TABLE ACCESS DRIVE	+	1.3 fc	0.1 fc	13:1
VILLAGE GREEN	+	2.1 fc	0.2 fc	10:1

ALL CALCULATIONS ARE TAKEN 3'-0" ABOVE GROUND



1. CALL TO ORDER

Chairman Ruffatto called the meeting to order at 6:30 p.m. on Thursday, December 10, 2015.

2. PLEDGE OF ALLEGIANCE

3. ROLL CALL

Present were Commissioners Isaakoo, Johnson, Powers, Ruffatto and Zangara. Commissioners Dorband and Sianis were absent with prior notice. Also present were Brooke Jones, Senior Planner and Mallory Milluzzi, Village Attorney.

4. CHANGES TO THE AGENDA – None

5. CITIZEN CONCERNS AND COMMENTS – None

6. CONSENT ITEMS - None

7. ITEMS FOR REVIEW

- A) [Docket No. 2015-11A&B](#)
Rios de Agua Viva Church
345 N. Wolf Road
(2015-11A) Zoning Variation to Reduce the Required Principal
Size of a Religious Assembly Use
(2015-11B) Special Use-Site Plan Approval of a Religious Assembly

See Findings of Fact and Recommendation for Docket No. 2015-11A&B.

Commissioner Johnson moved, seconded by Commissioner Zangara to recommend approval of Docket No. 2015-11A, granting a variation from Title 19, Zoning, of the Wheeling Municipal Code, Appendix A, Notes Referenced in Use Table, 4, Required principal building size for a religious assembly use, and associated sections to reduce the minimum building size from 10,000 sq. ft. to 8,320 sq. ft., for Rios de Agua Viva Church, to be located at 345 N. Wolf Road, Wheeling Illinois.

On the roll call, the vote was as follows:

Regular Meeting

AYES: Commissioners Isaakoo, Johnson, Powers, Ruffatto, Zangara
NAYS: None
ABSENT: Commissioners Dorband and Sianis
PRESENT: None
ABSTAIN: None

There being five affirmative votes, the motion was approved.

Commissioner Powers moved, seconded by Commissioner Johnson to recommend approval of Docket No. 2015-11B to grant special use approval for a religious assembly use as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections, and in accordance with the following exhibits submitted November 9, 2015, by Rios de Agua Viva Church, to be located at 345 N. Wolf Road, Wheeling, Illinois:

- Cover Letter (2 sheets)
- Site Plan A001
- Existing Floor Plan A101
- Proposed Future Floor Plan A102
- Proposed Landscape Plan L-2
- Photographs of Existing Landscaping (2 sheets)
- Plant Palette
- Photometric Plan
- Lighting Spec Sheets (7 sheets)
- Plat of Survey

And with the following conditions of approval:

1. The trash enclosure gates shall be constructed of cedar and braced to steel posts;
2. The parking lot shall be resurfaced;
3. Cross access is required for the angled parking. The angled parking may be reconfigured and reduced in number as needed if cross access is restricted;
4. Bicycle parking for four shall be provided on the sidewalk north of the accessible parking stall;
5. The maximum capacity of the worship hall shall be determined by the total number of parking spaces;
6. The future floor plan is not proposed or approved at this time;
7. Bus parking is prohibited; and
8. The rear fence shall be repaired.

On the roll call, the vote was as follows:

AYES: Commissioners Isaakoo, Johnson, Powers, Ruffatto, Zangara
NAYS: None

**Wheeling Plan Commission
Regular Meeting**

December 10, 2015

ABSENT: Commissioners Dorband and Sianis
PRESENT: None
ABSTAIN: None

There being five affirmative votes, the motion was approved.

Commissioner Powers moved, seconded by Commissioner Johnson to close Docket No. 2015-11A&B. The motion was approved by a voice vote.

- B)** [Docket No. 2015-12](#)
Space Self Storage
2500 E. Hintz Road
Concept Review of a Text Amendment, Rezoning and Special Use to Permit a
Self-Storage Facility in the B-3 General Commercial and Office District

Chairman Ruffatto called Docket No. 2015-12 on December 10, 2015. Present were Commissioners Isaakoo, Johnson, Powers, Ruffatto and Zangara. Commissioners Dorband and Sianis were absent with prior notice. Also present were Brooke Jones, Senior Planner and Mallory Milluzzi, Village Attorney.

Mr. Dan Shapiro, Attorney, Mike and Ken Schaeffges, Schaeffges Brothers, Inc., 851 Seton Court, 2A, Wheeling, IL and Al Maiden, Rolf C. Campbell & Associates, 910 Woodlands Parkway, Vernon Hills, IL were present.

Mr. Shapiro explained they were present for a workshop on a multi-layered zoning request. They want to get feedback before moving forward.

Mr. Shapiro stated that Space Self Storage had been in its current location for many years. It was brought in by annexation over 20 years ago. It has been operating as a good corporate citizen of the Village. He explained it was a good time to borrow money with the low interest rates. In the event, the Schaeffges family wants to refinance or do anything with the property that requires a bank now would be the time. They are currently legal, non-conforming so it would be very difficult to get a loan or refinancing. He explained the purpose of coming before the Commission was to gage the Plan Commission's reaction to a future request to get them to be legal conforming.

Mr. Shapiro looked at the zoning in the area. He mentioned some of the zoning was residential and some of the uses were more industrial. This property is in the residential zone. There is no other reasonable conforming way except for seeking a text amendment to a B-3 to allow for a self-storage facility and allow for the special use application. The Village would still be able to have control over the application and the site and the conditions.

Mr. Mike Schaeffges stated he was one of the owners of Space Self Storage which has been a family business for 28 years. He explained they have been receiving a lot of requests from their clients for more space and they are currently full. They are looking to expand their facility as shown and can't get financing with the current non-conforming status. They also want to beautify it. They have been maintaining the building for 28 years and it is well-maintained. They have not been able to beautify

the back.

Mr. Shapiro clarified they were not seeking approval of the site plan but provided future plans for a new building surrounded by significant landscaping.

Mr. Maiden reported he had assisted three villages in the last two years with the same type of amendments regarding self storage facilities. He welcomes the opportunity in a workshop type format to see the Plan Commission's recommendations. He reviewed the existing zoning and noted that self storage facilities were permitted in the I-3 and I-4 general and heavy industrial areas. He does not think industrial would be appropriate in the proposed location. He noted the area was not shown as a commercial use in the comprehensive plan but was shown as single-family. In his opinion, it was just reflecting the existing zoning condition. He referred to the comprehensive plan and noted there were areas of pink that were shown along the corridor starting west on Hintz Road to Buffalo Grove Road and continuing to Wheeling Road. There are various spots that are shown as commercial on the comprehensive plan. The zoning has similar zoning patterns in the area. There is B-3 along Hintz Road and Buffalo Grove Road and to the east. He thinks a B-3 would be an appropriate classification to consider for a zoning amendment to allow this type of use as a special use. They looked at the uses in appendix A and noticed there were footnotes to the various uses listed. He thought there were 14 different qualifications provided for select uses. They are looking at doing something similar.

Mr. Maiden explained there would be secondary access from a local street for emergency purposes. They added no more than three buildings would be on the plan for up to 5 acres. They looked at what could be achieved in regard to a site plan. They looked at the northern property and the circulation through the building. This is proposed as a potential building with one lower level below grade and two levels above. They aligned the entrance into the building to be aligned with the assisting aisles between buildings A and B and between buildings B and some of the outside storage. The primary access would remain the same on Hintz Road with emergency access provided off of Jackson. They looked at how it could be designed to have appropriate buffers considering the surrounding uses. They looked to the west with the R-4 residential area and were able to achieve a 90' setback in the area for this type of use. It would exceed the typical zoning standard if there was something they want to consider adding it into the Appendix A footnote. They are trying to give as much buffer from the residential side as possible. They also had a 50' setback on the north and an extensive setback on the Jackson Drive side because they were trying to make it align with the existing buildings.

Mr. Maiden referred to the landscape plan. The property owners wanted to have a concept that had a very multi-layered landscape buffer. Evergreen trees are proposed around the peripheral of the property to start to develop a screen. Internal to it, there would be some mixing in of shade trees, ornamental trees to give a variety in height. There is another buffer of foundation plantings for evergreens in that area. They would be from 6-8' at installation. They did not do any engineering plans but conceptual showed how they might provide areas for dry or wet detention. They also showed the proposed landscape plan in relation to the building elevations. They showed landscaping at installation size and future size.

Mr. Shapiro highlighted a part of his letter. He noted the properties were annexed in 1988. There have been no problems with the neighbors. The ordinance says that the Village Zoning

Administrator shall be directed to amend the official zoning map of the Village of Wheeling to reflect the zoning change by this correspondence. The zoning map was changed to reflect that the properties were annexed but there was never anything to reflect on the zoning map. There was a suggestion that the zoning map back in 1988 should have probably shown the appropriate zoning for the property as B-3 but it wasn't done.

Chairman Ruffatto referred to the new members on the Commission. He asked if Staff could review the definition of text amendment in general. Ms. Jones explained a text amendment was a change to the zoning code so what they were proposing could be allowed and would be considered legal conforming if the property was rezoned and they received a special use. Currently, the zoning code does not allow for self storage facilities in the R-1 district or B-3 district. The property is zoned R-1 and they would need to rezone it as a map amendment to B-3 and then a text amendment to allow a self storage facility as a special use in the B-3 district.

Chairman Ruffatto questioned if it would then allow a self storage to be in any B-3 within the Village. Ms. Jones explained they were also considering some additional restrictions. Mr. Shapiro explained they were proposing language so it could be a special use in the B-3 with some additional specifications so it wouldn't be popping up in all the B-3s. It would only apply to this area in the foreseeable future.

Ms. Milluzzi explained the use table would be amended in the B-3 to list self storage facilities, special use under the B-3 with a footnote similar to the current use table with restrictions. She suggested when thinking about the text amendment to the B-3, it would apply to all B-3s in the Village.

Ms. Jones explained Staff was maybe considering that the self storage facility in the B-3 district would be limited to properties that were single-use so you wouldn't combine self storage with a retail use. She noted there were some B-3 districts where it could happen. Chairman Ruffatto could see someone tearing down a small shopping center to put in a self storage facility. Mr. Shapiro explained they drafted the text amendment so it wouldn't happen. Mr. Maiden explained they tried to relate it to the roadways so it wouldn't qualify. They also tried to add other things such as size, number of buildings and other things. He agreed the B-3 shouldn't be open completely to allow self storage facilities. He felt there was a greater need for these types of facilities with individuals and families.

Chairman Ruffatto suggested B-2 which is less restricted in the Village. He felt there were fewer B-2 areas in the Village. Ms. Jones explained B-2 was generally slightly less intense than the B-3. It is considered a neighborhood commercial district and very similar to B-3 but not on a main roadway. Chairman Ruffatto noted there were very few B-2 areas. He questioned if it had been considered. Mr. Maiden explained the pattern along the Hintz corridor was B-3. He doesn't want any concern about someone saying it was spot zoning since there was no B-2 anywhere in the corridor. Ms. Milluzzi agreed to look into the issue. Mr. Shapiro felt it was a great suggestion and would look into it as well.

Chairman Ruffatto questioned if there was another self storage in B-3. Ms. Jones confirmed there were some on Milwaukee Avenue both north and south of Hintz Road.

**Wheeling Plan Commission
Regular Meeting**

December 10, 2015

Chairman Ruffatto questioned if the Commission was in agreement with letting them proceed with changing the comprehensive plan or text amendment. Ms. Milluzzi explained it would definitely need to be a text amendment. Commissioner Powers noted they would need to return regardless if it goes to B-2 or B-3. Chairman Ruffatto wants to prevent someone from taking down a shopping center in order to build a storage facility. He is personally on the fence about it. He questioned the proposed additional wording. Ms. Jones explained Staff was concerned about combining non-compatible uses with the self storage like office or retail on the same property. Mr. Shapiro referred to the limiting factors they proposed: a single user on a lot no larger than 5 acres with primary access to and from a roadway that is classified as a major arterial and has a secondary emergency access from a local street and consists of no more than three buildings.

Commissioner Powers questioned if the car care center was part of it. Mr. Shapiro confirmed it was not part of the discussion. The discussion includes the parcel on the north. Ms. Milluzzi explained they would seek to rezone both parcels. The text amendment is to the Code that would apply to that zoning district. The rezoning would apply to both parcels.

Commissioner Powers questioned how it would come to the Commission. Ms. Jones explained the petitioner could propose that both properties be rezoned or the Plan Commission and Board could consider that only the existing facility be rezoned. Mr. Shapiro stated it would be their preference to bring it all together since there was a risk of it not working if one was approved and one wasn't approved.

Chairman Ruffatto doesn't care right now about the landscaping, building, etc. He explained if the whole thing was rezoned B-3, they could come in with a completely different plan for the property to the north.

Chairman Ruffatto personally has a concern with changing the zoning for both properties. He is OK with the existing property but has a concern with the property to the north especially since it is next to R-1. Mr. Shapiro explained they could work on further limiting language.

Chairman Ruffatto wants it to be more restrictive. He wonders if the other property should be sold off and made residential instead of being B-3.

Commissioner Zangara explained if it was just one property then they would only be allowed one more building.

Chairman Ruffatto took a poll for the text amendment for both parcels.

Commissioner Zangara: OK

Commissioner Powers: wants northern most parcel kept as residential

Commissioner Isaakoo: OK

Commissioner Johnson: OK

The consensus was to consider it as one parcel.

Ms. Milluzzi clarified when it comes to the Commission it would be three separate parcels so the vote to rezone would be done separately.

Mr. Ken Schaeffges explained the two large buildings were on one parcel, the vehicles were another parcel.

Chairman Ruffatto questioned if they could put three buildings where the truck storage is located. Commissioner Zangara suggested combining the three pin numbers into one pin number so it would be all one parcel. Chairman Ruffatto thought it would be difficult.

Chairman Ruffatto felt there was concurrence to combine them and go with a B-3. He suggested working with Staff on the text amendment.

Commissioner Johnson questioned if the Village would want to rezone Wheeling Auto Care since they were also legal, non-conforming. Mr. Shapiro stated they were in the process of working on it.

8. APPROVAL OF MINUTES – November 12, 2015 and November 19, 2015 (includes partial Findings of Fact for Docket No. 2015-5)

Commissioner Powers moved, seconded by Commissioner Zangara to approve the minutes dated November 12, 2015 as presented. The motion was approved by a voice vote. Commissioner Isaakoo abstained.

Commissioner Powers moved, seconded by Commissioner Johnson to approve the minutes dated November 19, 2015 as presented. The motion was approved by a voice vote. Commissioner Isaakoo abstained.

9. OTHER BUSINESS

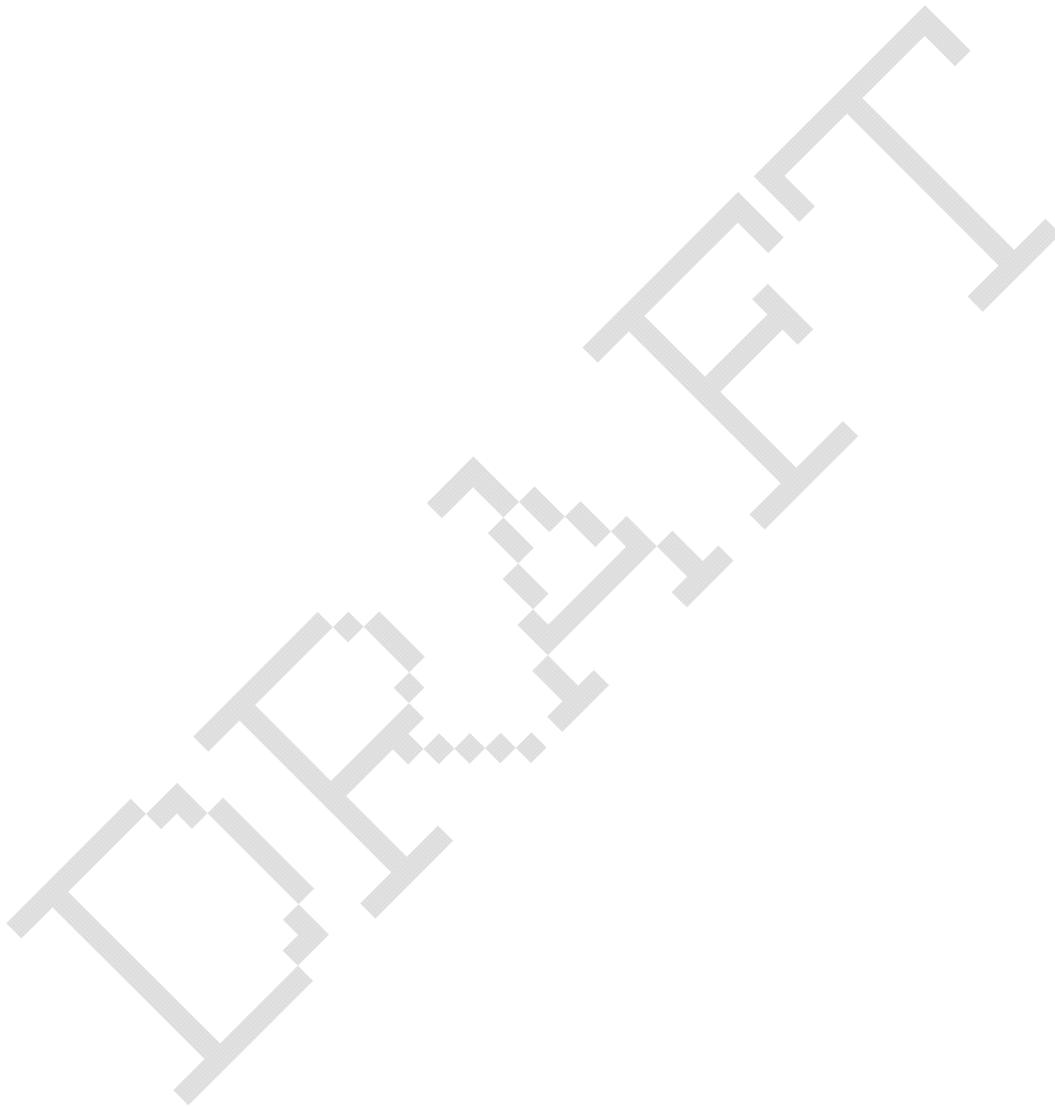
Commissioner Zangara had drawn his vision for the Wheeling Town Site on a copy of the site plan and distributed copies at the meeting. It showed the retail E smaller in order to get the second access, changed the parking and added more green space. The parking garage is in the front with three decks.

10. ADJOURNMENT

Commissioner Powers moved, seconded by Commissioner Johnson to adjourn the meeting at 9:15 p.m. All were in favor on a unanimous voice vote and the meeting was adjourned.

Respectfully submitted,

Steve Powers, Secretary
Wheeling Plan Commission



**DRAFT
FINDINGS OF FACT
AND RECOMMENDATION**

DOCKET NO. 2015-11A&B

To: Village President and Board of Trustees

From: Wheeling Plan Commission/Sign Code Board of Appeal

Re: Docket Nos. 2015-11A&B
Rios de Agua Viva Church
345 N. Wolf Road
(2015-11A) Zoning Variation to Reduce the Required Principal
Size of a Religious Assembly Use
(2015-11B) Special Use-Site Plan Approval of a Religious Assembly

Docket No. 2015-11A&B Silverio Mendez, Pastor for Iglesia Cristiana Rios de Agua Viva, tenant, is seeking the following actions to establish a religious assembly use at 345 N. Wolf Road:

2015-11(A) Variation from Title 19, Zoning, of the Wheeling Municipal Code, Appendix A, Notes Referenced in Use Table, 4, Required principal building size for a religious assembly use, and associated sections; and

2015-11(B) Special Use-Site Plan Approval as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections, to establish a religious assembly use in the MXC Commercial Residential Mixed Use District.

Chairman Ruffatto called Docket No. 2015-11A&B on December 10, 2015. Present were Commissioners Isaakoo, Johnson, Powers, Ruffatto and Zangara. Commissioners Dorband and Sianis were absent with prior notice. Also present were Brooke Jones, Senior Planner and Mallory Milluzzi, Village Attorney.

Commissioner Powers read the following statements aloud.

A zoning variation is intended to be a method of adjustment to equalize regulations where Title 19 of the Village of Wheeling (Zoning) has created an unnecessary hardship. A variation is designed to allow affected property owners the same rights and privileges that others enjoy in the same zoning district. In order to be granted a variation a petitioner is required to demonstrate through testimony to the Plan Commission at the public hearing why their request meets the conditions of the village code including, but not limited to, how their individual situation is unique or unusual. Prior to the public hearing the petitioner provides written statements meant to show that their request for variation meets the standards established in Title 19. The Commission Chairperson will typically direct that these statements be entered into the record without a full reading of them at the hearing. Based upon the testimony and supporting materials submitted, the Plan Commission will make findings in support of, or against, the petitioner's testimony and report those findings to the Village Board.

A zoning Special Use, as defined in Title 19, of the village of Wheeling (Zoning), is a use of parcel of land that requires review and consideration before approval due to circumstances or effects on the surrounding properties that may adversely affect them. In order to be considered for a special use

the petitioner is required to demonstrate through testimony to the Plan Commission at the public hearing why their request meets the conditions of the village code including, but not limited to, how the proposed use will not damage the enjoyment or use of the surrounding properties. Prior to the public hearing the petitioner provides written statements meant to show that their request for a special use meets the standards established in Title 19. The Commission Chairperson will typically direct that these statements be entered into the record without a full reading of them at the hearing. Based upon the testimony and supporting materials submitted, the Plan Commission will make findings in support of, or against, the petitioner's testimony and report those findings to the Village Board.

Mr. Dan Shapiro, Attorney, Pastor Silverio Mendez, Mr. Sean Ehlke, Architect, Ehlke Lonigro, 224 S. Milwaukee Avenue, Wheeling, IL and Ms. Jamie Jaderholm, Landscaper, 48 E. Hintz Road were present and sworn in.

Mr. Shapiro explained the special use was for a church to operate at 345 N. Wolf Road, Wheeling. The building would occasionally be used for church services, classroom or counseling. There would be one service on Sunday morning and then very limited hours during the week. It would also be available on an emergency basis for counseling services. The church had approximately 60 people interested in joining the church. It is their desire to grow but the growth is limited by the site and parking. The parking only allows for a maximum of 90 people. The site plan includes unassigned space that won't be used. They would return in the future for phase two if it was needed. The space is currently vacant and won't be used. Mr. Shapiro referred to the circulation in the parking spaces. They had discussion with the neighbor to the north whose building is currently up for sale so it is difficult for them to put anything in writing since they can't speak on behalf of a new owner. The neighbor to the north has given verbal permission for them to use the driveway as cross access if necessary. The neighbor to the north also has no objection to the request.

Mr. Shapiro noted they were a couple hundred feet short of the minimum size for such a facility. He explained they did not create it and they have tried to address the variation standards and the hardship if they don't get the variation.

Mr. Sean Ehlke referred to the plat of survey and explained their task was to prepare the documentation for the special use request. They have been involved since October. He thinks of it as a reuse and repurposing of the site and building. The original building is 8,320 square feet and was previously a mixed use of business and mercantile. The use for the church would change it to an assembly A3 use and an attorney's office as a B classification in a portion of the office area. The current site has 35 parking spaces with no handicap spaces or trash enclosure included in the count. They are proposing to try and maintain the 35 parking spaces but to incorporate a handicap parking space and trash enclosure on the property. The seven angled parking stalls in the northwest corner do not meet the current zoning ordinance requirement for 60 degree parking. The dimension for the sidewalk to the edge of pavement is 33' and it did not meet the current zoning ordinance requirement. The requirement is 40'. The property line and the edge of the pavement is right there so they are stuck with the 33' dimension.

Mr. Ehlke explained they were proposing to restripe everything to get the 35 spaces with the

handicap stall and trash area. They want to stripe it at 45 degree spaces, which drops the requirement from 40' to 35' in the zoning ordinance. They are still 2' short. He explained it was an existing condition but didn't know how to solve in any other way.

Mr. Ehlke referred to Staff's comment that the pavement should be resurfaced in addition to the striping. He stated that if the Church receives the special use and occupancy in the spring they would look to do the remedial work to the paving area and to have it look like new along with the striping. They are proposing additional striping with the cross hatch lines to help give a better sense on the property of the parking and traffic flow. Every parking space will get a wheel stop at the end of it. At the southeast corner, they will remove a stall and add a trash enclosure. The current trash situation for the building includes dumpsters that are wheeled out into the center of the aisle for a truck to pick up.

Mr. Ehlke stated that the church would utilize all of the parking spaces when they have services. The one service is on Sunday morning. If they grow, they may want to add a second service in the future. He feels the parking in the lot will only be filled at the time of service. The remaining time, the church staff and attorney's office has approximately 15 people in the building at the maximum business hours during Monday – Friday (9:00a – 6:00p).

Mr. Ehlke referred to the photometric plan. The existing lighting on the building does not meet the current code and ordinance. There are two light fixtures on the south and west walls of the upper portion of the south end. They have dropped down globe lens with light scattered. There is no cutoff with the existing fixtures. They are proposing to remove those fixtures and add six new LED fixtures on the building to adequately light the parking and walk areas. On the south wall, there will be two new "A" fixtures mounted at 18'. The one existing fixture is mounted at 14' and the top of the wall is 18'. They will mount two separate light fixtures on that wall at the parapet level to get the distribution needed to light the pavement. There are two other "A" fixtures on the west wall, southern portion of the building. They will add two additional LED fixtures, the "B" fixtures, that will be on the west wall of the lower office area that faces Wolf Road.

Mr. Ehlke referred to the floor plan. It showed the proposed layout for the congregational seating area with 88 seats. The seats are intended to be moveable, stackable seats. The platform area at the south end of the space is built to be removed as needed. The northeast corner by the exit door is for A/V equipment. It has a raised platform with half-high walls for the A/V equipment. The congregational seating area represents about 2,865 square feet. Directly north of it, is unassigned (2,876 square feet) open space. The office area to the west side of the north end has an existing kitchen area, walk-in cooler, exhaust hood over a residential stove and microwave. The south includes existing toilets and utility services. The hatched area to the south is the attorney's office that is 861 square feet.

Mr. Ehlke referred to the A102 drawing. The plan was provided in response to a Staff request to show what might happen. They are not asking for approval at this time. The drawing is concept only and is to show Pastor Mendez's vision. They look to add toilet facilities and classrooms in the future.

Ms. Jamie Jaderholm, Landscaper, referred to the existing landscape. There are some existing trees at the corners of the building. On the far northeast corner, there are some Siberian elm trees and American elm trees with some small maples on the opposite corner. There are some intermediate size and larger yews along with arborvitaes in the three beds surrounding the entrances of the building. She mentioned there was a lot of river rock. The landscaping has not been maintained for a long time.

Ms. Jaderholm referred to the proposed plan. The plan includes a variety of color and diversity to the beds. They want to plant some boxwoods, catnip and Russian sage. They want to plant hearty perennials and shrubs that can exist and look well in all seasons and handle salt in the winter. In the meantime, they will trim the existing landscaping.

Ms. Jaderholm provided a front elevation of the trash corral in the far southeast corner.

Pastor Silverio Mendez and Elsa Mendez came forward. They are proposing an assembly. He stated he had lived in Wheeling for many years and then moved to Arlington Heights. They are back living in Wheeling for 7 years and saw a necessity to do something for the community. He resigned from his job and started working on a chapel in Palatine and started to reach out to people. They counsel people on marriages and drug use and are both certified counselors. He read their mission statement aloud. He explained their mission was to help anyone, not only Hispanics.

Ms. Elsa Mendez thanked the Commission for their support.

Mr. Shapiro reported that Pastor Mendez spoke with the neighbors to see if anyone had a concern regarding the proposed use. Pastor Mendez stated that there was only one neighbor who voiced objection but he was not present at the meeting.

Mr. Shapiro reported the limited hours on Monday, Tuesday and Friday was from 6:00 p.m. – 10:00 p.m. and on Sunday from 9:00 a.m. to 1:00 p.m. The 6:00 p.m. – 10:00 p.m. weekday hours were for religion classes in the ceremonial room.

Mr. Shapiro reported the standards were addressed for both the variance and special use. He misspoke earlier; the difference is 1,700 square feet and not 200 square feet.

Mr. Shapiro referred to the audience and asked them to raise their hands in support of the church's request. The crowd in the audience raised their hands to show support.

Chairman Ruffatto asked if anyone from the audience wanted to speak. No one came forward.

Commissioner Johnson expressed concern regarding the cross access with the neighbor when the building sells. He asked if there was a contingency plan. Mr. Shapiro explained they would approach the new owner when the building sells and ask for cross access. If they don't receive cross access from the new owner, they could angle the parking spaces a little differently to allow more breathing room. If it doesn't work, they could lose 3-4 spaces and create another 3 spaces in a location noted on the plan near the in and out. Chairman Ruffatto questioned how parking could be

located near the front. Mr. Ehlke confirmed they could add 3 spaces but would need to lose 4 spaces in order to do it. Mr. Shapiro agreed to limit the seats if they reduced the number of parking spaces. The congregation would be limited to about 78 versus 90. They would agree to a condition.

Commissioner Johnson questioned the size of the previous Northfield church that moved from Northgate. Ms. Jones did not recall but thought it was over 10,000. Mr. Shapiro handled it but didn't recall the size.

Commissioner Johnson referred to the Fire Department's comments. Mr. Shapiro was aware of the comments.

Commissioner Johnson referred to the photograph of a bus and van parked in the back of the building. Mr. Shapiro confirmed the church owned the vehicles but they would not be used. Chairman Ruffatto explained the vehicles should not be parked in the back. Pastor Mendez confirmed they would remove them.

Commissioner Johnson referred to the existing lights on the east side of the building. Mr. Ehlke confirmed the existing lights adjacent to exit doors would remain and the two flood lights will also remain and can be adjusted down toward the pavement.

In reply to Commissioner Johnson's question, Mr. Ehlke stated the ceiling height of the worship area was about 14.8".

Commissioner Zangara questioned the owner of the existing fence. From the audience, Mr. Paul Doetch (107 Berkshire) confirmed the fence belonged to the property. Commissioner Zangara mentioned the fence was in disrepair. Mr. Doetch confirmed it needed to be straightened out. Commissioner Zangara questioned if bollards could be added so cars don't back into it.

Commissioner Zangara was not concerned about the angle parking since there was grass in the front.

Commissioner Zangara questioned where they planned to put the snow. He questioned the number of spaces they would lose. Mr. Shapiro explained the snow could be put in the grassy area or on the property which would lose two spaces.

Commissioner Zangara referred to the preschool located across the street. He questioned if they had contacted them regarding using their parking lot. Mr. Shapiro stated he had not but thought it was a good idea.

Commissioner Zangara questioned if the congregation got larger, would they consider adding a second service. Mr. Shapiro explained there would be two services and would be staggered a half hour apart.

Commissioner Zangara questioned the plan for the overhead door. Mr. Ehlke explained it was not used since it was in their worship area. However, there is the ability to open it if they need to bring in furniture. The inside of the door would have a drywall moveable door.

Commissioners Zangara and Powers had the same comments as the Fire Department.

Commissioner Powers referred to the proposed stockade fence in the trash area. He explained the Commission typically asks for a board-on-board enclosure. He questioned the proposed material for the trash enclosure. Ms. Jaderholm explained they did not have anything specific but agreed to use cedar.

Commissioner Powers felt the landscaping plan was great. He questioned if the existing rock in the landscaping would be replaced. Ms. Jaderholm confirmed they would replace the river rock with mulch.

Commissioner Powers questioned if the overgrown trees in the front would be removed. Ms. Jadersholm explained one of them was an overgrown yew and the other one was an overgrown arborvitae on the far side. They will keep the arborvitae since it looked healthy. All of the yews would be removed.

Commissioner Powers questioned if there was a water source in the front. Ms. Jadersholm confirmed there was a water source on the side bed area. She explained she had chosen plants that were really hardy and could withstand little water. Chairman Ruffatto stated that Village Code indicates that it must be irrigated.

Mr. Ehlke noted the bike parking would be in a paved area against the wall and sidewalk and would be screened with the landscaping. Commissioner Powers wanted it added to the plans.

Commissioner Powers referred to the condition of the parking lot. He wanted to leave the decision to Staff. He felt it looked like it needed to be resurfaced. He agrees something needs to be done with it but was uncertain as to the solution. Mr. Shapiro agreed to work with Staff to find a reasonable solution. Chairman Ruffatto mentioned that Engineering wants it to be resurfaced but the definition needs to be determined since Engineering was not present. He suggested adding a condition.

Mr. Shapiro addressed the irrigation. He noted hardy plants were chosen and the landscaping was very modest. He questioned if the Commission would consider waiving the built-in irrigation system. Chairman Ruffatto noted it was Code but agreed to consider it in a poll.

Commissioner Powers liked Commissioner Zangara's comment about contacting the preschool for extra parking.

Commissioner Powers questioned if the church was previously in another location. Pastor Mendez explained they would visit people in their homes.

Commissioner Powers questioned the plan for the moveable chairs and stage. Pastor Mendez explained they could move the chairs out if needed. Commissioner Powers questioned if the petitioner was limited to what they could do in the worship area. Mr. Shapiro explained the other

uses needed to be related to the religious use. There is no intent to have events there that were unrelated to religion.

Commissioner Isaakoo referred to the neighbor that was upset about the use. He questioned the reason. Pastor Mendez explained the neighbor was upset about music and that was the reason they decided to cover the overhead door. The neighbor was also concerned about garbage. Pastor Mendez explained there was going to be a trash enclosure. He agreed to add the enclosure immediately after approval.

Chairman Ruffatto was uncertain if a verbal agreement for the cross access would work. He asked for Staff's opinion. He asked if the current owner could provide something in writing. Mr. Shapiro agreed to make the effort. Ms. Milluzzi suggested imposing the more restrictive condition now that without any cross access agreement existing as of this date, the number is capped assuming the four lost spaces. The number of people would be capped at 78 from 90 with the four lost spaces. If they were able to get the cross access agreement in the future with the new owner, they could come in with the cross access agreement and ask that the condition be modified to be increased to the 90. Chairman Ruffatto questioned if the parking stayed or did they need to add the three spaces now and remove the seven spaces. Ms. Jones explained Staff's concern was not the 45 degree versus the 60 degrees because it was existing; they think the existing parking is just fine. Their only concern is the access. Chairman Ruffatto suggested giving them a timeframe to get a written verbal agreement with the existing owner and to keep the parking with a time limit since they need it.

From the audience, Mr. Doetsch stated he was familiar with the building and had leased it out in 1988. He confirmed the cross drive was there and had been used since 1988. He stated he spoke with an attorney who was involved with the original owner and since they used it for so long it was considered an easement by prescription. Ms. Milluzzi agreed it could be possible but it would need to be established if anyone challenged it in Court. Chairman Ruffatto wants something established. Ms. Jones was preparing text for tonight, "The angle parking may be reconfigured and reduced in number as needed if cross access is restricted." Chairman Ruffatto felt the easement by prescription would be a great way to solve the problem temporarily. When the building is sold, a cross access agreement would need to be in place and if not, it would need to be reconfigured. Ms. Jones suggested adding a condition that "cross access is required for the angle parking and that the angled parking may be reconfigured and reduced if cross access is denied or restricted". Everyone was ok with the proposed.

Commissioner Powers questioned if the cross access was recorded with the property. Ms. Milluzzi agreed it would be recorded with the property.

Ms. Jones prepared the following condition:

Cross access is required for the angle parking. The angle parking may be reconfigured and reduced in number as needed if cross access is restricted.

Chairman Ruffatto questioned if another condition should be added for when the six months after the adjacent property is sold if cross access agreement is not finalized, the petitioner needs to return. Ms. Milluzzi explained it allows them to continue until someone actually denies them cross access.

Chairman Ruffatto questioned the use of the unassigned space. Mr. Shapiro explained it would not be used. He was agreeable to add it as a condition if needed. Chairman Ruffatto suggested removing the proposed floor plan from the packet.

Chairman Ruffatto referred to the proposed hours. He questioned if there would be Staff present during the day. Pastor Mendez explained the people from the church would only be present during the day for counseling. The youth group is on Monday, Tuesday is for prayer and Friday is for family get-togethers from 7:00-9:30 p.m. The building is closed during the day. The law office is open with 5-7 people. Chairman Ruffatto felt the Friday nights might get crowded. He expressed concern for the parking on a Friday night. Mr. Shapiro explained the parking would probably drive the number of people vice versa. He did not foresee a problem. Chairman Ruffatto mentioned that parking on Wolf Road could be an issue. He questioned if no parking signs could be used.

Chairman Ruffatto reminded the petitioner that the kitchen could only be used for their functions. Mr. Shapiro concurred.

Chairman Ruffatto questioned what could be done regarding determining the definition of resurfacing since Engineering was not present at the meeting. Ms. Jones suggested making a recommendation that it be reconsidered upon further review. Chairman Ruffatto wants to avoid just patching it since it was in such bad shape. Mr. Shapiro explained there were a couple of ways it could be interrupted. Ms. Jones suggested taking a poll. Chairman Ruffatto felt the consensus was that it should be resurfaced. Ms. Jones noted that was also Staff's recommendation. Mr. Shapiro agreed to work with Engineering to understand the definition. Chairman Ruffatto felt a condition was not necessary since it was determined that it needed to be resurfaced. If the petitioner disagrees, they would need to return to the Commission. Ms. Jones suggested adding a condition that the parking lot shall be resurfaced.

Chairman Ruffatto took a poll for requiring an irrigation system.

Commissioner Johnson: not in favor
Commissioner Isaakoo: not in favor
Commissioner Powers: in favor
Commissioner Zangara: not in favor
Commissioner Chairman: not in favor

It was 5:1 in favor of not requiring an irrigation system.

Commissioner Johnson moved, seconded by Commissioner Zangara to recommend approval of Docket No. 2015-11A, granting a variation from Title 19, Zoning, of the Wheeling Municipal Code, Appendix A, Notes Referenced in Use Table, 4, Required principal building size for a religious assembly use, and associated sections to reduce the minimum building size from 10,000 sq. ft. to 8,320 sq. ft., for Rios de Agua Viva Church, to be located at 345 N. Wolf Road, Wheeling Illinois.

On the roll call, the vote was as follows:

AYES: Commissioners Isaakoo, Johnson, Powers, Ruffatto, Zangara
NAYS: None
ABSENT: Commissioners Dorband and Sianis
PRESENT: None
ABSTAIN: None

There being five affirmative votes, the motion was approved.

Commissioner Powers moved, seconded by Commissioner Johnson to recommend approval of Docket No. 2015-11B to grant special use approval for a religious assembly use as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections, and in accordance with the following exhibits submitted November 9, 2015, by Rios de Agua Viva Church, to be located at 345 N. Wolf Road, Wheeling, Illinois:

- Cover Letter (2 sheets)
- Site Plan A001
- Existing Floor Plan A101
- Proposed Future Floor Plan A102
- Proposed Landscape Plan L-2
- Photographs of Existing Landscaping (2 sheets)
- Plant Palette
- Photometric Plan
- Lighting Spec Sheets (7 sheets)
- Plat of Survey

And with the following conditions of approval:

1. The trash enclosure gates shall be constructed of cedar and braced to steel posts;
2. The parking lot shall be resurfaced;
3. Cross access is required for the angled parking. The angled parking may be reconfigured and reduced in number as needed if cross access is restricted;
4. Bicycle parking for four shall be provided on the sidewalk north of the accessible parking stall;
5. The maximum capacity of the worship hall shall be determined by the total number of parking spaces;
6. The future floor plan is not proposed or approved at this time;
7. Bus parking is prohibited; and
8. The rear fence shall be repaired.

On the roll call, the vote was as follows:

**Findings of Fact and
Recommendation**

DOCKET NO. 2015-11A&B

AYES: Commissioners Isaakoo, Johnson, Powers, Ruffatto, Zangara
NAYS: None
ABSENT: Commissioners Dorband and Sianis
PRESENT: None
ABSTAIN: None

There being five affirmative votes, the motion was approved.

Commissioner Powers moved, seconded by Commissioner Johnson to close Docket No. 2015-11A&B. The motion was approved by a voice vote.

Respectfully submitted,

Jim Ruffatto, Chairman
Wheeling Plan Commission/
Sign Code Board of Appeals

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FOR APPROVAL ON 01.14.2016**

1. CALL TO ORDER

Chairman Ruffatto called the meeting to order at 6:30 p.m. on Thursday, December 17, 2015.

2. PLEDGE OF ALLEGIANCE

3. ROLL CALL

Present were Commissioners Dorband, Issakoo, Powers, Ruffatto, Sianis and Zangara. Commissioner Johnson was absent with prior notice. Also present were Brooke Jones, Senior Planner, Andrew Jennings, Director, Community Development and Mallory Milluzzi, Village Attorney.

4. CHANGES TO THE AGENDA – None

5. CITIZEN CONCERNS AND COMMENTS – None

6. CONSENT ITEMS

- A) Docket No. SCBA 15-38
Shir Hadash
200 W. Dundee Road
Appearance Approval of a Freestanding Sign

Commissioner Dorband moved, seconded by Commissioner Zangara to approve the following consent item.

Approve Docket No. SCBA 15-38 to permit the construction of a freestanding sign in accordance with the following plans submitted December 2, 2015 (except as noted) by Quantum Sign Corporation, on behalf of Shir Hadash, located at 200 W. Dundee, Wheeling, Illinois:

- Site plan,
- Elevation plan, and
- Landscape plan (received 11.30.2015).

On the roll call, the vote was as follows:

AYES: Commissioners Dorband, Issakoo, Powers, Ruffatto, Sianis, Zangara
NAYS: None

ABSENT: Commissioner Johnson
PRESENT: None
ABSTAIN: None

There being six affirmative votes, the motion was approved.

7. ITEMS FOR REVIEW

- A) [Docket No. 2015-5](#) (Continued from November 19, 2015)
Wheeling Town Center Development
351 W. Dundee Road
Special Use-Site Plan Approval of a Preliminary Retail & Residential Planned
Unit Development

See Findings of Fact and Recommendation for Docket No. 2015-5.

Commissioner Powers moved, seconded by Commissioner Sianis to continue Docket No. 2015-5 to January 14, 2016.

On the roll call, the vote was as follows:

AYES: Commissioners Issakoo, Dorband, Powers, Ruffatto, Sianis, Zangara
NAYS: None
ABSENT: Commissioner Johnson
PRESENT: None
ABSTAIN: None

There being six affirmative votes, the motion was approved.

8. APPROVAL OF MINUTES – December 3, 2015

Commissioner Powers moved, seconded by Commissioner Dorband to approve the minutes dated December 3, 2015 as presented. The motion was approved by a voice vote. Commissioner Issakoo abstained.

9. OTHER BUSINESS

Ms. Jones referred to a question from a Commissioner regarding the sign that was on the consent agenda for Shir Hadash. The question was regarding the image on the changeable copy on the electronic message board. It says early childhood center. She did not think the image was representative as the exact imagery that would be projected. However, the Shir Hadash synagogue was approved as having Sunday school classes and education for K-8.

Commissioner Dorband mentioned that she had volunteered for “Make a Difference Day for Special Needs Children”, “Operation Warm” and/or “Shop at the Cops”.

Commissioner Dorband was honored to be included in a recent KCCOC dinner. She mentioned it was a wonderful evening with wonderful, heartwarming people. Chairman Ruffatto echoed Commissioner Dorband’s comments.

Chairman Ruffatto thanked the Commissioners for their dedication and work during the year.

10. ADJOURNMENT

Commissioner Dorband moved, seconded by Commissioner Powers to adjourn the meeting at 8:50 p.m. All were in favor on a unanimous voice vote and the meeting was adjourned.

Respectfully submitted,

Steve Powers, Secretary
Wheeling Plan Commission

**DISTRIBUTED TO THE COMMISSION 01.08.2016
FOR APPROVAL ON 01.14.2016**

**DRAFT FINDINGS OF FACT
AND RECOMMENDATION**

To: Village President and Board of Trustees

From: Wheeling Plan Commission/Sign Code Board of Appeal

Re: Docket No. 2015-5
Wheeling Town Center Development
351 W. Dundee Road
Special Use-Site Plan Approval of a Preliminary Retail & Residential Planned
Unit Development

WTC LLC, contract owner, is seeking the following for the property known as the Wheeling Town Center Development: Special Use-Site Plan Approval of a Preliminary Planned Unit Development for Retail and Multi-Family Residential Uses in the MXT Transit Oriented Mixed Use District, as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections. The subject property consists of: the vacant parcel at 351 W. Dundee Road (former Wicke's Furniture), the commuter parking for the Wheeling Metra Station, and the existing right-of-way of Northgate Parkway, all of which is zoned MXT Transit Oriented Mixed Use District and is comprised of a total of 16.25 acres.

Commissioner Powers read the following statement aloud.

A zoning Special Use, as defined in Title 19, of the village of Wheeling (Zoning), is a use of parcel of land that requires review and consideration before approval due to circumstances or effects on the surrounding properties that may adversely affect them. In order to be considered for a special use the petitioner is required to demonstrate through testimony to the Plan Commission at the public hearing why their request meets the conditions of the village code including, but not limited to, how the proposed use will not damage the enjoyment or use of the surrounding properties. Prior to the public hearing the petitioner provides written statements meant to show that their request for a special use meets the standards established in Title 19. The Commission Chairperson will typically direct that these statements be entered into the record without a full reading of them at the hearing. Based upon the testimony and supporting materials submitted, the Plan Commission will make findings in support of, or against, the petitioner's testimony and report those findings to the Village Board.

Chairman Ruffatto called Docket No. 2015-5 on November 12, 2015. Present were Commissioners Dorband, Johnson, Powers, Ruffatto and Zangara. Commissioner Sianis was absent with prior notice. Also present were Brooke Jones, Senior Planner, Mallory Milluzzi, Village Attorney, Andrew Jennings, Director, Community Development, Fire Chief MacIsaac, Fire Inspector Antor, John Tack, Village Engineer and consultants Kevin Shaffer, Haeger Engineering and Lynn Means, Gewalt Hamilton Associates.

Chairman Ruffatto explained the Wheeling Town Center Development had been discussed at the

Findings of Fact and Recommendation

DOCKET NO. 2015-5

conceptual level for several years and had been in front of the Plan Commission numerous times as a workshop. It is the Plan Commission's intention to review the various aspects of it and complete it in a timely manner after many previous workshops and discussions with Staff. This is the first public hearing. The preliminary PUD is an opportunity for the Commission to revise the plans with the help of Staff and the petitioner. This step is a confirmation of the master plan for the development and includes a more thorough review of the plan. The proposed project is a 16-acre mixed use development that includes numerous complex issues. Staff has an outline for the discussion to keep it focused, to get the preliminary review done and moved forward in the near future. He explained if they were unable to complete the review tonight, it would be continued to next week's meeting. However, it is the expectation that answers were not expected so it would be a continuation of the meeting. It would be unfair for Staff and the Commission to try and review it during the short time between meetings.

Village Attorney Milluzzi asked if the Commission wanted to adopt rules of procedures for the public hearing. Chairman Ruffatto was in agreement. Village Attorney Milluzzi distributed the procedures. A motion would be needed in order to adopt the rules of procedures. She explained it sets forth the general order of the presentation. The following order was provided:

- Swearing in anyone that wants to testify;
- Petitioner gives presentation;
- Staff gives a summary of the Staff notes;
- Public's cross examination of the petitioner, Staff introduces their own evidence or provides general comment;
- Petitioner has a chance of rebuttal to answer the questions made by the public and/or Staff;
- Plan Commission has opportunity to ask questions; and
- Final rebuttal by the petitioner.

Village Attorney Milluzzi further explained it also sets forth the understandings of the Plan Commissioner's role in placing reasonable limitations on evidence or testimony that is repetitious, irrelevant or immaterial and the Chairman's ability to control the hearing.

Commissioner Dorband moved, seconded by Commissioner Johnson to adopt the rules of procedures for Docket No. 2015-6. The motion was approved by a voice vote.

Mr. Friedman reviewed the order for the meeting. He will provide a presentation about the town center and will address a number of concerns that had been brought up based on his bi-weekly meetings with Village Staff. His team will address any questions relating to any specific questions regarding civil engineering, architecture, general contracting, landscaping, traffic or parking.

Mr. Brad Friedman, WTC LLC, 500 Lake Cook Rd, Deerfield, IL, Mr. Anthony Fasolo, Architect, RTKL, Mr. Steve Corcoran, Traffic Engineer, Eriksson Engineering, 145 Commerce Dr., Grayslake, IL and Mr. Eric Handley, Randolph Inc., 820 Lakeside Drive, Gurnee, IL were present and sworn in.

Mr. Friedman thanked the Commission for the opportunity to present. He gave an overall review of the development. It is a retail, restaurant, entertainment-based town center that revolves around the village green with a large luxury residential apartment building. They are trying to create a public realm and provide a sense of community with a central gathering place. He provided an image of

Findings of Fact and Recommendation

DOCKET NO. 2015-5

the town center site plan showing the residential building toward the back, the village green, Flix Brew House and other retail pads that would fill in over time.

What it takes to make a successful town center. Mr. Friedman explained according to the Urban Land Institute, they need to create a central gathering place for the community which they have done with the village green and theater. Integrate multiple uses which they have done with the entertainment component, the restaurant, the retail and the residential. Provide a pedestrian friendly environment which they feel they have done based on the site plan revolving around the village green, all very pedestrian friendly. A cohesive public/private partnership is needed which they have in place and also need to connect to the community. They have some wonderful facilities neighboring the subject property. There is the Park District recreation center, the aquatic center, the performance pavilion, Heritage Park, Village Hall, the Fire Department, the Police Department and the Metra station. The site plan has been designed to integrate with all of its neighboring properties.

The Wheeling town center is about a \$100 million mixed use transit oriented development with approximately 100,000 square feet of commercial space anchored by Flix Brew House. They have convenient surface parking throughout the project and a 5-story residential building with 295 luxury rental units that will offer a superior amenity package and a lavish courtyard. The anchor tenant, Flix Brew House is a unique first run movie theater that offers food and beverage service. They also brew their own beer on site. It is a state-of-the art movie theater and will host special events. They believe it will be the cornerstone of the town center that helps to create the central gathering place. A movie theater is a great centerpiece because it serves as a landmark and a central gathering place that becomes a part of the community. They will create it with the village green and the movie theater. They will have an outdoor dining experience, interactive water features, green space, pedestrian friendly and very user friendly. They want to create useable space where people can meet and gather. They want to host public events (i.e. street fairs, art festivals, farmer's market) and have seating areas, pergolas, trellises, ornamental pots, planters, trees, built in wood benches and natural stone seating.

Mr. Friedman provided a first floor plan of the residential building. The idea is to have the amenity space off of the village green, very inviting and open so when people are visiting the town center going to the restaurants and movies they can stop into the leasing office if interested. He provided photographs of similar projects with courtyards. They plan on having a pool, fire pit, seating areas, pergolas, putting green, bocce ball, ping pong gaming tables, etc. He provided a floor plan for a typical floor on levels 2-5. Pictures were provided of the interior units. They usually offer two different types of finishes, a light finish and dark finish. All will have high quality finishes with granite countertops, nice cabinets, a demonstration kitchen, gaming tables with a club room and lounge. It will be first class.

Mr. Friedman provided a preliminary elevation of the residential building. They will have leasing agents, a property manager, maintenance engineers and a full-time cleaning staff. The parking will have assigned parking within the parking deck. All of the spaces will be numbered and any renter will have an option of renting one or two parking spaces unless they have a studio which would be limited to one parking space.

Mr. Friedman referred to the external factors they needed to be identified and cleared up before

coming in for the preliminary PUD presentation. They received conditional approval from the Park District and now have the ability to present. The “lost” parcel which was an issue before has been identified through Near North National Title. The Village could provide a quick claim deed. Everyone seems to be on board on how it needs to be handled. West Shore Pipeline was another issue that needed to be resolved. There is a reimbursement agreement that has been reviewed by the Village Attorney and the petitioner’s attorney and is ready for execution. The other outstanding item was the Metra parcel. They have an easement from Metra. It is an easement agreement that also sets forth the terms of the shared parking moving forward. The document has been reviewed by a Village Attorney, the petitioner’s attorney and Metra’s attorney and the document is also ready for execution.

Mr. Friedman referred to the proposed timeline.

- November – working through the preliminary PUD;
- February – They hope to obtain final PUD. West Shore Pipeline is going to work on their engineering plans and get final bids.
- Spring 2016 – West Shore Pipeline should be ready to break ground and relocate the actual underground pipe. The permit-ready drawings will be finalized and submitted to HUD.
- Summer 2016 - Close on financing. Break ground, start work on the infrastructure and then go vertical with the residential building and the Flix Brew House. It is approximately six months for the core and shell for Flix Brew House and another six months for the FFNE.
- Summer 2017 – Opening of Flix Brew House. The residential building will still be under construction. There will also be some construction on the future retail pads as they are leased.

Mr. Friedman referred to the concern about the phasing of the construction and maintaining access especially for the Metra commuters. A diagram was provided of the overlay of the existing conditions which shows stage one of construction where they utilize the existing Northgate Parkway and the Metra parking at the north while they are doing construction to the southern portion of the relocated Northgate Parkway and the southern portion of the new shared parking. Stage two includes additional construction to the Metra parking. They will also do construction to Northgate Parkway. They will do construction to one side of Northgate Parkway, leaving the other side open and then flip it in order to ensure continuity of access for the Metra commuter parking lot. Once Northgate Parkway, Community Blvd. and the Metra parking have been constructed, they will then have the ability to commence construction on the residential building and the Flix Brew House. The future retail pads would be built as they get leased.

Mr. Friedman referred to the Metra parcel conveyance. The easement agreement would trigger a new reciprocal parking agreement. They would have to provide 150 commuter parking spaces plus 6 ADA parking spaces and 8 kiss and ride on Monday through Friday until noon. If demand exceeds 85%, they would provide 292 parking spaces for Metra commuters.

Mr. Friedman referred to the parking management. The site plan was reviewed and they received a memo from Sergeant Paul Hart and all of his recommendations had been taken into consideration. They have designated areas for Metra commuters; a designated area for the residents that would park in the parking deck, visitors would also park in the residential parking deck on the first floor. The Flix employees would park on the top level (6th floor) of the parking deck and then the customer

Findings of Fact and Recommendation

DOCKET NO. 2015-5

parking is throughout the entire town center. It is a shared parking concept. The Village would be in charge of collecting commuter parking fees and would manage and police it.

Mr. Friedman referred to the previous concern of angled parking in front of building E. He was unsure about the concern and mentioned that there were a lot of downtown areas with angled parking in front of the retail stores and that it worked well. He thinks it contributes to the charm and overall feel of what they were trying to achieve.

Mr. Friedman referred to the previous concern about valet parking. They added some addition queuing for valet parking. They now have space for 20 vehicles for drop off/pick-up. He met with Park Place Valet and reviewed the site plan with the owner of the company in great detail. He felt it was a perfect location for the valet parking. He felt there was ample room for drop off and pick-up. He had a couple of suggestions which include incorporating some awnings with built in heaters and some built-in key cabinets up against the buildings.

Mr. Friedman referred to the previous concern with giving vehicles the ability to turn left from Northgate Parkway into the northeast quadrant of the retail center between buildings A and C. He explained it was required in order to get Starbucks to come to the center. He understands the concern because vehicles that would be exiting the town center would queue up and nobody wants to see traffic buildup as vehicles are waiting to turn left. He suggested limiting the left turn to the mornings since Starbucks is busy. They would add a sign that prohibits left turns after 3:00p because in the evenings is when the traffic would stack up. This is needed in order to get Starbucks and he believes everyone would agree that it would be a wonderful tenant to have at the town center.

Mr. Friedman referred to the previous concern regarding Waste Management and the efficiency of how the trash would be picked up. He had a meeting with Waste Management. All of the buildings have an interior area with the trash facility. Waste Management would pull their vehicles in and wheel the trash containers out and then would be side loaded onto the container. They also looked at the loading for the cinema and waste pickup and didn't have any problem with the proposed design. There was one area they didn't think was appropriate to have the internal waste behind buildings F and G because of the parking. They didn't want to have any vehicles that would be parked and interfere with the potential pickup of the waste. They relocated the trash to the parking field and they would make sure it would be fully enclosed and protected. Mr. Friedman stated they wanted to keep the pedestrian cut-through open at all times. The best way to do it was to locate two ADA parking spaces which would ensure there would always be a pedestrian friendly cut-through where no vehicles would be parked.

Mr. Friedman referred to the previous concern regarding the residential loading area, parking garage and the trash area. He confirmed there will be two shoots (recycle and garbage). There is an overhead door for the waste area, a loading area and garage area. He provided turning radius diagrams that were prepared by his civil engineer to show there was ample space for vehicles to maneuver for the pickup of the waste and loading depending on the trucks size. A 3D rendering was provided of what the loading area and the garage entry would look like for the residential building.

Mr. Friedman referred to the previous concern regarding snow removal. He met with Bertog Landscaping who explained they would pile up the snow in the low parking demand areas but they

don't want to compromise parking so if needed, they would haul off snow to their facility which is located on the other side of the tracks. They have ample space to pile snow and have vehicles to handle the haul off of the snow if necessary.

Mr. Friedman referred to the previous request for a definition of public versus private ownership. He explained it was defined in the redevelopment agreement. It is Exhibit 5 to the redevelopment agreement. It shows Section 1.1 in yellow which defines the residential area and Section 1.2 in blue which is the area for Flix. The gray is the area that would be dedicated back to the Village. It is Northgate Parkway, Community Blvd. and the Metra parking. It is dictated by Metra. They are requiring that their commuter parking and access roads be owned by the Village.

Mr. Friedman referred to the previous request to provide clarity for the framework for the management of the town center. There will be a master association; WTC LLC will be the master association. Under the umbrella, there will be WTC Residential Development LLC for the residential building and there will also be WTC Retail for all of the retail buildings. There will be a declaration for the subdivision which will be prepared and recorded at an appropriate time. There will be a general blanket for the easement cross access, maintenance and parking.

Mr. Friedman referred to the previous request for the framework for the retail. It has been defined in the redevelopment agreement. There were some prohibited uses that were identified in Section 5.4 and Exhibit 3. Some examples of prohibited uses for the town center are automotive, mattress, currency, gas station, laundromats, pawn shops, tattoo parlors and video rental stores.

Mr. Friedman referred to the previous question about non-retail maximum square footage that would be allowed. This item was addressed in the redevelopment agreement. It is defined as non-sales tax producing retail. They would be limited to 15% of the total ground floor square footage in the town center.

Mr. Friedman referred to the previous question about pedestrian access throughout the site. He explained they feel it had been addressed with the overall site plan. They tried to identify a couple of areas of all of the pedestrian cross access pathways they were providing from the parking fields to the village green to the retail pads. They are also providing pedestrian access from the residential building to the Metra station and from the residential building to the Park District facilities.

Mr. Friedman provided a preliminary sign plan identifying monument signs to the north by Dundee and a number of vehicular directional signage, pedestrian signage and retail ID signage. He provided some conceptual drawings that were prepared to give an idea of what the monument signs might look like.

Mr. Friedman referred to the requested bicycle parking. He explained there was ample bicycle parking throughout the development in four locations that were identified on the plan. They will make sure they have signage for the bicycle parking and that they provide bicycle parking racks that are harmonious with the town center vibe they are trying to create.

Mr. Friedman referred to the street lighting. The landscape architect provided a couple of options for street lighting that could be used on the village green or on Northgate Parkway or Community

Findings of Fact and Recommendation

DOCKET NO. 2015-5

Blvd. that would help create a charming downtown feel. Pictures of lighting fixtures were provided. They are LED and the idea is that they were invisible during the day but would provide ample lighting at night.

Mr. Friedman referred to the proposed pavers. They have been talking about using permeable pavers in the village green and the areas surrounding the retail buildings. This is a product recommended by the general contractor. Pictures of the different colors and styles were provided.

Mr. Friedman referred to the previous concern regarding the exterior of the Flix building. He explained the movie theater doesn't have a lot of storefront windows. However, because it is a unique cinema, they have a bar and grill concept and have the entire northern portion of the building with glass front windows. The kettles and bar and grill area will be seen. There are no storefront windows to the south and is up against the village green. They want to make sure the area is not cold and is warm and inviting. The initial elevation provided from Flix needed some improvement. They put together a rendering showing what could be done to the building using different materials (lighting, planters, providing pergolas and trellis and seating areas) that would warm up the façade and make the village green more inviting.

Mr. Friedman referred to the expectations to approve the overall site plan and to approve the phase one buildings (Flix Brew House and the residential building). All of the additional retail pads would be approved at a later point in time as leases were finalized.

This concluded Mr. Friedman's introductory presentation. Chairman Ruffatto asked Mr. Jennings to provide a recap of the Staff Report.

Mr. Jennings wanted to reiterate some points that Commissioner Powers and Chairman Ruffatto made in their opening remarks. He mentioned that the proposed was a very large and exciting project for the Village of Wheeling. The location is unique for the region. It is uniquely situated with not just the traffic volume of Dundee Road but there is also fairly easy access to Lake Cook Road. Along the west side of the property, there is the Metra station and access to the municipal campus and access to the Heritage Park complex with all of its new improvements. He explained it was an exciting opportunity for the Village to realize a vision that had been put in place about 15 years ago.

Mr. Jennings reviewed Staff's comments. He noted that Lynn Means, consulting traffic engineer from Gewalt Hamilton assisted Staff and a consulting civil engineer, Kevin Shaffer, Haeger provided comments. Chief MacIsaac from the Fire Department also provided comments.

Mr. Jennings explained one of the items that Staff is trying to recognize is there are some unique challengers with the unique opportunity. The site does have limited access. There is a T intersection that essentially ends into the site with all of the traffic issues with Northgate and Dundee. With the preliminary PUD and the master plan associated with it, they want to ensure that the elements on the master plan were given their greatest chance of success. He explained the site was driven by the two anchor buildings and the village green. There is a residential building which is an anchor and then the theater building. He thinks there is an opportunity to work with the development team to ensure that those two buildings were supported to the greatest extent possible.

He felt there was a really well developed plan for both of the buildings. The Flix is extremely valuable to the success of the development as is the residential building. He wants to discuss the issue of the site plan as an element of supporting the functions of those two buildings. The anchor buildings are on the south end of the site. The north end of the south is primarily the second phase buildings. The speculative retail and the success of the speculative retail really depend greatly on the ability of the two anchor buildings to thrive in the site plan. Mr. Jennings referred to the challenge to provide adequate access around the site since it was narrow and the location of the tracks.

Mr. Jennings noted there were a few items that he felt had opportunities to help support the buildings better and give them an even greater chance of success. He mentioned the pedestrian access was limited on the east side of the site and the access to Village Hall was lacking. There are some concerns with the intersection design near Northgate and Dundee. The parking field that is in the middle of the speculative buildings was noted in the consulting civil engineer's report and has a substandard stall depth on the perimeter. He noted there was an opportunity here to improve the pedestrian access which could result in the loss of some of the parking spaces in the area. He mentioned there were utility locations that they have been working with the project engineer. There may be a need to deal with utility conflicts on the east side relative to the Village's storm water pipe and the water main in the area. He noted that Mr. Friedman's presentation did cover the issue of the extent of the initial parking.

Mr. Jennings referred to the south end of the site. He felt there was an opportunity to work at the preliminary PUD stage to help improve the access and the relationship between the buildings. The building is the centerpiece of the development. He felt there was an opportunity as it was currently designed that was missed to have it relate to the park. There is an existing access point which connects to the band shell and loop path around Heritage Park and has a great opportunity to increase the pedestrian access. He suggested maybe providing a ground level door. The east west vehicular connectivity on the site was somewhat limited. The extension of Community Blvd. at the north is the primary east west connection. The other connections are mainly pedestrian. He thinks one of the impacts of it that should be considered at the preliminary PUD stage was the volume of use that this particular drive would get. The cross traffic does not stop. The particular drive serves a large number of uses. It serves the entire residential building including the theater employee parking on the upper deck. It serves the residential guests, residential deliveries, and a portion of the valet. The number of parking spaces is 716. He thinks there is an issue where the residential function and the theater function were potentially negatively impacted by that limiting factor. He referred to the number of apartments and amount of retail was roughly equivalent to Northgate Crossing plus Arlington Club Commons.

Mr. Jennings referred to the Fire Department review. He noted that the turning radius exhibits illustrate potential points of conflict. The site plan may need to be revised especially due to the impact of snow accumulation. The civil engineer also noted a similar issue with the turning radius. A second point is the conflict between light pole bases and fire hydrants that needs to be resolved. He felt this had the potential to decrease the number of parking spaces in the parking fields. They require some separation from other structures to the fire hydrants.

Mr. Jennings referred to trash collection methods that may require further modifications. He thought

some of the additional discussion tonight might address most of Staff's concerns.

Mr. Jennings referred to the snow removal plan. He felt this detail could be done later but wanted to remind the Plan Commission of the challenges with the relatively limited runs in the parking lots that end in landscaping. It is a challenge to find locations to keep the snow on site. There may be some opportunities to stage snow in less utilized areas of the parking lot. He thought the areas might need to be adjusted over the course of a couple years of operation.

Mr. Jennings referred to the assumption in the Fire Department review that the buildings are of type 2 construction. He explained the detail has limitations on the separation. There is a minimum of building separation based on the type of construction.

Mr. Jennings referred to the Gewalt Hamilton Traffic Engineering memo. There is a point (listed as #6 in the memo) that notes that several aspects of the plan would require IDOT approval. The consultant has recommended that these items be discussed with IDOT as part of the preliminary PUD process.

Mr. Jennings referred to the question regarding the raised medium. An exhibit was provided at the meeting that shows a break in the medium. They can go back and discuss it with the consultant to make sure it was an adequate solution for it.

Mr. Jennings referred to the full access driveway relating to the same point. IDOT could be OK with it provided that there was a time restriction. It does require IDOT's review since it was very close to the Dundee and Northgate intersection.

Mr. Jennings referred to way finding signage. An exhibit was provided relating to way finding signage. There is a need to make sure motorists know where to go to find the valet, Metra parking, and access to the visitor parking for the residential building.

Mr. Jennings referred to the question from the traffic consultant regarding the overflow parking. The Village's consultant review of it was that the overflow parking was still necessary on a limited basis. He suggested having the two consultants go through the reasons related to it.

Mr. Jennings referred to the civil engineer's comments. He referred to the variation relating to the parking stall depth in a few locations. He noted in the locations, there were not adequate opportunities to overhang at the curb. A lot of times, there will be reduced depth parking where there is a large landscaped area so you overhang with a drip strip and that allows the corresponding decrease given an adequate drive aisle is behind it. In this case, there are locations of shorter parking stall depths. They are combined with limited width landscape strips which may produce some negative consequences according to the Village's consultant review.

Mr. Jennings referred to the sidewalk comments. There is a decrease in sidewalk depth. There are some sidewalks that were previously shown but were omitted on this version of the site plan. He thinks it was an oversight. There is no sidewalk connecting from Dundee south.

Mr. Jennings referred to the turning radius diagram. The footprint of retail E may pose some

challenges for the number of utilities that are in the area. There is a water main relocation that is likely to require an easement from the Park District to the south of the residential building.

Mr. Jennings referred to the storm water conveyance pipe. There is an existing 72" pipe with a connection at the southeast corner of the site. It is an easement that is half on Park District property and half on Village property. They need confirmation between the water main and the storm sewer. Chairman Ruffatto opened the discussion to the public.

From the audience, Mr. Adam Cole, Director of Development, Tri City Foods, 1400 Opus Place, Downers Grove was present and sworn in. He noted that Tri City Foods was the tenant and operator of the Burger King business located at 425 Dundee Road.

Mr. Cole referred to the previous meetings and comments but expressed his disappointment that Burger King had not been mentioned by the developer or Village Staff as an adjacent business. He mentioned that they are the only operating retail business in the area on Dundee and utilizing the intersection. He noted that Burger King was mentioned in the reports that he had just received earlier in the day. He noted that they had never been contacted. He understands the developer sent a notice to the landlord who had notified him today about the meeting.

Mr. Cole understands if the preliminary approval is granted, the final plan must just conform to the preliminary approval and Code. He felt it was important that their concerns were heard and addressed.

Mr. Cole referred to the common locations and the north boundary listed on page 1 of the Staff Report and noted it was listed as vacant commercial. He referred to the Burger King that was operational and adjacent to the proposed development.

Mr. Cole referred to the luxury apartments and stated that they would love to see the growth in the neighborhood. He is happy to see it moving forward but was not happy that there had been no communication or effort to discuss the plans with the sole retail business operating adjacent.

Mr. Cole acknowledged that the land owner, Mr. Alvarado was present at the meeting.

Mr. Cole referred to Section A4 and the concern about offsite parking. He explained they had the same concern about parking in their lot which they were paying for to support their customers and the expenses that might be associated on high volume nights to provide extra security. They want to make sure there is adequate parking.

Mr. Cole referred to Page 6 in Section B3 regarding traffic management. He emphasized there was no discussions with Burger King about traffic management and they were a business that does 70% of their business through drive-through operations. They have a significant component of cars ingressing and egressing the site daily with extended hours from 6:00a to 11:00p seven days a week, including holidays.

Mr. Cole believes the presented plans would have a detrimental effect to their business and will create disruption to the movement they enjoy with the two current curb cuts on Northgate and would

render one of their access points. He referred to the crossing over traffic that was indicated to be higher volume traffic. He referred to the proposed cut between retail A and C for the left inbound so that a Starbucks could be contemplated. He didn't hear any mention that it could help the Burger King keep an access or ingress or egress. He explained if the cut were made, people south heading northbound up Northgate might try to turn left through the access to visit Burger King at the same time someone was trying to turn left on southbound Northgate going into the Starbucks creating an unsafe situation. He felt the design was ill conceived and does not consider the existing operations.

As an operator of a drive-through business, Mr. Cole believes the concept of a raised medium and timed intervals of acceptable left turns were not practical and would be in contrast with the signage. He felt it could be a complication and a better design could provide for a better solution.

Mr. Cole stressed that the existing driveways at Burger King were 23' wide and so under the design, the south driveway would have to convert to an ingress and egress but he felt it wasn't an adequate width for two-way traffic. He felt 24' was a minimum he saw typically by municipalities and many require 25'.

Mr. Cole referred to the traffic engineer Item 6 on Page 8 that recommends a discussion with IDOT occurs for approvals they must grant. He is unclear whether it has occurred and wants to be involved.

Mr. Cole referred to Item 12 on Page 8 that the sidewalks to Dundee along Northgate were not illustrated and it wasn't clear. He felt since his business fronts on Dundee and Northgate that he would be concerned and wants to see it.

Mr. Cole referred to setback adjustments and the developments on the frontage on Dundee were a little unclear. He wants to see more clarity.

Mr. Cole didn't see the signage review addressed. He felt the PUD would typically include how the signs would impact or their proximity to the existing Burger King pylon sign. He was uncertain that the proposed spacing on the widened Northgate meets the requirements for spacing under the ordinance nor does he believe there was any consideration given to the pre-existing user and tenant. He asked the Village that they not grant anything different then they would to two adjacent businesses elsewhere in the community in a way that excessive signage would hinder or harm the viability or success of the pre-existing business.

Mr. Cole referred to the proposed schedule. He didn't see anywhere on how to protect the existing businesses from extreme disruption over such a lengthy period. He expressed concern about the massive amounts of site work, earth relocation, paving, dusts, disruption, extreme level of mud over four seasons a year that will clutter the site and surrounding roads and the entrance to their restaurant. He encouraged that any approvals for this had a clear and comprehensive plan for site maintenance, cleanliness, street cleaning, dust containment and etc. from the construction activities.

Mr. Cole thinks there should be a requirement for reasonable coordination to prevent disruption from the existing businesses in the community. As he saw it described, the entrance to Burger King would be impacted during phase 1, 2 and 3 from 2017-2021. He does not want to put up with the

Findings of Fact and Recommendation

DOCKET NO. 2015-5

hassle of it every day between now and then.

Mr. Cole thinks it is shameful of the developer and the Village for not recognizing the impact it could have on a longstanding business partner in the community.

Mr. Cole referred to the civil engineering report. He referred to Item 6; they are concerned about the demolishing not being clear. They are concerned with Item 10, the deceleration lane changes and the stacking it would create. He reminded the Commission that they are a drive-through business and rely on cars being able to come in and out of their site. He thinks there needs to be a more conscious design and the drive-through specifics need to be addressed in the transportation study.

Mr. Cole referred to the unclarity of the sidewalk plans on number 11 and the landscaping plans on number 23. He referred to the drive-through consideration on number 24 and thought it was in the developer's favor but not the existing business. He referred to the parking concerns in number 25.

Mr. Cole referred to the traffic report and the concerns of the double left lane, the raised median and egress. He felt he did not have enough notice to read the entire report. He wanted to be invited to another meeting because he felt their comments were sincere and warranted. He felt the proposed development as presented would have a detrimental impact on their business.

From the audience, Mr. Pedro Alvarado, property owner, 425 Dundee Road was present and sworn in. Mr. Alvarado stated he had the same concerns as Mr. Cole. He requested that the right hand turn should be discussed at a future meeting. He expressed concerns regarding the proposed apartment building. He stated there were more Latinos in the area than non-Hispanics. He questioned the price point of the apartments. Mr. Friedman stated they were between \$1,000-\$1,200/per month. He questioned what the catalyst would be for changing the demographics in the area. He did not believe the demographics matched. He stated that he had been an appraiser since the 1990s and studied markets. He referred to the DePaul and Lincoln Park areas that had been a high crime area but when DePaul University moved in and housing was rebuilt the area changed. He also referred to The Glen. He felt the proposed development would work in the City but not in Wheeling. He was not saying it shouldn't be built but felt the residential portion was a big question for him. He wants it to succeed for everyone.

Village Attorney Milluzzi addressed the brief notice issue. She reported that notices were sent to all the property owners within 250' including Tri City Foods. Mr. Cole stated that he was not aware of Tri City Foods receiving any notice. He stated their landlord received the notice and provided it to him today. Village Attorney Milluzzi referred to an address of 2824 N. New Castle Road, Chicago, IL. Mr. Cole stated it was an address of one of the entities of the landlord but was not the address of Tri City Foods. Village Attorney Milluzzi explained it was the address listed on the Treasurer's website that pays the property taxes and that is what was used to send out the notices. Mr. Cole confirmed Tri City Foods was not located at that address.

Chairman Ruffatto asked if Mr. Friedman wanted to respond to the Village.

Mr. Friedman questioned the relationship between Tri City Foods and Heartland. Mr. Cole explained Tri City Foods purchased a number of assets owned by Heartland in November 2014. Mr.

**Findings of Fact and
Recommendation**

DOCKET NO. 2015-5

Friedman stated that he had several phone and e-mail conversations with Jeff McDonald from Heartland Corporation regarding the development. He invited him to his office to go over the site plan to discuss the town center and project in detail. He also reached out to the attorney who represents Mr. Alvarado and asked on a number of occasions to have meetings with the property owner to discuss the town center project in great detail but the response was too busy and didn't have time to meet with him.

Chairman Ruffatto asked Mr. Friedman if he had comments about the tenant or from what Mr. Jennings presented.

Mr. Friedman explained a lot of the items relating to the traffic would need to be addressed by the traffic consultant. He suggested addressing them one by one.

Chairman Ruffatto asked Mr. Cole and Mr. Alvarado if they had any comments to Mr. Friedman's statement. Mr. Cole stated that Tri City Foods acquired the property in November 2014. He was unsure what occurred prior to that date. He stated that he had received no calls during the last twelve months.

Contact information was exchanged between Burger King, the petitioner and Village Staff.

Chairman Ruffatto requested a break.

Commissioner Powers moved, seconded by Commissioner Dorband to take a recess at 8:00 p.m. The motion was approved by a voice vote.

Chairman Ruffatto turned the meeting over to the Commission for questions or comments relating to the traffic and onsite flow.

In reply to Commissioner Powers' question, Ms. Jones explained if there was something that changed the site drastically after preliminary PUD approval; the petitioner would need to return.

Commissioner Powers thanked Staff for all of their work.

Commissioner Powers referred to the traffic flow between Northgate Parkway and the Village campus. He questioned if Staff felt it was acceptable. Mr. Jennings deferred to traffic consultant, Lynn Means. Ms. Means feels they have a handle on how much traffic would be coming in and out and the potential impacts along Northgate Parkway as well as the internal and external connections through the development.

Commissioner Powers referred to the east side of the development. He questioned if it would be addressed if it became a problem in the future. Ms. Jennings explained the concern he had mentioned was between retail H and E. He had suggested that there was an opportunity during the preliminary review to look at how the different parts of the site function together. He had a concern that the daily resident in and out experience could be difficult here because of the wide mix of uses at that location. There are so many other users that share that particular drive. He suggested there was an opportunity at this time to work with the developer so as it gets refined and to ensure the

greatest chance of success for the building.

Commissioner Dorband questioned if Staff was comfortable with it. Mr. Jennings felt the preliminary Community Development review was relative to the site plan and the function of the site plan and its relationship to its surrounding. They have supporting reviews from the Village's consulting traffic engineer and a supporting review from the Village's civil engineering consultant. The Fire Department also contributed some review relative to the site plan and utility layout. He felt Staff did have some concern with some of the elements of the plan. He thinks generally everyone was pretty confident that there could be modifications. When you look at the reviews, you are looking at suggested modifications and issues that should be addressed. He doesn't want to say that Staff is comfortable with it exactly as proposed but thinks there is a good level of confidence that with some refinement the various elements could work together very successfully.

Commissioner Dorband questioned if the traffic light at Community Blvd. along with the traffic at Northgate would be enough for the flow to move along or would there be a backup along Dundee Road or in another direction. Ms. Means explained there are some capacity constraints that currently and would still be realized in the future. The study did identify that there was need for future improvement along Dundee Road for a third through lane. However, there are no current plans for it so there will still be capacity constraints especially in the morning going eastbound and in the evening going west along Dundee Road. They did propose an additional access to the site via modification of the limited one-way access in between Northgate and Community Blvd. to provide it to be a right in and right out access along with a right turn lane added in the east bound direction along Dundee Road. There are some additional improvements to help to facilitate the external traffic movement on Dundee Road but there will still be some constraints as there are today.

Commissioner Johnson felt the IDOT approval was number one on the list. He thinks some major work still needs to be done at the intersection because of Burger King. He can't imagine going through the drive through and trying to turn left and get home while the food was still warm. He wasn't too concerned with the Village Hall drive and Community Blvd. He was more concerned with the intersection. He referred to the double left turn and questioned the reason for it. Ms. Means explained it was because the study found that most traffic would be heading in that direction. He felt if someone wanted to turn right and someone at the front of the line was going straight it would back up traffic that could have otherwise gotten out. Ms. Means explained the volumes would be shared with the through and the right and was modeled to handle between that distance the through and the right turn volumes without necessitating an additional right turn lane on the northbound approach. She agreed there would be delay and they can't stack freely in a right turn lane and make the turn if there was another person in front of them waiting to go straight.

Mr. Friedman added if someone was going right there were two other access points going east. There is the cross access drive and then Community Blvd. Commissioner Johnson felt it was designed to get out on Northgate.

Mr. Corcoran, Traffic Consultant, Ericksson Engineering explained more than half of the parking was on the east side of the site, including the garage. He felt there was opportunity for them to use the right out as well as to use the Community Blvd access to go east. In the morning, the apartment dwellers may find that it's the easier way to go to Community Blvd. He agreed there would be right

Findings of Fact and Recommendation

DOCKET NO. 2015-5

turning traffic on the south leg at Northgate but there were other opportunities to turn right. They were not expecting the through volume to be very high so it justifies one lane and not two.

Commissioner Zangara asked if they contacted the railroad regarding the gates going up and down. He felt that was the biggest traffic issue when the train comes and everything gets backed up. Mr. Friedman confirmed they had many conversations with Metra because of the easement and shared parking. He explained the Metra platform needs to be relocated significantly to the south which would allow the gates to remain open.

Commissioner Zangara referred to the traffic between buildings E and H and a concern during phase 2 and the construction. He suggested exiting straight through the cross roads by the apartment building garage. Ms. Friedman explained he would love to have cross access but the Park District doesn't want any cross access along that area. The Park District wanted to eliminate any traffic patterns on the private Park District Road. They didn't want the increased traffic.

Mr. Eric Handley, General Contractor, Randolph Inc. explained they have a great deal of experience involving similar phased projects. He mentioned they worked on the Walmart store and expanded that project and kept them operational the entire time. He explained when they develop the phasing plan; they will address contractor parking, contractor access and all the concerns regarding storm water, dust control, etc. The site will be fenced in and secured. The construction will be segregated from the operating areas at all times. They will make sure that there are clear and distinct paths for construction and the residents. He confirmed they have a good plan in place for it.

Chairman Ruffatto referred to the Northgate entrance on Dundee Road. He asked for the petitioner to address the comments from the Village's traffic consultant and what could be done to make it better. Mr. Corcoran provided a drawing. They show the three outbound lanes, the dual lefts and the shared through right. He referred to the request for a raised medium which was added. They left a break. He referred to the concern when someone was stopped that they would block the in bound to the center. He mentioned the lane was much wider (approx. 26-28') so trucks could make the right and left turns. They have room to put a painted medium so a car could get out of the way with through traffic. In the morning the center is closed except for Starbucks. The Metra parking is coming in and not going out in the morning. Starbucks is asking for the left turn to come in so they could access the store. They added the medium to address the issues. He understands Burger King's concerns but also added it was a safe place if a vehicle needed to stop in the morning to get out of the through traffic. There is only one lane of traffic coming in at a time and one section will always be free.

Chairman Ruffatto felt it was somewhat disjointed. He referred to getting to the apartments. He feels it doesn't seem to have a good flow since it wasn't a straight shot. He wants to see the flow improved. He referred to the deliveries to the eastern side which would add even more traffic to the traffic flow from building C to H. Chairman Ruffatto referred to the pedestrian area between Flix and the apartment complex. He felt it would make a perfect spot for an access to the parking on the west. He felt the area should be improved. Chairman Ruffatto didn't think there was good front access to the apartment complex. Mr. Friedman agreed and felt it was killing the rent ability of all the units that faced the pedestrian corridor by installing the street. Mr. Corcoran referred to the access for the apartments and felt the connection didn't really help it. He understands the concern

about some of the circulation for parking for the customers. He felt it would not be that much of a help for the majority of the parking in terms of the traffic. Chairman Ruffatto questioned if it would relieve some of the volume on the east side that had been discussed. He questioned how the deliveries would be handled for the retail and were they being restricted to certain times. Mr. Friedman explained they hadn't gotten into deliveries for the future retail pads since they don't know the tenants. Chairman Ruffatto referred to deliveries for Flix. Mr. Friedman explained they had a loading area to handle the demand.

Chairman Ruffatto asked about the status of the IDOT review process. Ms. Corcoran explained over the summer they worked with Staff and the consultants to modify the plan. Once they were comfortable, they submitted the site plan and traffic studies to IDOT and it is currently in their review queue. They won't give a date for completion. Chairman Ruffatto questioned if it needed to be complete for final. Mr. Jennings explained it was a question for the Plan Commission. The specific elements rely on an assumption of IDOT approval. The traffic study includes some proposed modifications. He felt there was general agreement about some of the modifications but the question remains if IDOT was likely to accept them as drawn or require modifications that could ultimately impact the flow into and out of the site. He thinks it is a fair question to go through with the traffic consultants.

Mr. Corcoran mentioned they were not on the same page whether the medium was painted or raised. He didn't think it would have much of an impact on the greater plan since the basic roadway stays the same. It was a construction detail. He referred to the issue of the eastbound right turn lane on Dundee Road and noted both of the traffic consultants were in favor of it. He questioned if Staff was comfortable to move forward but have the final IDOT determination before final approval. Chairman Ruffatto felt it would be necessary to have it well before final and part of preliminary. Ms. Jones stated Staff was in agreement. Mr. Jennings didn't think Staff would be comfortable going on to end preliminary. He thought at best there would be a conditional approval that if IDOT was not in agreement they would have to come back and discuss the impact it would have on the site plan. He explained if there was a willingness of the Commission and the Board to move ahead with the understanding the final would not be reviewed without it. Staff would generally go with a more conservative approach to have some preliminary discussion with IDOT prior to moving ahead with the master plan of the preliminary PUD. Chairman Ruffatto agreed.

Chairman Ruffatto questioned if there were any major disagreements that needed to be discussed. Ms. Means explained most of them were highlighted by Mr. Jennings and related to the Northgate approach at Dundee Road. She felt it was a significant concern just addressing the medium access and working to try and get the right turn lane on to eastbound Dundee at Northgate. There are key elements that need to be in place related to way finding and valet parking. She felt working with Burger King to make the property work with the existing use was also important. She felt there was still some concern to be worked out related to the valet parking in close proximity to the main intersection as well as potentially having one main east west corridor through the property to have parking in front of building E. She felt it would be more desirable to have the parking in the rear of the building on the other side south of building E instead of backing into the main aisle.

Chairman Ruffatto requested that the petitioner address the parking by building E. Mr. Corcoran explained some retailers felt it was important to have the parking in front of the store. He mentioned

Findings of Fact and Recommendation

DOCKET NO. 2015-5

it was not an uncommon situation on much busier roads and a number of suburban downtowns and other town centers. He referred to The Glen in Glenview which has a lot of parallel and angle parking. He reiterated that they were trying to create a town center as opposed to the typical suburban development.

Mr. Friedman addressed the valet. He explained the goal was to have valet parking easily accessible for vehicles that were entering the town center from Northgate Parkway. He explained the proposed design allowed vehicles to turn right or left and immediately find valet parking staff. When he met with the valet parking operator and owner and they discussed the plan. He mentioned from an operational standpoint there was the ability to have staff at the intersection helping traffic and directing them to areas for valet parking. He felt it was an operational and management issue that would be handled.

Commissioner Johnson referred to coming off of westbound Dundee into the striped area to turn left into the Starbucks or retail D, C or E and questioned the number of cars that could stack up between the opening and the crosswalk in order to be out of the intersection. Mr. Corcoran stated there was room for three or four vehicles. Commissioner Johnson felt it wasn't a lot. Mr. Corcoran explained the intention was to serve Starbucks in the morning and not at lunchtime or other hours of the day. Commissioner Johnson sees a big conflict. Chairman Ruffatto felt it was a valid point. Ms. Means explained it was part of their concern and that the analysis shows that the queue in the morning time period, 95% of the time it would exceed the storage provided within the break. They would expect that opening to be blocked a majority of the time. They also have a concern without a restriction to be able to go left out of the driveway between buildings C and A given the relationship to Metra. Some folks would want to go in and get Starbucks and then get a train. The opening of the medium is one of their greatest concerns.

Mr. Friedman reminded everyone that if the medium was closed off it would then be detrimental to Burger King. Chairman Ruffatto questioned if Ms. Means addressed it. Ms. Means suggested that the applicant work with Burger King to potentially come up with a solution to possibly improve access by either having the medium extend not farther than the south access so that way they could still get patrons out successfully from their driveway. She mentioned another possible solution was to consolidate the north and south driveways and potentially looking at redesigning the parking lot so they could allow full movements coming in and out of one driveway to the Burger King access.

Mr. Corcoran confirmed he had reviewed the Village's traffic study and provided a reply to the comments. He referred to the lack of communication with Burger King so they have not been in a position to generate any discussion on how they could assist and improve their access. He believes it will happen after the meeting. He referred to the morning traffic and mentioned that a 95 percentile queue was the maximum queue they would see and the typical queue would be smaller. He agreed there would be times when the inbound left would be blocked but it would not be blocked constantly. He stated a left out was not their intention and they would put a medium there if necessary.

Ms. Jones noted for the record that Staff received a memo from Mr. Corcoran today regarding traffic updates. Staff did not have a chance to review it.

Chairman Ruffatto asked for comments and questions regarding parking.

Commissioner Johnson wanted details regarding the parking structure. Mr. Friedman confirmed the top deck was just for Flix employees. The visitor parking is on the first floor. The parking will be gated. Visitors could go in but would need a ticket in order to get out. The Flix employees and residents would have key fobs. The master key fob allows residents access to the parking garage areas, common area amenities, the hallways and was a unique system.

Chairman Ruffatto requested an explanation regarding the visitor parking. Mr. Friedman explained visitors get out with a disposable parking pass which the tenants would have.

In response to Commissioner Johnson's question, Mr. Friedman stated the parking fee for the residents was \$40/month. Their market research found out that new suburban projects charged a \$75+ per parking space a month. Commissioner Johnson questioned what would prevent a tenant that didn't want to pay it from using the surface lot. Mr. Friedman explained it would be policed. There will be a designated area for overnight parking.

Commissioner Johnson brought up his previous concern that Park District patrons would use the eastern side of the lot because it was closer than the Park District's south end parking lot. He was unsure on how it would be managed. Mr. Friedman felt it could go the other way as well. Commissioner Johnson questioned what would prevent a commuter who didn't want to pay for the Metra parking from parking in a surface lot and walking through the green space. Mr. Friedman explained it was addressed in the memo and the operational management aspects that would be required. He explained it was a training exercise where Staff and Metra commuters would need to get used to how the operation worked and what the best way to police and enforce it.

Commissioner Johnson questioned if they planned to include electric charging stations. Mr. Friedman confirmed they were including electric charging stations in the residential parking deck. They will also have rental car options available (Zip cars).

Commissioner Zangara asked for an explanation on how the Flix employees get out of the parking deck. Mr. Fasolo, Architect, RTKL reviewed a drawing that showed the stair and elevator on the sixth floor parking deck located in the southeast corner and a stair on the southwest corner. He felt most people would take the southeast corner down and then walk through the garage to a painted path in the garage to get out on the first floor. The employees would not have access to the residential building. The floors would require access with a fob.

Commissioner Zangara referred to the valet parking. He questioned where they suggested dropping off the cars and where the valet would park the cars. Mr. Friedman explained the drop off and pickup areas were the same. The cars would probably be parked in the southwest corner of the site which was the least desirable parking space for customers.

Commissioner Zangara questioned the location for overflow parking. Mr. Friedman explained they don't feel there was a need for overflow parking. In the event there was a demand and they required additional parking for the employees, there were neighboring facilities including Bertog Landscaping and the Metra lots that could be utilized for the employees. A shuttle would be

provided if necessary.

Commissioner Zangara questioned if they would consider having valet by the shops. Mr. Friedman explained they would have an awning with a built-in heat lamp and would have built in key cabinets. The area would be sufficient if someone wanted to wait outside for the car or they could wait inside the building until their car arrived.

Commissioner Zangara questioned if there was a bus stop on Dundee Road. Mr. Jennings explained that Pace had stopped along Dundee Road in the past but they have a tendency to stage their buses in locations that the Village had an issue with in terms of the access to the Strong and Milwaukee intersection. Their intention is to utilize the Metra area to stage. They had previously no intention of providing regular bus service to the Metra station but there had been recent discussions about bringing the buses into the kiss and ride area.

Commissioner Powers felt it would be a full time job to manage the parking but felt it would be adjusted as needed. Mr. Friedman agreed it was an operational and management issue that needs to be addressed throughout the process. The project is not getting built overnight and as it evolves they will better understand the needs that need to be addressed.

Commissioner Dorband referred to the Zoning Code parking requirements that show it was 25% below the requirement. She questioned if there was an issue with it. Mr. Friedman explained the overall issue is that it was a transit oriented development and based on market research, there is less demand for parking in transit oriented residential developments. Mr. Corcoran confirmed they were meeting 78% of the Zoning Code requirement. The differential of 156 spaces is being offset because most of the demands for the other users were at night and Metra will be in the morning and afternoon. The parking demands for the residents and other retail uses were more in the evening. He further explained that in residential transit oriented design they are seeing less parking than what the Zoning Code requires. They are providing 85% of the residential Code requirement. They think the number will be a lot less due to the transit oriented design and the type of development based on studies for this issue. They are asking for a 13% reduction for the commercial uses because of shared parking.

Commissioner Dorband asked if they were still considering using the Metra lot after 11:00 a.m. Mr. Friedman explained it was part of the Metra shared parking agreement. He noted that his attorney and the Village attorney had reviewed it. They are fine with the shared parking component. Mr. Corcoran noted that the basic train schedule showed that 95% of the people were in the Metra parking spaces by 10:00 a.m.

Chairman Ruffatto asked for a review of the variation on the apartment parking. Mr. Corcoran explained the Village Code had two requirements depending on the number of bedrooms. For studios and 1 bedrooms it is 1.7 spaces per unit and for 2 and 3 bedrooms it is 2.2 spaces per unit. They are providing 1.67 spaces per unit. Chairman Ruffatto questioned if Staff felt it was a number that was acceptable. Mr. Jennings explained if it was broken down by the units, it was equivalent to each residential unit having a parking space. They tried to play it out how it would look like over time. Because they will be able to manage the leases, the distribution of spaces and the restriction on overnight parking, Staff felt they were fairly comfortable with it. They felt it was likely to work

Findings of Fact and Recommendation

DOCKET NO. 2015-5

out. Staff's concerns were more about the assumptions made with use restrictions on the commercial side. Mr. Friedman provided numbers based on the assumptions for the parking ratio based on the units. He stated that a studio would get 1 parking space, a 1 bedroom would get 1.3 spaces and the 2 & 3 bedrooms would get 2 spaces.

Chairman Ruffatto referred to the variation on the commercial. He asked for an explanation on the variation. Mr. Corcoran explained the variation for the commercial was 964 required versus 836 provided. He explained it was due to the variation by the time of day for some of the uses. They also looked at the industry parking demand for some of the uses. He noted that retail was generally a little less than 4. The bank in the northeast corner would be closed at night and the parking would be available for the other uses at night and the weekends. He stated their analysis on an hourly basis showed they could provide the parking shown.

Chairman Ruffatto referred to the table provided on page 9 of the traffic study. He questioned if they took into consideration the Park District activities when they did the traffic studies. Mr. Corcoran confirmed they took it into consideration for the traffic study. They received projections from the Village and made some adjustments. They have not assumed any Park District parking coming onto their parking or vice versa. Chairman Ruffatto was not certain why it wasn't taken into consideration since it would happen. Mr. Friedman explained there was synergy amongst all the uses and that was the reason the Village decided to take the land and create a town center. He noted that when he met with Village Staff, the number one comment was they wanted to create a town center area that revolves around the municipal campus and that works in synergy with the existing facilities offered by the Park District. He believes it was the desire when the master plan and the RFP were issued for the site.

Chairman Ruffatto referred to the table. He noted at 8:00 p.m. was the minimum amount of spaces that would be available. The table showed it had 701 vehicles and was 84% of the capacity but would still have 135 spaces open during the week and 67 on the weekends. Mr. Corcoran agreed the statement was correct. Chairman Ruffatto questioned if the calculations took valet parking into consideration. Mr. Corcoran confirmed it was the parking demand regardless of who parked the car.

Chairman Ruffatto questioned the uses used for the parking study. Mr. Corcoran explained it was based on the Flix with 1,032 seats including the bar area and includes a bank, a Starbucks and the roughly 50,000 square feet remaining was based on 20,000 of it being restaurants and roughly 38,000 for retail users.

Chairman Ruffatto referred to the turning ratio issues and felt it would definitely impact parking. Mr. Friedman explained the turn radius diagrams had been provided. The parking report was consistent with the submitted plan. The diagram shows areas where parking spaces were eliminated. The current parking count reflects the spaces that were shaded out in order to accommodate the turning radius. He confirmed the calculations reflected the recommended changes for turning radiuses that the Fire Department recommended. They are not reflected in the plans. The parking study is updated.

Chairman Ruffatto asked for an explanation for the overhang spaces in the northeast corner. He questioned what the impact would be if it needed to go back to regular sized parking. Mr. Friedman

questioned the concern with the overhang. Mr. Tack explained his comment was regarding the variance and the overhang on the perimeter spots that were in locations with very narrow widths and would cover the limited green space. Mr. Friedman agreed there was limited green space but noted the development was located next to a park.

Ms. Jones referred to the 17' dimension of the stall length and explained Staff also had concerns regarding the safety for the drive aisles. They felt motorists may not pull in all the way to overhang a full 2' dimension in the rear into the drive aisle creating a hazard. Mr. Friedman felt overhangs were common in every downtown area. Mr. Corcoran agreed overhangs were common not only in downtown areas but also in suburban parking lots especially along curbed and landscaped areas. He stated they were providing a 62' bay and in the retail industry standard it was a 60' bay so they were actually a little wider than normal. He mentioned most vehicles were generally under 17' except for the large SUVs, Pick-ups and mini vans.

Chairman Ruffatto asked Mr. Cole if he had any questions regarding the traffic or parking. Mr. Cole referred to the Starbucks slide and expressed concern that the ingress and egress would have on the customers of Burger King. He questioned if trucks were included in the traffic study. He was referring to the trucks that serviced the food and beverage tenants and other retail tenants. He mentioned the typical delivery truck in Chicago was a minimum of 24' box on a truck. It is 36' long with a fixed axel. He noted that more typically for a national retailer would be a 48' semi-trailer or a 53' semi-trailer. He expressed concern if there was inbound and outbound traffic of trucks it would be one truck that would fill the void. He questioned if it was analyzed in the traffic study. George reported the width of the driveway was in excess of 24'. Mr. Cole expressed concern with the outbound since one semi would prevent any inbound traffic from crossing over the lane since the truck would physically block it. Mr. Corcoran explained it was happening at Dundee Road with two lanes and when someone was turning to make a right. Mr. Cole questioned if trucks were considered in the timing, queuing and flow through. He hopes it is considered in the plans as it relates to the intersection.

Mr. Cole questioned if Burger King's egress and ingress traffic was included in the study. Mr. Corcoran confirmed it was included. Mr. Cole questioned who supplied the data. Mr. Corcoran explained they did traffic counts. Mr. Cole offered to supply the data on an hourly and daily basis.

Mr. Cole questioned if the parking for the center was metered. Ms. Jones confirmed it was not metered parking. Mr. Cole expressed concern that people would park in the Burger King parking lot when the center was under parked or if it was metered. Mr. Corcoran stated they were not under parked. Mr. Cole thought it was a 13.3% reduction per Code. Mr. Corcoran explained they expected demand for the development to be less than Code.

Mr. Alvarato asked for details regarding the eastbound traffic. Mr. Corcoran explained the results of the traffic study, traffic counts and projections indicated that a separate right turn lane was warranted to go from eastbound into the development. He explained there wasn't enough right-of-way in this section of Dundee Road for them to implement it. Mr. Corcoran explained they did not have the control of the land to do it so they are not adding another lane. Mr. Alvarato mentioned he had seen a drawing with an extra lane. Chairman Ruffatto confirmed the extra lane was not being added. Mr. Friedman explained when they met with the Commission for the concept meeting, the

**Findings of Fact and
Recommendation**

DOCKET NO. 2015-5

recommendation was to provide a cross access easement for Burger King so that was when he reached out to the property owner and suggested if they could work with them on the right deceleration lane they would provide the cross access.

Chairman Ruffatto suggested continuing the docket to next week's meeting. The existing documentation would be used in addition to the presentation that was provided tonight. The review would continue next week.

Commissioner Johnson moved, seconded by Commissioner Dorband to continue Docket No. 2015-5 to November 19, 2015.

On the roll call, the vote was as follows:

AYES: Commissioners Dorband, Johnson, Powers, Ruffatto, Zangara
NAYS: None
ABSENT: Commissioner Sianis
PRESENT: None
ABSTAIN: None

There being five affirmative votes, the motion was approved.

PUBLIC HEARING NOVEMBER 19, 2015

Chairman Ruffatto called Docket No. 2015-5 on November 19, 2015. Present were Commissioners Dorband, Johnson, Powers, Ruffatto and Zangara. Commissioner Sianis was absent with prior notice. Also present were Brooke Jones, Senior Planner, Mallory Milluzzi, Village Attorney, Andrew Jennings, Director, Community Development, Fire Chief MacIsaac, Fire Inspector Ron Antor, John Tack, Village Engineer and consultants Kevin Shaffer, Haeger Engineering and Lynn Means, Gewalt Hamilton Associates.

Mr. Brad Friedman, WTC LLC, 500 Lake Cook Rd, Deerfield, IL, Mr. Anthony Fasolo, Architect, RTKL, Mr. Steve Corcoran, Traffic Engineer, Eriksson Engineering, 145 Commerce Dr., Grayslake, IL, Mr. George Dreger, Eriksson Engineering, Ms. Michelle Kelly, Upland Design and Mr. Eric Handley, Randolph Inc., 820 Lakeside Drive, Gurnee, IL, were present and previously sworn in. Mr. Peter Farquhar, Randolph Inc, 820 Lakeside Drive, Gurnee, IL was present and sworn in at tonight's meeting.

Chairman Ruffatto requested an update on the progress made with the Burger King property. Mr. Friedman reported there was a meeting yesterday at Village Hall to discuss alternatives to the access and curb cuts along Northgate Parkway. Chairman Ruffatto questioned if progress was being made. Mr. Friedman explained they were having discussions but had not yet come up with an alternative plan. The petitioner's traffic engineer and civil engineer were present at the meeting and will work with the architects in order to redesign Burger King's existing curb cuts based on some suggestions and requests that came up at the meeting and ultimately would shift the "Starbucks" left turn access a little to the south.

Chairman Ruffatto questioned if the petitioner had any meetings with IDOT. Mr. Friedman confirmed they had not met with IDOT. Mr. Corcoran confirmed a meeting had not yet been scheduled with IDOT but were in the process.

Chairman Ruffatto questioned if Mr. Cole had any comments about the discussions. Mr. Cole confirmed they met with Village Staff, the developer and consultants. They heard their concerns and would work together, hopefully to a solution that would be agreeable to both parties. His comments addressed parking, concerns about the infrastructure on egress/ingress and in search for some public parking between their lot and the public way. A primary point of discussion was signage and the proposed placement of it. He is hopeful that the developer will respond positively with some change that was good for them and the landlord.

Mr. Alvarado stated he was a real estate appraiser with over 500 hours of appraisal classes. He has done appraisal reviews throughout the country. He clarified that his mother was the actual owner of the Burger King property for 11 years and not him. He felt the tax collections from Burger King had likely helped to subsidize the purchase of the proposed site. He mentioned that Burger King had been in existence for over 30 years and has had a left turn access entrance from the Northgate Northbound traffic for the entire period. He mentioned last week he learned that one of the proposed changes was to lose the access. He mentioned the public was shown Burger King as vacant land in the request for Commission action staff project review docket 2015-5. He felt the two actions were prejudicial to his family and the lease holder. He mentioned it was the only Hispanic owned property adjacent to the soon to be town center that leased to a restaurant that employs a number of Hispanics. He explained that his mother relies on the existence of the Burger King for her livelihood. The property was obtained after several decades of hard work and savings. It is her entire nest egg. Ms. Milluzzi reminded Mr. Alvarado to keep it relevant to the actual PUD. Mr. Alvarado explained that none of the changes shown in the plans were made or discussed with him before being posted on the website last week. He received an e-mail from Mr. Friedman on May 11, 2015 requesting a new right turn lane on Dundee Road and placing a sidewalk on his property. The request did not include any survey on what was being requested. He spoke with Mr. Friedman after receiving the e-mail and requested the change again and he declined. Chairman Ruffatto reminded Mr. Alvarado to keep the discussion to the PUD and not the history of the e-mail exchanges.

Mr. Alvarado referred to yesterday's discussion at the meeting regarding the possible solution to the left turn matter. He expressed concern that the changes discussed would have a negative impact to the layout of the site making it difficult to attract a similar national tenant if the Burger King leaves. He felt the only reason for the change to the left turn was for one perspective tenant, Starbucks. He asked if a coffee shop was an important service in a development that the traffic consultant, Mr. Corcoran and Mr. Friedman have both stated that was a transient oriented development. He referred to a December 2014 report by the Regional Transportation Authority (Living a Transit Lifestyle) that it was not. 38,000 respondents from 14 transit oriented developments were surveyed for the report. He provided documentation which indicated municipalities should ensure that their TOD areas offered a full range of use at a scale appropriate for the market that addresses every day needs of the residences. They can include a grocery store, restaurant, pharmacies and other services while avoiding offering only limited services such as a coffee shop and dry cleaner. He felt even if they allowed the left turn to occur, there was no guarantee there wouldn't be a bottleneck.

Mr. Alvarado asked every Commissioner to please don't allow the left turn into the roadway off Northgate since it made no sense and would cause bottlenecks and would hurt the Burger King business.

Mr. Cole clarified that there was a suggestion that they were not in agreement on some concepts to mitigate their concerns. He didn't feel it was the case. They will work with the landlord and were not in contrast with the objectives of the landlord. The relationship between landlord and tenant require them to be inline and there may have been some misunderstandings on the events of the meeting. Chairman Ruffatto confirmed he did not have any misunderstanding.

Chairman Ruffatto asked if Mr. Friedman wanted to comment. Mr. Friedman indicated that John Melaniphy would have more information regarding the economics of a Starbucks versus a Burger King.

Chairman Ruffatto explained that the Plan Commission needed to ensure that the petitioner was working together with the franchisee, the landowner and IDOT to ensure that it was a quality town center and the traffic flow worked. He felt a lot of it would depend on IDOT.

Chairman Ruffatto asked the petitioner to address the site plan and pedestrian access.

Mr. Friedman believes they have created a town center that provides a lot of pedestrian access so pedestrians that were parking in the parking fields had the ability to access the retail stores and also access the village green and that the residents who live in the residential building had the ability to access the Metra area and the Park District facilities. They believe they have addressed it and provided a very pedestrian friendly town center which revolves around a Village green that was 100% pedestrian friendly. There is no parking surrounding the area, nor is there any vehicular traffic in front of the residential building. They believe the design contributes to the entire concept of providing a pedestrian friendly transit oriented development town center.

Chairman Ruffatto asked the Commission for their comments and questions.

Commissioner Zangara liked the site plan better since the landscaping and the Burger King was included. He wants the town center to feel like a campus with everything connecting. He didn't believe there was a sidewalk from retail E to the Village. He felt there was a disconnect in that area. He felt the residents would use the surrounding green space.

Commissioner Zangara referred to Starbucks and felt their customers would get their coffee one way or another. He referred to the Starbucks at Deer Park Center and at Kensington Road and Route 83 which had a one-way in and one-way out and were always packed. He questioned if retail A and B could be flipped. Mr. Friedman agreed it could be done and was a great suggestion. He mentioned they originally had the Starbucks in building B but Starbucks changed their mind and wanted to go in building A instead. He explained in today's world, the retail tenants such as Starbucks were calling the shots and they had to accommodate them. Mr. Friedman believed Starbucks drinkers were habitual and they would continue to visit the location and eventually learn the preferred route. He believes the preferred route was to turn right on the cross access road. Starbucks has made it

clear they want the access and if they need to shift it down a little to the south, and shave off a little of the retail square footage in building C they could make minor tweaks and adjustments in order to make sure the Village, the Burger King, the property owner, IDOT and Starbucks were all happy.

Commissioner Zangara questioned if there was any reason not to make retail E smaller to gain another in and out access. Mr. Friedman stated they did look at it but was rejected by the Park District. If they kept the road straight and then cut through, the issue from a traffic standpoint was that it didn't allow vehicles enough maneuverability.

Commissioner Zangara questioned if they could have the cross access from parking lot to parking lot for the cars in order to avoid people exiting the movie theater in front of the first floor apartments. He felt those first floor residents would never open their windows or blinds. He also suggested maybe removing the apartments on the first floor. Mr. Friedman explained from a residential living standpoint, it was nicer to have a pedestrian pathway that had been decorated with pavers, pergolas, benches and planters. He felt it was much more inviting. They felt it was a very important lease-ability aspect of the project to provide the buffer without any vehicular traffic that would interfere with the tenants who would have units on the first floor. They want to keep it a very open and pedestrian friendly environment.

Chairman Ruffatto asked for details on the landscaping.

Ms. Michelle Kelly, landscape architect, provided the landscape plan. She explained that all of the plants hadn't been designed yet but would be done after this phase. She confirmed all of the landscaped areas had lush landscaping with ornamental shrubs, evergreen shrubs, trees, perennials, ground covers which would provide flowering throughout the summer and evergreens during the winter.

Commissioner Dorband felt there needed to be a lot of landscaping with all of the proposed parking.

Commissioner Dorband referred to the previously mentioned conditionally approval from the Park District regarding the pipeline. Mr. Friedman explained there was conditional approval from the Park District for the cross access area. He believes part of the agreement was a cross access agreement which would allow for Community Blvd. to extend and cut through to the site. The conditional approval from the Park District was needed in order for them to proceed with the preliminary PUD because part of the traffic study identifies it as an additional right in and right out access point. He further explained that the pipeline was a separate issue. There was an underground pipe (high pressure petroleum) that needed to be relocated. Commissioner Dorband questioned if he had concern of any contamination. Mr. Friedman didn't have any concern. West Shore Pipeline did a feasibility study so it was addressed in their report with regards to the relocation. They have already done an environmental report and soil report so they were familiar with the site conditions. Commissioner Dorband questioned if they were good with the relocation of the pipeline. Mr. Friedman explained they were waiting on some exhibits from West Shore Pipeline. In general, all parties were in agreement with the structure of how it will be relocated.

Commissioner Dorband questioned the capacity of the number of people that could fit in the Village green area. Mr. Friedman didn't know the capacity from a pedestrian standpoint. He explained the

Findings of Fact and Recommendation

DOCKET NO. 2015-5

general concept was to create user friendly space. They can bring in a lot of plush landscaping through planters, ornamental pots and other seating areas and wall planters.

Commissioner Dorband referred to Commissioner Zangara's suggestion to swap A and B. She questioned if it was a deal breaker for Starbucks. Mr. Friedman stated that Starbucks wants the left access. He believes the issue can be resolved by making slight modifications to the site plan in order to satisfy IDOT. He believes IDOT will dictate what can and can't be done. He was confident if the left turn was shifted to the south, it would resolve a lot of issues.

Commissioner Johnson suggested moving Starbucks to the north end of retail C so the drive-thru would be between C and D. Mr. Friedman would be OK with it but explained that Starbucks was dictating what they want at this location. Commissioner Johnson explained it would move the entry further away from the Burger King problem and stacking problem. Mr. Friedman would prefer to have Starbucks closer to the village green experience but Starbucks wants a freestanding out lot.

Chairman Ruffatto reiterated that it was the Village's town center and he wants to see something more concrete. He does not want the Village to be driven by a retailer. He wants more analysis.

Commissioner Johnson felt Starbucks was acting like the anchor.

Commissioner Johnson referred to the far south end of the west side parking where it abuts against the Park District parking lot. Mr. Friedman explained it was because the Park District didn't want cross traffic there. Commissioner Johnson was still having issues with the single access for the residents. He was not familiar with a similar project in the area that had a one way in and out with the residential building at the far end. Mr. Friedman noted the North Shore 770 located at Dundee and Skokie Blvd. was similar. Commissioner Johnson would not want to drive through the pedestrian activity and the parking lot if he was a resident living in the development.

Commissioner Powers questioned if the proposed layout would allow someone to turn left into Burger King from Northgate Parkway when leaving the development. Mr. Friedman explained he was not in favor of the raised medium. The suggestion came from the Village's traffic consultant.

Commissioner Powers suggested a no left turn sign to go into the area of retail A and B and would force everyone to go down to the triangle. He thinks traffic may flow better because there will be a double left out to go westbound on Dundee.

Commissioner Powers liked Commissioner Zangara's suggestion to make retail E smaller to gain another in and out. He thinks a bottleneck is at the entry into the garage. He questioned if there was a way to have multi entries into the garage. Mr. Friedman confirmed it was not possible with the current design. He explained the Park District has made it clear that they do not want to provide cross access with additional traffic. They have done the best they could with a single access to the residential garage. He referred to a similar building designed by RTKL in Orland Park, 9750 on the Park was a 295 5-story building with residential units that wrapped around a garage with a single entrance and exit and had no problems.

Commissioner Powers questioned if the walkway by the residential building was an access point.

Findings of Fact and Recommendation

DOCKET NO. 2015-5

Mr. Friedman confirmed it was a fire emergency pedestrian only access point.

Commissioner Powers agreed that landscaping was needed because he did not want to see all parking lot. He asked that they add a lot of landscaping.

Commissioner Sianis commented that a lot of his concerns echoed other comments that had been made earlier in the meeting. He thinks the site plan is more pedestrian friendly. He thinks the site plan has an issue with vehicular traffic with how you get through it and how residents of the mid-rise building would access it. He thinks some of it needs to be rethought if it was going to be a useful development.

Commissioner Sianis felt items like painted crosswalks, asphalt streets, brick pavers were all part of a site plan. Mr. Friedman stated it would be a combination of asphalt streets and parking lots. The village green would have a lot of pavers. The original intention was to have pavers for the crosswalks but after initial comments from the concept review and board meetings they were looked down upon from a maintenance standpoint. Painted pedestrian crosswalks would be easier. He is open to whatever the Village wants for the crosswalks.

Commissioner Sianis felt the site plan worked for retail E but left the cinema complex, residential mid-rise and retail H left out. He questioned if the developer hoped that people would come in and park toward the front and walk in. Mr. Friedman confirmed that was their plan and it would be similar to a mall or lifestyle center. He thought it was the request from the original meetings on what the Village wanted to see and provide to the community.

Commissioner Sianis questioned if the parking along the west side of the property was solely dedicated for Metra. Mr. Friedman confirmed it was shared parking between Metra and the theater. It is part of the Metra shared parking agreement that sets forth the terms where they provide 150 parking spaces designated for Metra commuters Monday through Friday until noon and then it becomes open public parking for the retail.

Commissioner Sianis questioned if there would be signage along Northgate when you enter to assist with direction. Mr. Friedman confirmed there would be way finding signage. He feels it is an operational aspect and will evolve and be a management issue. He referred to the valet company who would have the ability to stand at the intersection and direct traffic.

The Commission took a break at 7:37 p.m. and reconvened at 7:47 p.m.

Chairman Ruffatto referred to building E and the shortened parking stalls and didn't think there was access on that side to the whole campus. He questioned if there was an existing sidewalk on the north end by retail A and B. Mr. Friedman confirmed there was an existing sidewalk. Chairman Ruffatto wanted to see a walk to Village Hall. Mr. Friedman questioned the width that could go in that wouldn't upset the fire department's truck turning radius.

Mr. Fasolo explained a 3' walk could be added if the minimum drive aisle could be reduced to 24' from 25'. He explained it wasn't very much with added landscaping. Chairman Ruffatto suggested working on it.

Chairman Ruffatto referred to the walk heading south on the west side of the apartment building. Mr. Faolo confirmed the walk did continue down south but wasn't highlighted on the plan.

In reply to Chairman Ruffatto's question, Mr. Faolo explained there was a side access on the west and south sides of the apartment building along with the main lobby on the northeast corner and a secondary lobby on the east side. Chairman Ruffatto wanted to see a walk on the south side of the parking area on the north side (208 parking spaces). Mr. Fosolo suggested adding a crosswalk from building D to the landscaped area on the south side of the lot and then cross over by building E. Chairman Ruffatto asked to add some pedestrian access on the south side of the parking lot with 208 spaces.

Chairman Ruffatto felt it was important to have access to the Village, Park District and Metra. Mr. Friedman asked if he was asking for a painted pedestrian walkway, striped or pavers. Chairman Ruffatto wanted it to match the walkways within the Wheeling Town Center.

Chairman Ruffatto referred to the landscaping. He agreed with Commissioner Powers that it was a large variation to request. He wants to work on it to add more green. The entire Commission was in agreement for the need of more landscaping.

Chairman Ruffatto referred to the signage. He questioned if there was a sightline view of the signage. Mr. Friedman explained it was just requested yesterday in the meeting with Burger King.

Chairman Ruffatto referred to snow removal. He questioned if there were any issues from a Fire Department concern. Fire Chief MacIsaac explained the biggest issue associated with the site is the fire truck turning radiuses. They have to jump the curb in several places. He further explained there were a number of locations where it was very tight on some of the turning radiuses and in some cases in good weather were cutting across curbs and parking spaces. He explained if snow was added it would only exasperate the situation for the Fire Department. They will work with the petitioner on tweaking a lot of the preexisting turning radiuses but it still leaves the issue of snow and how tight it would be to operate. Fire Chief MacIsaac referred to the Metra parking area with 51 spaces and explained they would need to cut across one of the islands. He agreed it would be very tight and it would be a concern for the Fire Department with winter snow removal. He also mentioned medical calls and fire alarms that would also occur.

Commissioner Sianis questioned if some of the issues could be solved with additional access provided along the southeast portion and southwest portion of the development. Fire Chief MacIsaac agreed it would help but wouldn't solve the problem. He mentioned the site was a very tight development.

Fire Chief MacIsaac referred to the east side and the emergency access point since they realized there would be times they would be cut off and would need a secondary route in a larger incident. He noted it was also not perfect since it would be a tight turn. He would prefer to see less parking spaces so they could improve the turning radiuses. He referred to the islands that will have trees that will become mature trees and would impact the turning radiuses of their vehicles. He noted that these issues would be also very similar to the issues associated with trash collection and delivery

trucks.

Mr. George Dreger, civil engineer stated they will be working with the Fire Department. He mentioned some of the radii had already been improved. Fire Chief MacIsaac confirmed the dialogue had been very active and they had been very receptive to working with the Fire Department. He cautioned that it was a very dense site and for every change made, there was a secondary affect somewhere else. He asked the Commission to keep it in mind.

Chairman Ruffatto wanted assistance from Staff in help with the snow removal and enforcement. He wants assistance with the wording for a condition. Mr. Jennings explained the natural pairing for it would be the overnight parking regulations that were discussed at last week's opening of the hearing. The developer had noted there wouldn't be overnight parking in the development. There is discussion of a designated space for limited overnight parking but the rest of the development would not allow it. This would allow for a more complete overnight snow removal.

Commissioner Johnson questioned if larger fire trucks would respond. Fire Chief MacIsaac explained that they had been working with the developer and on the turning radius drawings a dual axel ladder truck was used which is the worst case scenario.

Commissioner Powers referred to the areas on the site that required a variation to go down from 18 ½' to 17'. He questioned where they were located on the site. Mr. Fasolo explained they were usually along the perimeters where they abut a landscaped area in the east property line and west property line. Commissioner Powers questioned the percent of the total parking. Mr. Fasolo was unsure but confirmed it was mostly the perimeters spots. Mr. Dreger confirmed they took the worst case scenario in determining the turning radiuses.

Commissioner Sianis felt there needed to be another access along the south side of the development in order for the residential portion to succeed.

Chairman Ruffatto felt the main focus should be the residential property and Flix but he didn't think they were getting the access.

Chairman Ruffatto referred to the walking area in between Flix and the residential area. He didn't think it seemed to work. He asked the petitioner to look at that area.

Mr. Jennings referred to the Village's traffic consultant's comment for a raised medium. He asked the traffic consultant to address it.

Ms. Means referred to the raised medium at the Northgate Parkway access. She explained it was in their opinion that the raised medium would be required by IDOT just based on the proximity of the proposed driveway between buildings C and A on the site as well as associated with the dual left turns on the approach. She explained it was standard practice by IDOT to require a raised medium adjacent to it. As they suggested based on the raised medium, they recommended potentially that the medium extend to just to the north of the southern current configuration of the Burger King access. The only access it would inhibit would be from the town center to the Burger King. They made a further suggestion of possibly looking at exploring a consolidation of other two access points

to the Burger King that would permit full access at the southern point. It would allow all movements coming in and out at a south access point. Another consideration would be for the Burger King and Town Center to work together and maybe provide a cross access on the south limits of the Burger King property and the northern limits of the town center property.

Commissioner Johnson questioned if it would only affect the northbound traffic being able to get into Burger King. Ms. Means explained if the raised medium were to extend to just north of the southern driveway you could get a southbound in and coming out you could turn both left as well as right. You wouldn't be able to come in northbound so that is why they potentially suggested they look at consolidating both the north and south drives to have one driveway that allowed both entering and exiting into the Burger King access.

Commissioner Powers questioned if a left turn going eastbound when entering the property on Northgate Parkway with the raised medium could still be made. Mr. Friedman requested holding off on further discussion regarding the curb cuts on Northgate Parkway since they were in the process of redesigning it.

Chairman Ruffatto asked the petitioner to review the appearance of both of the buildings before discussion. He asked if anyone from the audience had any questions or comments regarding the discussion of the site plan.

Mr. Cole had a comment regarding signage. He mentioned there was only a minimal amount of signage other than the site plan itself. He explained they had a strong objection to the proposed size and placement of the primary monument sign being on the most minuscule portion of the Wheeling town center site in the right-of-way as opposed to somewhere on the east side of Northgate. They think it creates a lot of interference with their business and felt there were so many other options from a site plan perspective.

Mr. Fasolo referred to the primary elevation of the residential building. There is a large canopy at the entry and is centered on the green into the residential lobby off the first floor. They have not changed much on the design of the residential building as far as the materials and look. They have a mix of traditional and modern materials, limestone at the ground floor, masonry above mixed in with some projected bays of a cementitious board, aluminum windows, projected balconies (except on the corners) constructed of metal, storefronts with canopies at the base along the lobby and amenities. The rhythm wraps around all sides of the building so there is no backside.

Mr. Friedman referred to the Flix building. He explained they received a drawing from the Flix architect and it needed work because they were not satisfied with the façade that was up against the village green. He thinks the north portion of Flix will look fantastic. It has a lot of brick, a lot of storefront windows, bar and grill area, beer garden, beer kettles and nice signage. The concern was with the southern portion of the Flix that fronts the village green because there were no storefront windows.

Mr. Friedman provided renderings from Flix and a rendering on the petitioner's vision. Through the use of different materials, planters, lighting, seating areas and pergolas that would abut up against the building and contribute to the village green they will work with their general contractor to come

Findings of Fact and Recommendation

DOCKET NO. 2015-5

up with materials that contribute to the overall feel they were trying to achieve with the village green and town center.

Chairman Ruffatto requested a review of the light standards. Mr. Friedman explained they didn't have a lighting plan but the idea was to create ambiance lighting all along Northgate Parkway, the village green and Community Blvd. He provided a slide of the lighting options. The proposed lighting for the parking areas were invisible during the day but provided sufficient lighting at night. They are all LED energy efficient lights. Chairman Ruffatto explained the location of the lighting and a photometric plan would be needed in the future.

Commissioner Powers referred to the rendering of the residential building. He asked if stucco was going to be used on the building. Mr. Friedman explained that stucco would be used in very minimal areas that were not necessarily exposed but more as a filler for certain areas of the building. Commissioner Powers did not want to see a lot of stucco. He requested an illustration on where the stucco would be used. Commissioner Powers was OK with the appearance of the residential building.

Commissioner Powers referred to the Flix building. He had never seen metal ridged panels used. He asked if it was specific to Flix or if there were other buildings in the area that had used the same panels. Mr. Friedman explained it was a suggestion from Flix but found it to be totally unacceptable. He mentioned it would probably not be used.

Chairman Ruffatto asked for clarification of the proposed stucco. Mr. Fasolo confirmed it was EIFS. Chairman Ruffatto asked if the petitioner had seen the Village's standards and mentioned it was low on the list of preferred materials. Mr. Fasolo stated they could minimize it to areas in the courtyard. Chairman Ruffatto wanted to know the specific locations.

Commissioner Sianis questioned if the hardy board type façade was used only at the entrance of the residential building. Mr. Fasolo explained it would be mixed in throughout. It would be used mostly in the bays throughout all sides of the building. There will be a mix of masonry and hardy board.

Mr. Farquhar explained they were in the design development stage and were working with the architects to achieve the desired look. He explained there were some materials that look and feel like brick and stone but were a fiber cement product that goes on in sheets and was more resilient and normally had a 50 year warranty. He has used it on a number of buildings throughout the area. It is more cost effective and is typically used on the upper levels. Chairman Ruffatto referred to the Village's standards and order of preference. Mr. Farquhar explained it was their intention to work together as a team to come up with the best solution.

Commissioner Sianis' preference was to have a full brick façade to match the existing campus. He felt it would help to provide a more unified look throughout the campus.

Commissioner Sianis suggested considering a faux tinted window along the base of the Flix building to break up the monotone look. Mr. Friedman would consider it as long as there was a wall in front of the glass component in order to block out the sun. Mr. Fasolo explained it would be a spandrel

glass. Mr. Friedman agreed to explore the suggestion.

Commissioner Johnson didn't have any issues with the residential building. He liked Mr. Friedman's vision for the Flix building. He wants to see the other sides of the building. Mr. Friedman agreed and explained they planned on working with the general contractors and the Flix architects and business development team to achieve a look that was satisfactory to Flix and the Village. Commissioner Johnson wants to make sure the east end was attractive for the residents who would be looking at the building.

Commissioner Dorband wants to see the buildings in the town center compliment the Park District and Village Hall buildings.

Commissioner Zangara referred to the front façade of the residential building. He would prefer to have windows and not sliders for the three or four apartments on the main floor. He did not think anyone would place outdoor chairs in that public space. Mr. Fasolo explained that every unit had a private outdoor space. Mr. Friedman explained the patios would be fenced in. He questioned the concern. Commissioner Zangara didn't know the patio would be fenced in. Mr. Friedman felt it was important for image and comfort since renters want the ability to access the outdoors from their unit.

Commissioner Zangara suggested eliminating the 3-4 apartment units in the front and making them a retail space. Mr. Friedman explained the village green leads out to the main entry. The idea is for it to be open and inviting. They want to design it where people think it was a café but was a common area as part of the residential building. They will have a little coffee area with tables, chairs and a small fitness room. They will have a golf simulator, yoga studio, conference room, large club area with a pool table. They will have a demonstration kitchen for events and will provide a gathering place for the tenants to watch sporting events. He explained the idea was to create a community space. He provided photographs of the community space from his last project.

Mr. Friedman referred to Commissioner Zangara's suggestion to eliminate the 3-4 apartments. He explained they really did try and utilize as much of the first floor space to provide the amenities. He thinks there are people that want an urban lifestyle.

Commissioner Zangara questioned if they would provide coverage for the fifth floor balconies. Mr. Friedman explained he would not cover those balconies. He referred to units in the Logan Square area that had similar balconies. He feels the balconies without a cover appear as a greater space. He believes there is something for everybody, some with canopies and some without.

Commissioner Zangara referred to the Flix building. He questioned if it would return again to the Plan Commission when it was ready to go in. Ms. Jones explained actual elevation plans would be needed for preliminary PUD approval.

Commissioner Zangara questioned the number of theaters. Mr. Friedman stated there would be eight screens with 970 seats.

Commissioner Zangara questioned how the brewery would work with underage customers. Mr. Friedman explained it was an operational aspect that he didn't control. He will rely on the Flix

Findings of Fact and Recommendation

DOCKET NO. 2015-5

operators to address it.

In reply to Commissioner Zangara's question, Mr. Friedman confirmed there would be no gaming machines.

Chairman Ruffatto questioned the location of the proposed pavers. Mr. Friedman noted the locations on the site plan. Chairman Ruffatto requested that the locations be included on the final plans.

Chairman Ruffatto questioned the location of the slotted solar screens. Mr. Fasolo explained they were using the materials on some of the retail outlet buildings but were not being proposed on the apartment or Flix buildings.

Chairman Ruffatto questioned the color of the metal canopy over the entrance. Mr. Friedman stated the color pallet had not been finalized. Chairman Ruffatto wanted color pallets and material samples included on the final plans.

Chairman Ruffatto questioned the material of the privacy fence for the apartments. Mr. Fasolo thought it would be a cedar fence with plantings. Chairman Ruffatto thought it should be wrought iron to match the metal on the buildings. He wants it detailed when they return.

Chairman Ruffatto questioned the material of the trash enclosure. Mr. Friedman referred the question to his general contractor. Chairman Ruffatto wanted it described.

Chairman Ruffatto questioned the locations of the bike racks. Mr. Friedman provided a slide of the locations and noted there was also bicycle parking in the residential building for the tenants. There will also be a bike kitchen which allows the residents to maintain their bicycles.

Chairman Ruffatto questioned if there was storage for the apartment units. Mr. Friedman explained there was storage in certain corners of the building and additional storage in the parking facility. Each apartment has the ability to have a storage unit. There will be different size options available.

In reply Chairman Ruffatto's question, Mr. Friedman confirmed there were washers and dryers in each unit.

Chairman Ruffatto wants the buildings to align with the Village's standards.

Commissioner Zangara referred to Burger King's existing fence along the back property. He questioned if they were going to keep the fence. Mr. Friedman explained it was in the area they were contemplating relocating their access drive based on yesterday's meeting. He explained it was ultimately Burger King's decision. Mr. Cole stated they wanted to keep the fence.

Chairman Ruffatto questioned the phasing of the management. Mr. Jennings explained the suggested topics in the outline were written prior to the slides being presented. Mr. Farquhar explained they would originally have the 150 parking spaces in phase one parking. During that time they would be constructing the parking area below it and would create 160 parking spaces. During

Findings of Fact and Recommendation

DOCKET NO. 2015-5

the first phase, they will be working on clearing the land in the main area and putting in the new infrastructure and making the site ready for construction. Phase two is where the parking is in place for the Metra parking and they will be constructing a two-way residential area along with the village green and 2B would be constructed for Flix. They have a plan to maintain the entrance to the Burger King. They will keep one half open as they work on the other half. They will provide cross access so it would not affect the Burger King operation. He envisions all the roads to be constructed for the future phases for the additional retail.

Chairman Ruffatto expressed concern for future phase in the two parking areas. Mr. Farquhar explained initially during construction they would need to return when there was another retail tenant.

Chairman Ruffatto questioned where the parking would be once the apartments and Flix were built. Mr. Friedman explained Flix would park on the western portion of the site and the residential building would part itself.

Ms. Jones requested a plan that would show what parking would be constructed upon the completion of the Flix and apartment buildings.

Mr. Jennings referred to the continuity of access issue from the April 10th concept review. The continuity of access and coming up with the plans on how to provide access to the Metra parking, access to the Burger King and access to the construction site.

Chairman Ruffatto referred to private vs. public ownership. Mr. Jennings mentioned that Metra had agreed conceptually to everything going on with the development but one of the issues was the nature of the agreement with respect to the ownership. Staff had previous discussions with Metra in which the indication to the Village was that the property in its entirety could be conveyed to the developer. The agreement would be an agreement with the Village because it relates to the original stationery development agreement from the late 90s. The agreement will be done with the Village and the conveyance of the land would be like a covenant. The developer had a subsequent discussion and a slightly different understanding. He felt there was general agreement at this point among all of the parties about the concepts associated with the development but they need to go back to Metra. The Village Attorneys, Village Staff and the developer and his attorney need to go back to Metra and clarify the draft agreement to determine if it was a requirement that the Village maintains the ownership of certain roads. The Village's preference is to not own the roads inside the core of the development. If it is an absolute requirement with Metra, they need to come up with a maintenance agreement that spells out who does what. There is a concern with the method of snow removal. He felt it was an operational detail.

Chairman Ruffatto questioned if it needed to be in place during preliminary PUD. Mr. Jennings explained the agreement didn't need to be executed but they needed to know the expectation for the ownership arrangement. The Village has not been in favor of ownership and a maintenance responsibility of the drive aisles.

Chairman Ruffatto referred to the plan for the management of the entire complex. Mr. Friedman explained the general idea was that there was a master association. The master association would be

Findings of Fact and Recommendation

DOCKET NO. 2015-5

WTC LLC with separate associations for the residential building and a separate association for the retail buildings. There would be blanket easements covering the parking, maintenance and the cross access and there would be a declaration of the subdivisions between the spaces recorded. Chairman Ruffatto questioned if they were separate taxing entities. Mr. Friedman explained WTC Residential Development LLC was a single purpose entity and there was the same thing for the retail components. They would both fall under a blanket umbrella of WTC LLC.

Chairman Ruffatto questioned if it would be similar to the Westin development that had different owners for each segment. Mr. Friedman explained they would need to have separate parcels that were owned by WTC Residential Development LLC. The retail pads is the remaining portion of the development but there needs to be cross access for the residential dwellers in order to access the garage and loading areas. Chairman Ruffatto questioned if each pad would be a separate owner. Mr. Friedman explained as of now it is WTC Retail LLC and until further notice that was the plan. Their plan is to build and lease out the retail buildings and to have ownership of the town center. He does not believe it works if they are separated from an ownership standpoint. He thinks there are too many issues and liabilities. He felt ownership for the retail components should really be under one LLC or one umbrella LLC.

In reply to Chairman Ruffatto's question, Mr. Friedman explained ideally they would be built without knowing they were future phases. Once they make progress and break ground and show the retail tenants that the project was happening they would have an opportunity to finalize a lot of the LOIs that they had received over the years and build the retail pads.

Chairman Ruffatto referred to last week's packet and the sheet on variations. He mentioned there were a number of variations that would be voted on and wanted to ensure that everyone read them and understood them. He asked for the Commission to address any concerns with the developer.

Commissioner Powers felt the landscaping was short. He wanted more of it since a lot of it was being consumed by parking.

Commissioner Dorband questioned how Staff felt about the variations. Mr. Jennings felt it was important to remember with a PUD was that there was an underlying assumption of Code relief with any PUD. They do require them to provide a list so the Commission understands the variations associated with it. He suggested looking at the variations side by side with the plan.

Chairman Ruffatto reviewed the list of items that were needed for the next meeting.

- Need IDOT resolved including the Burger King property;
- Greater percentage of landscaping;
- Pedestrian traffic to the total campus;
- Traffic flow to give the best access to the two primary sites (Flix and residential);
- Provide a lighting plan, photometrics, materials, designation of overnight parking;
- Status of agreements;
- Turning radiuses.

Commissioner Powers asked for multi-point access into the garage to improve the access.

**Findings of Fact and
Recommendation**

Chairman Ruffatto referred to the retailer (Starbucks) on the north side of the property. Mr. Friedman felt it came down to the access and making sure the design was appropriate. He realizes there needs to be some modifications. He is open to modifying the intersection so it works for everybody.

Mr. Friedman referred to the comment about the landscaping and green space. He felt there was a sacrifice of either parking or pavement. He questioned what was more important, having a clean environment with planters and nice seating areas or grass. He questioned the overall vision for the project. He agreed to work with his team on a plan.

Commissioner Powers liked the rendering provided but wanted to know what would be surrounding the retail buildings. Mr. Friedman explained a lot of it comes to allowing the restaurants to have the ability to have the outdoor dining experience and providing a buffer for privacy. He wants it to be useable space for a farmer's market, additional vendors for street fairs, art festivals and etc.

Ms. Jones summarized the suggestions of the Plan Commission which would be addressed with a new submittal in time for the December 17th continued date.

- Provide feedback from IDOT;
- Resolve Burger King access;
- Increase amount of landscaping;
- Consider additional pedestrian access to the campus;
- Consider additional access to the anchor buildings, Flix and residential;
- Provide greater detail especially for lighting, materials, overnight parking, outside agency agreements;
- Turning radius, etc.;
- Review of the sign;
- Make sure there is no conflict near retail E that would affect the placement of the building;
- Define ground level patios at residential building;
- Elevations of Flix.

Commissioner Sianis moved, seconded by Commissioner Johnson to continue Docket No. 2015-5 to December 17, 2015.

On the roll call, the vote was as follows:

AYES: Commissioners Dorband, Johnson, Powers, Ruffatto, Sianis, Zangara
NAYS: None
ABSENT: None
PRESENT: None
ABSTAIN: None

There being six affirmative votes, the motion was approved.

Chairman Ruffatto called Docket No. 2015-5 on December 17, 2015. Present were Commissioners Dorband, Issakoo, Powers, Ruffatto, Sianis and Zangara. Commissioner Johnson was absent with prior notice. Also present were Brooke Jones, Senior Planner, Andrew Jennings, Director, Community Development and Mallory Milluzzi, Village Attorney.

Mr. Brad Friedman, WTC LLC, 500 Lake Cook Rd, Deerfield, IL, Mr. Anthony Fasolo, Architect, RTKL, Mr. Steve Corcoran, Traffic Engineer, Eriksson Engineering, 145 Commerce Dr., Grayslake, IL, Mr. George Dreger, Eriksson Engineering, Ms. Michelle Kelly, Upland Design and Mr. Eric Handley, Randolph Inc., 820 Lakeside Drive, Gurnee, IL, were present and previously sworn in. Mr. Peter Farquhar, Randolph Inc, 820 Lakeside Drive, Gurnee, IL was present and previously sworn in.

Mr. Jennings noted the previous discussion identified several items that the Plan Commission requested follow-up. The development team has been working on the follow-up. He gave an overview of some of the feedback relative to IDOT. The Village was copied on the IDOT correspondence.

Mr. Jennings referred to the impact of having the dual left turn at Northgate. The question was whether or not IDOT would require a median in that location. IDOT responded that they do not require it because it was not technically part of the State right-of-way. They strongly recommend it because it relates to the function of the State right-of-way. Their indication was that they would prefer for it to break farther south and the developer has been working with Burger King to discuss how the access would be provided.

Mr. Jennings referred to the second issue of a dedicated right turn lane. The response from IDOT was that the dedicated right turn lane should be required in and out of the development. There were two options for the configuration of the lane coming from Dundee onto Northgate. IDOT indicated a preference for a dedicated right turn as opposed to a slip lane (similar to the existing). The developer is working to try and provide the preferred configuration of the turn lane. The developer's traffic consultant did not agree with IDOT on the issue of a turn lane out of the development. It is currently shown for the development to have a straight right shared lane. IDOT has indicated that a right turn dedicated from northbound Northgate on to eastbound Dundee should be provided.

Mr. Jennings referred to the setback of perimeter buildings. There had been a question raised by the third party civil engineer consultant for the Village relative to the drive-thru configuration along Dundee Road. The question was whether adequate separation was provided. IDOT does not have a standard so it would essentially be the review of the Commission and their recommendation.

Mr. Jennings referred to the issue regarding the function of the intersection itself with the signal. The introduction of the dual left turn lane is going to have an impact on the phasing of the signal. The response from IDOT was a suggestion because of the dual left turn requires dedicated left turn only time, the amount of right turn from southbound can be increased by going with a straight right lane and a dedicated right lane.

Mr. Jennings provided a slide that showed Burger King's preliminary comments from the operator

of the restaurant but not the landowner. There will be a need for both parties to sign off on the changes. A summarization of Burger King's operations response to the modification was a suggestion that some of the parking stalls would be more appropriate for use by Burger King. The request was that the development's monument sign shift to the other side of Northgate. There is a request to provide a new drive aisle to safely get to the south without having to backup. The suggestion was made to eliminate some of the parking spaces to create a drive aisle.

Mr. Jennings referred to a note in the Village's civil engineer's review regarding the utility complex along the east side of the site. The Village is still working with the development team. There are some adjustments expected to the utility layout in the area. They do expect that the January 14th version should be able to address the issues in that location.

Chairman Ruffatto explained tonight's meeting is a public hearing but there wouldn't be a vote since the packet didn't get to Staff in time to properly review it. The meeting is to bring the Plan Commission up-to-date. The hearing continuation will be scheduled to January 14th. Therefore, the resubmittal packet needs to get to Staff by December 31st.

Mr. Friedman reviewed some of the items addressed based on the last two meetings and some of the changes that were made to the site plan. There were a lot of comments and concerns about trying to increase landscaping in certain areas. Some adjustments were made to the parking space dimensions internally to allow adequate aisle widths for the fire trucks for turning radius requirements which allowed them to increase the sidewalk area and add additional landscaping based on suggestions from the Plan Commission and Village Staff.

Mr. Friedman explained they shifted building C to the north and made the curb access for the northeast quadrant parking field further south not to interfere with the intersection also now not to interfere with Burger King's access. They've made some changes with regard to their entry and that has caused them to shift the out lots over to the right. They lost one drive-thru aisle for building B. Burger King had mentioned that they would want an easement for their directional signage entrance and exit so their customers could easily identify the signs.

Mr. Friedman referred to the northwest parking field quadrant. There was a slight redesign of the layout for the parking area. They created some additional landscaping and it also provides them with some additional space for snow storage and snowplow.

Mr. Friedman referred to Main Street. He stated that Mr. Jennings had brought up a point that Community Blvd. is a northwest street and then turns to an east west street which could cause some possible confusion. He suggested that the developer may want to rename the street to avoid confusion. They were currently using Main Street as a placeholder and will come up with an appropriate name for the street to help clarify and avoid confusion.

Mr. Friedman explained they added some crosswalks. They want to bring everybody over to the south side of Main Street, in front of building E. They want all of the pedestrians to have access to the site. They have increased the width of the area so they now have additional landscaping in front of building E. They have also made some slight changes to the corner area so it was now be a more landscaped plush corner. Additionally, they have shaved off some square footage of retail building

Findings of Fact and Recommendation

DOCKET NO. 2015-5

E to allow for an additional right turn out since there were concerns and comments about residential tenants having the ability to exit the center.

Mr. Friedman referred to the northeast parking quadrant. The idea was to highlight the area and provide additional landscaping. They provided a pedestrian crosswalk in the area to help identify it as a main entrance with beefed up landscaping.

Mr. Friedman referred to the southern portion of the development with the residential building. Sidewalks have been added for the pedestrians as suggested by the Plan Commission and Staff.

Mr. Friedman referred to the phasing diagrams of the project and tried to clean them up so they were easier to read and understand.

Mr. Friedman referred to a slide based on the concern from Burger King that their monument sign would block their visibility. They did a site line study and shifted their monument sign to the south in order to increase their visibility. They made a suggestion to add signage to identify the town center and possibly to help identify some of the Park District facilities. They want to try and get some of the traffic to utilize Community Blvd. and Main Street as part of their access to the project.

Mr. Friedman reported that they had met with Fire Chief MacIsaac. He is very satisfied with the truck turning radius diagrams presented.

The elevations have been updated with some new colors and materials. Samples were provided at the meeting.

Mr. Friedman provided the plan for the first floor residential building. Not much has changed from the last version. They made some slight changes to the amenity area but in general, the overall unit mix and concept remained the same for the residential building and the layout of the amenity space. Updated elevations of the Flix building were provided. He met with ATMI. A revised perspective based on some of the meetings was provided. They made sure the elevations on all sides did a good job of breaking up the building so it wasn't one long monotonous building.

Mr. Friedman provided the landscaping plans. He noted that landscaping had been brought up at the last meeting. They have done everything possible to increase the landscaping. They had talked about potentially renaming the concept of the village green and maybe calling it a plaza. The purpose is to create a central gathering place for the community and to handle the pedestrian traffic. Most lifestyle centers, shopping centers and urban plazas have a lot of brick pavement along with a lot of landscaping to help create the plush environment with trees, lighting and hanging baskets. He explained it was their intention to create an urban plaza that could handle the pedestrian traffic but at the same time would provide a wonderful central gathering place with a lot of landscaping elements that would make it feel very warm, plush and inviting. He thinks it will work very well with the movie theater, restaurants and outdoor seating.

Chairman Ruffatto requested material and landscaping details.

Mr. Fasolo explained they didn't really change materials. Off the main plaza, the white will be a

cast stone that would be a combination of a smooth finish and a rock face finish. There will be a projected metal canopy over the main entry off of the plaza that leads into the residential lobby. There are metal balconies that are hung off the structure for each dwelling unit. There are paved patios on the ground floor. Mr. Fasolo reviewed the building material and color samples. They will use a lighter grey brick and a burgundy brick used on the main elements on the upper floors. They are looking at using a double glazed glass with a darker grey or black frame color for all windows. The storefront windows for the Flix building would have a clearer glass. The brownish horizontal panels in between windows would be either a fiber cement product or a metal panel. Chairman Ruffatto explained that a decision regarding the product needed to be made before the next meeting. The projected bays would be metal panels.

Mr. Farquhar reviewed the perspective of the Flix Brewhouse. The front element will have a thin brick product and the other elements are an architectural precast with some darker color stained elements. The entire shell is made out of a precast material with either thin brick or architectural sandblasted/stained finish. The lines are architectural reveals that are about ½” deep. He mentioned that the Rosemont Outlet Mall was made of a precast panel. The glazing will be more of a clear glass in order to see the brewery in the background.

Mr. Farquhar referred to the loading area that has a typical overhead insulated door.

Chairman Ruffatto asked about the green elements on the Flix drawing. Mr. Friedman explained it was a suggested option from RTKL to offer some additional green components with a green screen. He explained it could be a combination of the green screens and the brick overlay.

The Commission looked at the material samples that were provided at the meeting.

Ms. Michelle Kelly, Upland Design referred to the plant list that was provided. There will be a variety of flowering shrubs that will provide color throughout the seasons. In addition, there are layers of perennials along with bulbs. Boxwoods and yews are included to provide color in the beds when everything else is dormant. There are proposing 120 trees on site with lots of variety.

Ms. Kelly referred to the plan and explained they were able to increase the size of a couple of beds and added additional landscaping.

Ms. Kelly referred to the first floor patios. She provided a sketch which includes shrubs along the building and then wraps around the patios. They will add some perennials in the bump outs. They are proposing a 5’ tall ornamental fence. The shrubs will help provide some visual barrier for the first floor units. The east, west and south sides of the buildings have a little different layout that includes grasses and shrubs. There is nice green landscape around the building.

Ms. Kelly referred to the village green. It is divided into different areas. The outside area will have outdoor seating with the retail buildings or cinema. It is a pedestrian friendly area but there won’t be much plant materials. In the center, there is entry features both at the north and south which will be welcoming with both planting beds and pots. They will be elevated a little so it appears as a feature. The two central activity areas will include seat walls and other community seating. The flexible space is not owned by a restaurant but the whole community. The central space is the interactive fountain area which lights up at night. A tree could be added during the winter or other greens using

the lighting even when the fountain is off.

Ms. Kelly referred to the landscape sections. It gives the feeling of a streetscape and a town look to it. Each side has a street tree and planting bed and some light poles with banners or hanging baskets. Seating and pots are also included along with a landscape bed.

Chairman Ruffatto opened the discussion to the audience.

From the audience, Mr. Adam Cole, Director of Development, Tri City Foods and operator of the Burger King on Dundee and Northgate came forward and was previously sworn in. He appreciated the forwarding of the comments by Mr. Jennings. They have been working with the developer in trying to address some of Burger King's concerns. He felt the plan was a good improvement over the first presentation. They were happy to see it and thankful for the cooperation. They still believe there were some challenges created with it. He referred to some concerns about inbound traffic being headlight to headlight with someone else in the parking lot. They feel it could create a dangerous situation so they would like to see some improved circulation as a result of all the massive changes at the intersection. He referred to the past presentation that included two pods of parking in front of their curb cuts or just east of them. They had expressed concern about them at the last meeting. There were 13 spaces presented east of the property line between their property line and Northgate and now there are 6 spaces. They strongly believe that the whole concept of parking in front of their entrances was infeasible because it was a one way entry. Once parked, the only way to get out was to go through their private parcel for egress. He did not believe it was allowable. He had expressed concern about the striping and tapering that were presented but were not shown on the current plans. They are concerned that the widths and so forth were not up to par that would typically be turning lanes or striping. He felt it was acting like a turn lane for the inbound traffic from Dundee going southbound on Northbound on Northgate and turning left into the retail driveway (between retail C and D). They were wondering why it was not presented as a left hand turn lane. They are concerned that the proposed parking is proposed at 3.3-3.5 times increase of hourly traffic at peak times per the current traffic so they felt it was a lot of additional movement. He mentioned a stop sign was being proposed which they are OK with the concept but felt it becomes inferior to all of the northbound and southbound movement. He referred to their current two forms of egress at the north and south entrance and were now being asked to go to one place of egress only on the south side with traffic increasing 3.5 fold in each direction during peak hours. They are requesting a better solution or better explanation of how it works functionally from a traffic engineering standpoint.

Mr. Cole referred to the continue proposal for the monument sign to be located on the green space parkway. He felt it was a large sign at 15' wide and 30' tall. He felt it was a huge obstruction and a distraction to their building and business. He noted it would be the largest sign in the Village located on 60' of frontage.

Mr. Cole believes there is a big step forward but they still had some concerns and felt some of the critical items that effect their operation hadn't been fully addressed.

Mr. Cole referred to the phasing. They asked that the driveway configuration be changed first before the hard medium comes in. Mr. Farquhar confirmed the medium would go in last in the

second phase.

Chairman Ruffatto asked the petitioner to address Mr. Cole's comments.

Mr. Corcoran reported they had attempted to address many of Mr. Cole's comments. He mentioned they could adjust the striping for the left turn into his site. He referred to the sign and explained it was set far enough back that it wasn't causing a traffic related issue for people going into the right turn lane to turn right. He referred to the issue with the six parking spaces. They will look at the suggested alternatives and will try and work with Mr. Cole to resolve his concern.

Commissioner Zangara referred to the signage. He questioned if there had been discussion to allow Burger King to go on the sign. Mr. Friedman explained he didn't think it was appropriate since fast food restaurants were a prohibited use to the town center but he was in agreement to discuss it. Commissioner Zangara questioned if a coffee shop was considered fast food. Mr. Friedman explained the redevelopment agreement allowed for fast casual restaurants such as Pot Belly, Panera and certain sandwich places. It prohibits fast food restaurants such as Burger King, McDonalds, Taco Bell and KFC, etc.

Commissioner Zangara referred to the parking. He liked the idea of moving the drive but would like it to go past building D. Mr. Friedman explained it was to allow the outlots to have the additional access. He felt most of the town center was really based along Northgate Parkway. He mentioned there were some requests from the retail leasing agent that some people want the traditional retail shopping center and that was the reason for the location of building E. He explained the concept was to satisfy the need for those tenants that wanted the traditional outlot with the drive-thru or the in-line space that would face Dundee Road. In order to lease and make the outlots attractive, the tenants need to be offered a perceived curb access as opposed to having to go all the way through the in-line stores and then back up.

Commissioner Zangara referred to the turn in area between the two buildings and suggested it would be a good opportunity to make it a parking garage. Mr. Friedman explained they had considered it but the issue was the garage would block all the visibility of retail building E. Commissioner Zangara felt building E was set back far enough. Mr. Friedman explained they would need to do some additional site line studies. Commissioner Zangara preferred having the study done now instead of in the future. Mr. Friedman mentioned he would love to have a parking garage in the area by the West Shore underground pipe but nothing could be located above it. He stated that there was sufficient parking according to the parking studies. Commissioner Zangara felt it looked like a shopping center with all the parking and not a town center. He felt the garage would help hide the cars and protect the cars during inclement weather. Mr. Friedman agreed to look at the site lines and ramifications of adding a garage and how it would impact the visibility of retail building E.

Commissioner Zangara referred to the valet and questioned if the customers pick up the cars where they are dropped off. Mr. Friedman explained it was an operational question that would be handled by the operations and management. The idea is to give the customer a choice.

Commissioner Zangara questioned overnight parking. Mr. Friedman explained the overnight parking for the visitors of the residential building was housed in the garage. If overnight parking

Findings of Fact and Recommendation

DOCKET NO. 2015-5

was ever needed in the parking lot, it was suggested that the triangle area would be the appropriate place for it. Mr. Friedman just became aware that the Village does not currently offer overnight parking in the Metra commuter parking lot. They will cooperate with the Village's direction regarding the overnight parking.

Commissioner Zangara referred to the pie shape parking area and suggested changing the parking to provide more green space. He provided a rough drawing. Mr. Friedman was open to new options. Commissioner Zangara felt the smaller green space islands at each end were hard to maintain so he suggested a larger green area that was easier to maintain.

Commissioner Zangara suggested adding lights during the holidays since there will already be electric for the irrigation system. Mr. Friedman agreed to the concept and having it flow into the Village town center, Northgate Parkway, Community Blvd. and the plaza.

Commissioner Zangara questioned if there was coping on the top of the Flix building. Mr. Farquhar confirmed there would be some type of metal coping on the top.

In reply to Commissioner Zangara's question regarding the residential building, Ms. Kelly explained there were shrubs either in front of the fence or grasses. Commissioner Zangara questioned if there would be a wall to form the patios. Mr. Friedman confirmed there would be no wall.

Commissioner Dorband questioned the depth of the balconies to determine if a chair would fit. Mr. Fasolo stated the depth was 5' 6".

Commissioner Dorband liked the green on the Flix building.

Commissioner Dorband liked the choice of the landscape plantings and the proposed fence style.

Commissioner Dorband was confused about the parking since 10 spaces were lost since the original meeting. She questioned if there was anywhere they could be added. She expressed concern and referred to the issues at the Westin and didn't want the same issues. Mr. Friedman explained the addition of a garage would make it difficult to lease building E because of the visibility. He mentioned they had lost 2,000 square feet of retail space with all of the adjustments and then the demand for parking also drops.

Commissioner Powers questioned if the monument sign was moved since the last meeting. Mr. Friedman confirmed it was moved further south which increased the site line coming from the west.

Commissioner Powers liked that the drive between buildings C & D was moved further. He questioned if they still had the right-in capability east of the Park District cross access area. Mr. Friedman confirmed it remained. He felt it was the preferred route and felt repeat customers would learn the best route.

Commissioner Powers questioned if there had been a double drive-thru in retail B that was made into a single drive. Mr. Friedman explained building B was a three lane drive-thru which was now a two lane drive-thru. The footprint of building A stayed consistent but was shifted over.

In reply to Commissioner Powers' question, Mr. Corcoran confirmed the hard median was a typical 6" barrier curb.

Commissioner Powers questioned if they were planning to save some of the existing trees on the site. Ms. Kelly confirmed all of the existing trees would be removed. A majority of the trees were not of good quality.

Commissioner Powers appreciated the additional green and beds especially around retail E.

Commissioner Powers asked about the proposed irrigation. Ms. Kelly explained the plan was to irrigate the majority of the landscaping. They typically do not irrigate the islands in the parking lots. The streetscape up and down the areas would be irrigated. Commissioner Powers requested an irrigation plan.

Commissioner Powers appreciated the turning radius diagram showing the flow.

Commissioner Powers referred to the overnight parking. He questioned if a guest of the residential building could park in the garage. Mr. Friedman confirmed guest parking was located in the garage. He noted that he had reviewed the guest parking plan with a property manager from a neighboring new construction residential development and he had thought the plan was brilliant and much better than their plan.

Commissioner Powers requested an explanation of using a stain over precast. He questioned if it was durable and maintenance free. Mr. Farquhar noted the stain was only used on the Flix building. It was typically a 20-year warranty. The intent was to use as much of the natural pre-cast concrete as possible.

Commissioner Powers referred to the white doors on the west elevation of Flix. Mr. Friedman confirmed they were delivery doors. Commissioner Powers questioned how the signs on the west side were applied to the building. Mr. Friedman was unsure since it wasn't part of the elevation and was added after the building was up. He confirmed the posters would be lit and protected. Mr. Farquhar explained the delivery doors could also be painted to blend in with the rest of the architecture.

Commissioner Powers felt the Flix elevations were great.

Commissioner Sianis echoed Commissioner Zangara's comments about looking into a parking garage on the northeast side of the development.

Commissioner Sianis referred to the southeast quadrant. He thinks there needs to be some type of gated access so residents don't have to travel through the town center to access the parking garage. He felt it would be easier for the residents and would decrease traffic through the town center. Mr. Friedman explained he really wants to but can't do it. He made another call into Larry at the Park District regarding getting another curb cut but the Park District was not in agreement. Mr. Friedman agrees with Commissioner Sianis' suggestion and hopes the Park District changes their mind in the future. He mentioned the Park District's concerns related to the traffic at the pre-school drop off.

Commissioner Sianis thanked the petitioner for the improved plans. He likes a lot of the modifications that were made.

Commissioner Issakoo questioned if there was a charge for the garage parking. Mr. Friedman confirmed they would charge residents for parking spaces. He explained it was customary to charge for parking at all new construction apartment buildings. They will charge \$45/month per parking space which is well below the normal charge of \$75 to \$100+. He confirmed there was no other overnight parking allowed.

Commissioner Issakoo requested information regarding their parking study. Mr. Friedman confirmed they were meeting the requirement for retail and were asking for a variance on the residential parking based on the fact that it is a transit oriented development and looking at other competing apartment buildings and parking ratio they demand. Mr. Corcoran stated they did a parking study and looked at several different things. One of the reasons they were asking for a variance was that the residential parking demand, and the type of transit oriented design as well as looking at other similar apartment complexes within the Northwest suburbs to help support the residential reduction. From the commercial and zoning standpoint, they had to consider the 154 Metra parking plus the retail demand and add it together and come up with one number when in reality, the Metra parking is basically during the day and the peak for the restaurants and Flix was in the evening. During the day there are Metra parking commuters and as they leave, there is Flix and other restaurant users for the shared parking. They did an analysis that showed the demand for every hour during the day. They made projections for Metra for the future since their demand will increase as population in Wheeling increases and came up with numbers that will be supported by the parking demand.

Chairman Ruffatto questioned how the reduction in the square footage translated into the reduction of parking. Mr. Corcoran explained the basis for the reduction was the changes in the site plan. The Zoning Code was 4 spaces per 1,000 square feet so it was roughly 8.5 spaces.

Chairman Ruffatto requested additional details on the phasing. Mr. Farquhar explained initially they needed to maintain the 160 Metra parking spaces and then they would work on developing the parking area. They will build the parking first while they are doing the earth work and putting in the site utilities. The intent is to do all the earth work in the area and put in the utilities and then build the residential and Flix buildings and central court area. He noted another critical area was revising the main area before the two buildings become operational. Half of the roadway would be done at a time so it enables them to keep the Burger King open with minimizing their traffic interruption. They will always have a minimum of two lanes. Buildings A&B, C&D and E&F are all future phases and would be applied for later. At the end of phase 2 (getting roads in, building residential & Flix buildings), there will be 1,028 parking spaces. The other parking areas will be built in the future with the future phases of retail. Mr. Friedman added that chances are during the 18-month construction of the residential building, they will have made additional progress with some of the other retail tenants, LOIs and leases. He felt the average person would not realize that the project was going in phases since chances are they would be breaking ground and constructing the other retail pads while the residential building is wrapping up its construction. A lot of the retail buildings are just a 6-month build out with another 5 months for the furniture and equipment on the interior.

Chairman Ruffatto noted there would be an apartment building and Flix building after the first phase. Mr. Friedman stated that Flix was a 38,000 square foot movie theater with 960 seats. He explained that Flix was constantly making changes to their plans for the cinemas and seating. Chairman Ruffatto referred to the 446 parking spaces for the movie theater. Mr. Friedman mentioned the 100 spaces on the sixth floor of the garage for employees. Chairman Ruffatto questioned if 446 spaces were enough spaces for the 960 theater seats. Mr. Friedman was unsure. Mr. Corcoran stated that the Zoning Code requires 444 spaces for the proposed sized theater. The 1,028 parking spaces represented 79% of their parking.

Chairman Ruffatto expressed a concern with the circulation and that people could still get stuck in the back of the town center. He thinks they have done a good job in the other areas but still has a concern and is unsure if it could ever be addressed.

Chairman Ruffatto questioned why the sign couldn't be moved to the east side of the property. Mr. Friedman explained it related to the architectural site planning standpoint. The project based on the site plan and location of the sign it was centrally located and also allows vehicles to identify the retail signage before they turn into the town center. They are having multiple monument signs with retail identification. They want to make sure they provide enough signage for the cars using the Northgate Parkway, Community Blvd. but also the private Park District cross access area.

Chairman Ruffatto wanted to keep publicizing Village and Park District events and incorporate it on the reader board. Mr. Friedman explained they had not yet discussed if they would replace it or add to it.

Chairman Ruffatto referred to the egress for the Burger King on the south side. He felt it almost looked like a right turn only on the south entrance of Burger King. Mr. Dreger explained they moved the driveway in part from the meeting with Burger King to accommodate some of their concerns. They also knew the north entrance would be a right-in and IDOT would probably require the raised median. They could curve the southern part of it a little and make it more like an "S" shape. He felt people would learn how to use it.

Chairman Ruffatto referred to the west elevation of Flix. Mr. Friedman explained it was in-flux regarding the type of signage and advertising that would be displayed. He was unsure of the type of advertisement Flix wanted. He questioned the concern. Chairman Ruffatto explained he did not have a concern but questioned if it would be considered part of the sign package. Ms. Jones explained it would probably be considered more the design of the building to allow for changeable copy related to the movie theater and their branding. It would be separate from the business identification signs. Chairman Ruffatto felt it was a great idea since it broke up the wall. Mr. Friedman noted it faced the tracks so the Metra commuters had the ability to see the town center with different advertising opportunities.

Chairman Ruffatto felt the general consensus from the Commission was that the parking garage should be considered. Analysis was needed. He referred to a comment made by the petitioner that it was the first he had heard about a parking garage. Chairman Ruffatto had brought it up in one of the workshops. He thinks it should be considered and evaluated for the next meeting.

Chairman Ruffatto questioned if the concerns for IDOT been addressed. Mr. Jennings explained that he summarized the issues from the discussion points. He mentioned the fourth item which questioned if there was something else that should be done with the signal timing of the intersection. IDOT has indicated that the right turn out of the development would be required to be a dedicated right turn lane. The traffic consultant for the development team has taken issue with it. The way IDOT had phrased it was based on the traffic study. There is disagreement about the interpretation of that peak hour number and applying it. Mr. Corcoran explained they had provided three ways to go right on to Dundee Road (Northgate signal, right-in and right-out for the Park District, alternative to go to Community Blvd.). The traffic volumes turning right were not that high to justify a single lane. In their traffic study they have between 50 and 75 cars an hour turning right during different peak hours. They don't feel the volume projects warrant providing a separate northbound lane.

Chairman Ruffatto questioned when the dialogue with IDOT would occur. Mr. Corcoran explained they would not have an answer by December 31 because of the holidays. They could draw up an alternative that shows a northbound right turn lane if they lose the battle with IDOT. Mr. Friedman questioned if it was a recommendation or a requirement from IDOT. Mr. Corcoran confirmed it was a requirement.

Mr. Jennings explained Staff's suggestion regarding the issue was similar to what Mr. Corcoran suggested. Show it as required by IDOT and then let the development team, consultants work with IDOT to see if it could be removed.

Chairman Ruffatto explained he expected to have the following for the January 14th meeting.

- Final packet
- Engineering
- Fire
- Lighting plans
- Review of a parking garage
- Address all of Burger King's concerns
- IDOT issue resolved

Chairman Ruffatto explained there was a possibility there could be a vote at the next meeting but he could not guarantee it. The Commission wants to ensure what they are shown is what goes before the Village Board. It needs to be clear and concise. There can't be any question about what was being presented and would be built for the town center.

Commissioner Dorband mentioned that one of the daylily plants was mislabeled. The photo provided was not of a daylily. Ms. Kelly confirmed it was mislabeled. The King Alfred was a daffodil.

Commissioner Zangara questioned if the only access was through the stop light between phases one and two. Mr. Friedman explained the cross access could not be done until the traffic signal at Community Blvd. was installed. Mr. Jennings stated that it had a highly likelihood of being built before any of the buildings would be occupied. He explained the project for Community Blvd.'s signalization involves other modifications to Dundee Road.

**Findings of Fact and
Recommendation**

DOCKET NO. 2015-5

Chairman Ruffatto questioned if phase 1 cut off the access to the Park District. Mr. Friedman clarified that the Park District wants it cut off.

Mr. Jennings felt there was confusion about how the phasing will work. He suggested that the phasing drawings should be shown based on the existing conditions as a transition to the proposed.

Chairman Ruffatto suggested reviewing the list of items discussed. Ms. Jones reviewed the following list.

1. Specify all building materials and colors;
2. Address Burger King's concerns;
3. Explore options for a parking garage;
4. Consider reconfiguration of the triangular parking area to create a larger landscaped island;
5. Provide a landscape irrigation plan at final PUD;
6. Consider options for an additional east/west connection to create a circular traffic flow;
7. Consider options for an additional access to/from the residential building separate from the retail traffic;
8. Explore options for monument signs;
9. Resolve dedicated right turn requirement from northbound Northgate as noted by IDOT;
10. Clarify phasing plans with existing conditions.

Mr. Friedman asked for details regarding item 1. Ms. Jones explained the Plan Commission wants definitive proposals and not options.

Mr. Farquhar questioned how a change would be addressed during the final construction phase. Ms. Jones explained they would approach Staff who would determine how it would be addressed.

Mr. Jennings referred to the question about resolving the potential impact of utilities. He suggested adding to the list that the utility conflicts had been resolved to the extent necessary for the preliminary review.

Commissioner Powers questioned if the storm water issue had been addressed. Mr. Jennings explained it was one of the items being referred to in the utilities conflict.

Commissioner Powers moved, seconded by Commissioner Sianis to continue Docket No. 2015-5 to January 14, 2016.

On the roll call, the vote was as follows:

AYES: Commissioners Issakoo, Dorband, Powers, Ruffatto, Sianis, Zangara
NAYS: None
ABSENT: Commissioner Johnson
PRESENT: None
ABSTAIN: None

There being six affirmative votes, the motion was approved.

Respectfully submitted,

Jim Ruffatto, Chairman
Wheeling Plan Commission/
Sign Code Board of Appeals

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