

**WHEELING PLAN COMMISSION  
THURSDAY, SEPTEMBER 22, 2016 6:30 P.M.**

**AGENDA FOR A REGULAR MEETING OF THE PLAN COMMISSION  
to be held in the Board Room of the Village Hall  
2 Community Boulevard, Wheeling, Illinois**

**Revised September 15, 2016**

**This meeting will stream live and be televised on Wheeling's Cable Channels 17 & 99**

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- 1. CALL TO ORDER**
- 2. PLEDGE OF ALLEGIANCE**
- 3. ROLL CALL**
- 4. CHANGES TO THE AGENDA**
- 5. CITIZEN CONCERNS AND COMMENTS**
- 6. CONSENT ITEMS - none**
- 7. ITEMS FOR REVIEW**
  - A) [Docket No. PC 16-10](#)  
Wheeling High School  
900 S. Elmhurst Road  
Minor Site Plan & Appearance Approval of Athletic Field Modifications**
  - B) [Docket No. 2016-19](#)  
Target  
1400 Lake Cook Road  
Variation from Title 21, Sign, Related to Sign Setback Requirements**
  - C) [Docket Nos. 2016-14A,B,C&D](#)  
Builders Asphalt  
571-581 Wheeling Road  
(2016-14A) Text Amendments to Title 19, Zoning, Related to 'Concrete / Asphalt Plant'  
(2016-14B) Rezoning of the Property from I-2 Limited Industrial District to I-4 Heavy Industrial District  
(2016-14C) Special Use-Site Plan Approval of an Asphalt Plant  
(2016-14D) Variations from Title 19, Zoning, Relating to the Construction of an Asphalt Plant**
  - D) [Docket No. 2016-15](#) (Continued from August 25, 2016 hearing; to be continued to October 13, 2016 hearing without discussion)  
Dundee Commons  
430 W. Dundee Road**

Special Use-Site Plan Approval of a Preliminary Planned Unit Development for  
Retail Use

8. **APPROVAL OF MINUTES** – none
9. **OTHER BUSINESS**
10. **ADJOURNMENT**

**IF YOU WOULD LIKE TO ATTEND A VILLAGE MEETING BUT REQUIRE  
AUXILIARY AID SUCH AS A SIGN LANGUAGE INTERPRETER, PLEASE CALL  
(847) 459-2600 AT LEAST 72 HOURS PRIOR TO THE MEETING.**

**Public Hearing Information  
Wheeling Plan Commission Meeting  
September 22, 2016  
(Attachment to Agenda)**

**Docket Nos. 2016-14A,B,C&D** 571 Wheeling LLC, property owner, is seeking the following actions for 571 and 581 Wheeling Road, hereinafter legally described below:

**2016-14 (A)** A text amendment to Title 19 Zoning, of the Wheeling Municipal Code, to add 'Concrete / Asphalt Plant as a Special Use in the I-4 Heavy Industrial District (Under Appendix A, Use Table); and

**2016-14 (B)** Rezoning the property from I-2 Limited Industrial District to I-4 Heavy Industrial District; and

**2016-14 (C)** Special Use-Site Plan Approval as required under Chapter 19-07 Industrial Districts, Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections, in order to establish an Asphalt Plant in the I-4 Heavy Industrial District; and

**2016-14 (D)** Variations from the Wheeling Municipal Code, Title 19, Zoning, Chapter 19-07, Industrial Districts, Section 19.07.050 I-4 Heavy Industrial District, relating to maximum height of accessory structures, number of principal structures on a lot, minimum required green space, and associated sections to construct an Asphalt Plant.

**Docket No. 2016-15** Bill Hein & Associates, applicant, on behalf of 400 W Dundee Wheeling LLC, owner, is seeking Special Use-Site Plan Approval of a Preliminary Planned Unit Development for a retail development in the MXT Transit Oriented Mixed Use District, as required under Chapter 19-05 Mixed Use and Overlay Districts, Chapter 19-09 Planned Unit Developments; Chapter 19-10 Use Regulations, and Chapter 19-12 Site Plan Approval Requirements, and associated sections. The subject property consists of the vacant parcel at 430 W. Dundee Road, which is approximately 2.96 acres at the northeast corner of Dundee Road and Northgate Parkway.

**Docket No. 2016-19** Target, tenant, is seeking a variation from Title 21, Signs, of the Wheeling Municipal Code, Chapter 21-06 Signs in Commercial and Industrial Districts, Section 21.06.500 Permitted Signs, Subsection (b) Freestanding Signs, #5 Location, and associated sections, to reduce the required setback of an existing freestanding sign upon the upcoming expansion of the public right-of-way, at an existing retail store at 1400 Lake Cook Road, Wheeling Illinois.

**REQUEST FOR PLAN COMMISSION ACTION**  
**STAFF PROJECT REVIEW**

**TO:** Chairperson Ruffatto and Members of the  
Wheeling Plan Commission

**FROM:** Andrew C. Jennings, Director of Community Development  
Brooke A. Jones, Senior Planner

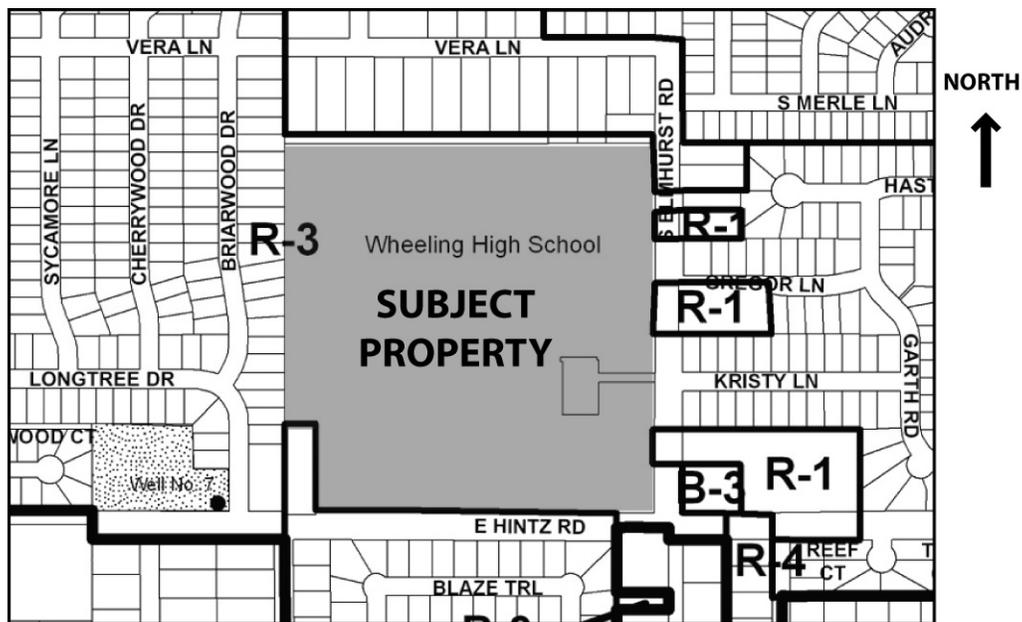
**DATE OF REPORT:** September 13, 2016

**DATE OF MEETING:** September 22, 2016

**RE:** **Docket No. PC 16-10**  
**Wheeling High School**  
**900 S. Elmhurst Road**  
**Minor Site Plan and Appearance Approval of Athletic Field**  
**Modifications**

**PROJECT OVERVIEW:** The petitioner is seeking minor site plan and appearance approval for relocation of the varsity softball field.

**LOCATION MAP:**



**GENERAL PROPERTY INFORMATION**

**Applicant Name:** Ted Birren, Director of Operations

**Property Owner:** Township High School District 214

**Common Property Address:** 900 S. Elmhurst Road

<b><u>Common Location:</u></b>	Located at the northwest corner of Hintz and Elmhurst Roads
<b><u>Neighboring Property Land Use(s):</u></b>	North: Single-Family Residential South: Multi-Family Residential & Commercial East: Multi-Family Residential & Commercial West: Multi-Family Residential
<b><u>Comprehensive Plan Designation:</u></b>	Public / Institutional
<b><u>Property size:</u></b>	approximately 37.4 acres
<b><u>Existing Use of Property:</u></b>	Public high school
<b><u>Proposed Use of Property:</u></b>	No change
<b><u>Existing Property Zoning:</u></b>	R-3 Single Family Residential
<b><u>Previous Zoning Action on Property:</u></b>	
PC 11-5	Minor S.P. & Appearance Approval of football facility improvements (2.11.2011)
2009-26	Ordinance No. 4500, granting S.U. & S.P. Approval of a telecommunication tower (3.01.2010)
2002-23	Ordinance No. 3658, granting S.U. & S.P. Approval for an expansion (7.22.2002)

### **DESCRIPTION OF PROPOSAL**

The petitioner (District 214) is requesting minor site plan and appearance approval for modifications to the existing athletic fields including the relocation of the existing varsity softball field to the southeast corner of the site. The existing two-story shed building will also be removed.

The proposed relocation of the softball field will allow for future site and building improvements that are planned for 2017.

### **SITE PLAN REVIEW**

**Scale of Site Plan:** 1' = 20'

**General Site Layout:** The existing varsity softball field, which is located directly southeast of the principal building, will be relocated to the vacant southeast corner of the site.

**Accessory Structures:** The new field will include a “clamshell” style backstop, two grade-level dugouts, two five-row bleachers, and a new shed. The site plan indicates the location of the new structures. Field renderings also help to visual the new structures. The existing two-story shed will be removed. A new (one-story) 10-foot by 12-foot shed is proposed for equipment storage. A photograph of the shed is provided.

**Site Lighting:** No new lighting is proposed.

**STAFF REVIEW**

**Fire Department Review:** The Fire Department has provided a comment memo dated 9.15.2016. The comments will be address at permit.

**Engineering Division Review:** The Engineering Division has provided a comment memo dated 9.14.2016. The comments will be address at permit.

**Impact on Adjacent Uses:** It does not appear that the proposed site plan modifications will have a negative impact on the surrounding properties.

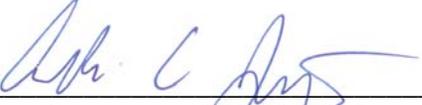
**Staff Recommended Action:** Staff recommends approval of the modifications to the athletic field.

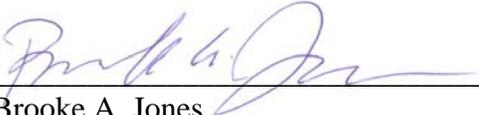
**PROPOSED MOTION**

If the Plan Commission approves of the requested minor site plan modifications, an appropriate motion would be to:

**Approve Docket No. PC 16-10** to grant minor site plan and appearance approval as shown on the following exhibits submitted September 8, 2016, by School District 214, for Wheeling High School located at 900 S. Elmhurst Road, Wheeling, Illinois:

- Cover letter (2 pages),
- Site demolition plan,
- Proposed site plan,
- Field renderings (3 pages),
- Photograph of shed, and
- Plat of survey.

  
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Andrew C. Jennings, AICP  
Director of Community Development

  
\_\_\_\_\_  
Brooke A. Jones  
Senior Planner

**Attachments:**      [Memo from Fire Department, dated 9.15.2016](#)  
  
                              [Memo from Engineering Division, dated 9.14.2016](#)  
  
                              [Photos of existing conditions \(staff\)](#)  
  
                              [Cover letter](#)

[Site demolition plan](#)

[Proposed site plan](#)

[Field renderings](#)

[Photograph of shed](#)

[Plat of survey](#)



## MEMO – Fire Prevention Bureau

**TO:** Brooke Jones, Village Planner  
**FROM:** Ronald S. Antor, Fire Inspector  
**CC:** Andrew Jennings, Director of Community Development  
Keith Maclsaac, Fire Chief  
FPB File  
**DATE:** September 14, 2016  
**SUBJECT:** Softball Field Improvements - Phase I, 2017 Building Improvements, Wheeling High School – 900 South Elmhurst Road - Plans received for review by the Fire Department, September 9, 2016.

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The Wheeling Fire Department has reviewed the submittals received related to the above referenced project and has the following comments:

### Site Plan

1. The scope of work being performed in the plans reviewed consists of removing an existing softball field and two story building from the northwest portion of the schools property at Hintz Road and Elmhurst Road. The ball field will be relocated to the southeast corner of the same parcel of property. Included with this relocation will be new dugouts, relocated bleachers and one new 120 square foot storage shed.
2. Based on the plans reviewed there are no changes in Fire Department access to the school building or to the ball fields at this location.

At this time there are no other Fire Department comments related to the project as presented in the documents reviewed.



## MEMORANDUM

**TO:** Brooke Jones, Senior Planner

**FROM:** Kyle Goetzelmann, Civil Engineer I

**COPY:** Jon Tack, Village Engineer

**DATE:** September 14<sup>th</sup>, 2016

**SUBJECT:** **Wheeling High School Improvements – Phase 1**  
**900 S. Elmhurst Rd (Wheeling High School) - Review Comments**

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The Engineering Division received a Project Cover Letter, Property Plats of Survey, Field Renderings, Demolition Plan and Site Plan for the subject project on September 9<sup>th</sup> 2016. The Engineering Division has completed a review of the above referenced submittal and offers the following comments at this time:

1. If disturbance is greater than ½ acre, MWRD permit will be needed.
2. Silt fence should be shown on the downstream side of the disturbance.

# Wheeling High School – 900 S. Elmhurst Road

Docket No. PC 16-10 (Minor Site Plan and Appearance Approval of Athletic Field Modifications)  
Plan Commission Meeting – September 22, 2016



Existing conditions of fields (looking south)

# Wheeling High School – 900 S. Elmhurst Road

Docket No. PC 16-10 (Minor Site Plan and Appearance Approval of Athletic Field Modifications)  
Plan Commission Meeting – September 22, 2016



Existing conditions of shed and ball field (to be removed)



Township High School District 214

2121 South Goebbert Road  
Arlington Heights, Illinois 60005  
847-718-7600 ■ www.d214.org

**Dr. David R. Schuler**  
Superintendent

September 8, 2016

Village of Wheeling  
Plan Commission  
2 Community Boulevard  
Wheeling, IL 60090

Re: Wheeling High School 2017 Additions and Renovations

*Members of the Plan Commission:*

It is with great pleasure that I submit for your review the plans and specifications of Phase 1 of the Wheeling High School 2017 Additions and Renovations projects. Phase 1 consists of moving the existing varsity softball field, located on the southeast area of the property, to the southeast corner of the property. The movement of the varsity softball field is necessary to prepare the site for the Phase 2 work yet to be presented to this body for review.

The varsity softball field will consist of a “clamshell” style backstop with horizontal projections around the face of the backstop opening, in an effort to prevent any foul balls from leaving the playing area and landing on either Hintz or Elmhurst Roads. The backstop is expected to be similar to other backstops on softball fields at the other schools of Township High School District 214 and the rest of the Mid-Suburban League (MSL), being consistent with height and width regulations of the Illinois High School Association (IHSA) and the National Federation of State High School Associations (NFHS), the governing bodies of Illinois high school athletics.

On either side of the backstop will be grade-level dugouts for each team, consisting of a galvanized chain-link fence surround with a simple shingled roof to keep the players out of the elements during play. To the outside of the dugouts will be a concrete pad, on which will be located existing five-row portable bleachers for spectators. The concrete pad on the east side will be further extended beyond the bleachers to accommodate an equipment storage shed. The storage shed will be store-bought rather than built on site, and the appearance of the shed is complimentary to the surrounding property. Pictures of the shed are attached as presented in the display area of the Palatine Home Depot store.

The infield area will be bare and a mix of calcined clay, sand and other ballfield infield material, with a permanently placed pitching rubber and base anchors at first, second and third bases and home plate. Bases are removed from the field in-between games. The outfield will be grass, consisting of a seed mix typical of all District 214 schools for athletic play surfaces.

**Exhibit received September 8, 2016**

Buffalo Grove High School ■ Community Education ■ Elk Grove High School  
John Hersey High School ■ Newcomer Center ■ Prospect High School ■ Rolling Meadows High School  
The Academy at Forest View ■ Vanguard School ■ Wheeling High School

No artificial lighting will be provided for the field. Games are scheduled according to the softball season calendar of the IHSA and as coordinated on an annual basis by the MSL Board of Control. The dimensions of the playing field and outfield are to be as required by the IHSA and NFHS for softball.

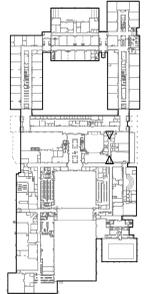
Wheeling High School currently maintains a competitive girls' softball program, has for several years, and does not anticipate any change to the program for the future. The spectators and participants are expected to be the same number as in past years, with current facilities for parking and access being maintained as they currently exist.

I look forward to meeting with you at the next meeting of the Commission and having the opportunity to discuss this project with you further. In the interim, I can be reached at my office at (847) 718-7619 or at [ted.birren@d214.org](mailto:ted.birren@d214.org) should you have any questions.

Thank you for your consideration,



Ted Birren  
Director of Operations  
Township High School District 214



**KEY PLAN**  
**SCOPE DOCUMENT**  
 This drawing indicates the general scope of the project in terms of architectural design concept, the dimensions of the building, the major architectural elements and the type of structural, mechanical, and electrical systems. As Scope Documents, the Drawings do not necessarily indicate or describe all work required for full performance and completion of the requirements of the Construction Documents, on the basis of the general scope indicated or described. The Contractor shall furnish all items required for the proper execution and completion of the work.

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**PROJECT**  
 CPP 2016-2017  
 renovations at  
**WHEELING HIGH SCHOOL**  
 900 South Elmhurst Road  
 Wheeling, IL 60090

THE BOARD OF EDUCATION  
 TOWNSHIP HIGH SCHOOL  
 DISTRICT 214  
 2121 South Goebbert Road  
 Arlington Heights, Illinois  
 60005

CLIENT PROJECT NUMBER

STR PROJECT NUMBER

NUMBER	DATE	COMMENTS
1	09/08/2016	ISSUE FOR PLANNING AND ZONING APPROVAL

DRAWING TITLES

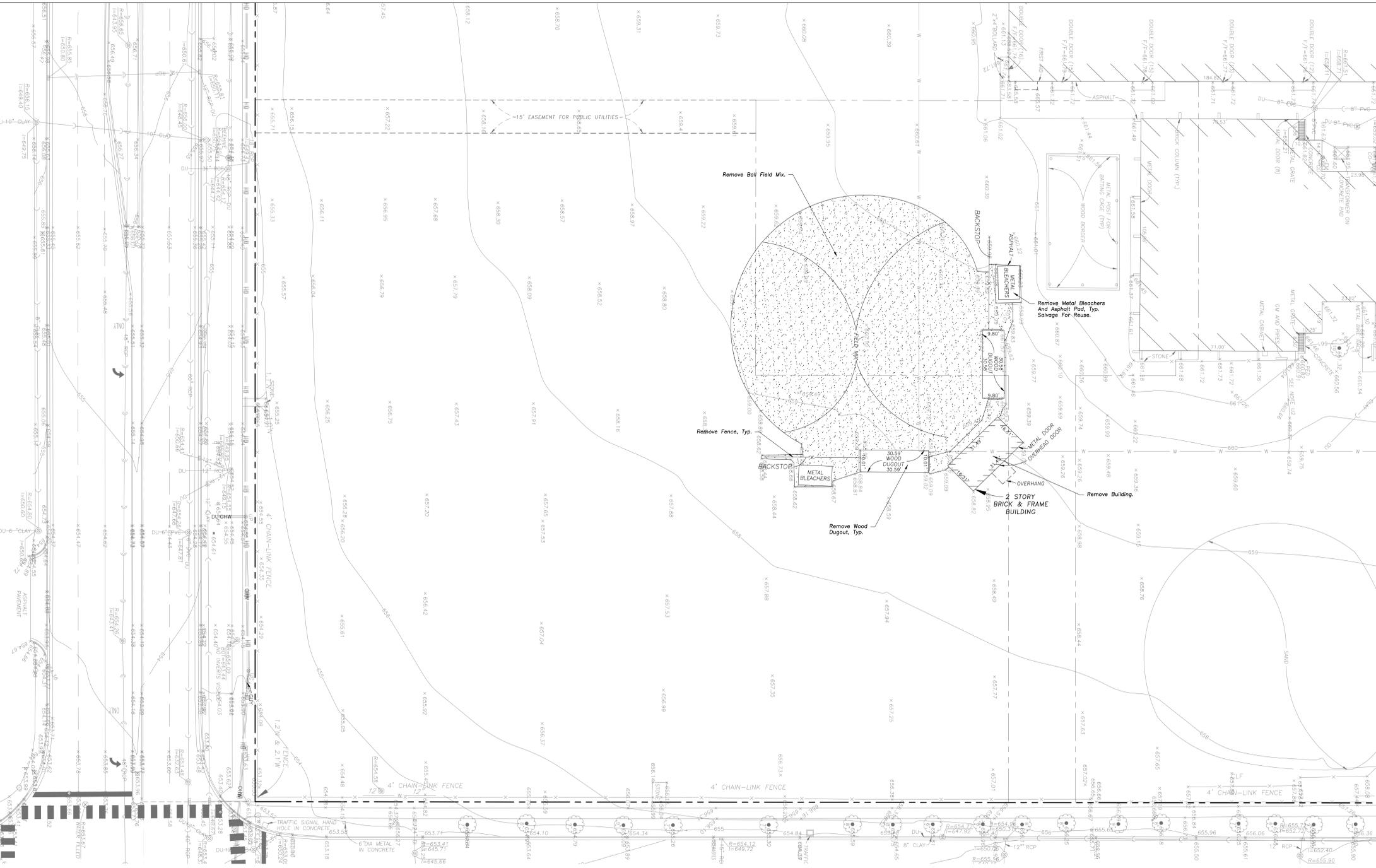
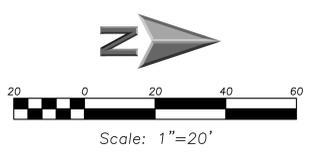
1 09/08/2016 ISSUE FOR PLANNING AND ZONING APPROVAL

DRAWING TITLE

**SITE DEMOLITION PLAN**

SHEET NUMBER

**C-1.0**



**DEMOLITION LEGEND**

- Ball Field Mix Removal (Full Depth)
- Structure Removal

**DEMOLITION NOTES**

1. All Signs to Be Removed Shall Be Salvaged and Stored in the Owner's Facility for Future Use as Applicable.
2. Keep All Village Streets Free and Clear of Construction Related Dirt/Dust/Debris.
3. Coordinate Existing Utility Removal with Local Authorities and Utility Companies Having Jurisdiction.
4. The Existing Building is to Remain Operational During Construction. Therefore, the Temporary Relocation of All Necessary Utilities Serving the Existing Building Shall Be Coordinated Prior to the Commencement of Construction Operations.
5. All Sawcutting Shall Be Full Depth to Provide a Clean Edge to Match New Construction. Match Existing Elevations of Points of Connection for New and Existing Pavement, Curb, Sidewalk, etc. All Street Locations Shown are Approximate and May Be Field Adjusted to Accommodate Conditions, Joints, Material Type, etc. Remove Minimum Amount Necessary for Installation of Proposed Improvements.
6. Provide and Maintain All Necessary Traffic Control and Safety Measures Required During Demolition and Construction Operations Within or Near the Public Roadway.
7. All Light Poles to Be Removed From Private Property Shall Be Removed in Their Entirety, Including Base and All Appurtenances. Coordinate Abandonment of Electrical Lines With Electrical Engineer and Owner Prior to Demolition.
8. Perform Tree Pruning in All Locations Where Proposed Pavement and/or Utility Installation Encroach Within the Existing Drip Line of Trees to Remain. All Trenching Within the Drip Line of Existing Trees to Remain Shall Be Done Radially Away From Trunk If Roots in Excess of 1" Diameter are Exposed. Roots Must Be Cut By Reputable Tree Pruning Service Prior to Any Transverse Trenching. Obtain Approval Of The Architect Prior to Operations For a Variance From This Procedure.
9. Coordinate Tree Removal with Landscape Architect. All Trees to Be Removed Shall Be Removed in Their Entirety and Stumps Shall Be Ground to Proposed Subgrade. Use As Much for Proposed Landscaping Where Applicable and Acceptable to Architect.
10. Provide Tree Protection Fencing Prior to Construction Operations. Maintain Throughout Construction.

**GENERAL NOTES**

1. The Location of Existing Underground Utilities, Such as Watermain, Sewers, Gas Lines, Etc., As Shown on the Plans, Has Been Determined From the Best Available Information and is Given For the Convenience of the Contractor. However, the Owner and the Engineer Do Not Assume Responsibility in the Event That During Construction, Utilities Other than Those Shown May Be Encountered, and That the Actual Location of Those Which are Shown May Be Different From the Location As Shown on the Drawings. Contact Engineer Immediately if Surface and/or Subsurface Features are Different Than Shown on the Drawings.
2. Notify the Engineer Without Delay of Any Discrepancies Between the Drawings and Existing Field Conditions.
3. Notify the Owner, Engineer and the Village of Wheeling A Minimum of 48 Hours in Advance of Performing Any Work.
4. All Areas, On or Off Site, Disturbed During Construction Operations and Not Part of the Work as Shown Herein Shall Be Restored To Original Condition to the Satisfaction of the Owner at No Additional Cost to the Contractor. It is incumbent Upon Contractor to Show That Damaged Areas Were Not Disturbed by Construction Operations.
5. These Drawings Assume That the Contractor Will Utilize An Electronic Drawing File (DWG) and State All Site Improvements Accordingly.
6. No Person May Utilize The Information Contained Within These Drawings Without Written Approval From Eriksson Engineering Associates, Ltd.
7. The Engineer is Furnishing These Drawings For Construction Purposes As a Convenience To the Owner, Architect, Surveyor, or Contractor. Prior To the Use of These Drawings For Construction Purposes, The User Of This Media Shall Verify All Dimensions and Locations of Buildings With The Foundation Drawings And Architectural Site Plan, and Coordinate All Dimensions and Locations of All Site Items. If Conflicts Exist The User Of This Information Shall Contact The Engineer Immediately.
8. Provide An As-built Survey Prepared By A Licensed Professional Land Surveyor in Accordance With The Authorities Having Jurisdiction Which Shall Include As a Minimum All Detention Basins and Best Management Practices, Include All Storm and Sanitary Sewers, Structure Locations, Sizes, Rim and Invert Elevations, Final Detention Volume Calculations For the Basin(s), Watermain and Valve and Appurtenance Locations.
9. The Illinois Department of Transportation Standard Specifications For Road And Bridge Construction Latest Edition, And All Addenda Thereto, Shall Govern The Earthwork And Paving Work Under This Contract Unless Noted Otherwise.

**LEGEND**

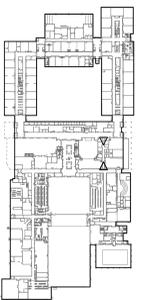
- | EXISTING | PROPOSED                  |
|----------|---------------------------|
|          | Manhole                   |
|          | Catch Basin               |
|          | Inlet                     |
|          | Area Drain                |
|          | Clean Out                 |
|          | Storm Sewer               |
|          | Sanitary Sewer            |
|          | Combined Sewer            |
|          | Water Main                |
|          | Gas Line                  |
|          | Overhead Wires            |
|          | Electrical Cable (Buried) |
|          | Telephone Line            |
|          | Fire Hydrant              |
|          | Valve Vault               |
|          | Buffalo Box               |
|          | Downspout                 |
|          | Hand Hole                 |
|          | Light Pole                |
|          | Utility Pole              |
|          | Sign                      |
|          | Fence                     |
|          | Accessible Parking Stall  |
|          | Curb & Gutter             |
|          | Depressed Curb            |
|          | Curb Elevation            |
|          | Gutter Elevation          |
|          | Pavement Elevation        |
|          | Sidewalk Elevation        |
|          | Ground Elevation          |
|          | Contour Line              |
|          | Deciduous Tree            |
|          | Coniferous Tree           |
|          | Brushline                 |

**PROJECT BENCHMARKS**

1. Project Benchmark:  
 USGS Monument LAK COO 1B (A2852)  
 Monument Described As Located Within The City Of Wheeling Approximately 2.0 Mi East Of Buffalo Grove In Section 2, T42N, R11E. Station is Located in The Southwest Corner of Intersection of IL Rt 21 (Milwaukee Ave) and Lake Cook Rd. Station is Located Between Lake Cook Rd Off Ramp and IL Rt 21 Approximately 240 Ft West of IL Rt 21 and 55 Ft South of Centerline of Lake Cook Rd. Station is 49 Ft Southeast of Light Pole, 45.5 Ft Southwest of Monroie, 7.6 Ft South of Guard Rail, 17 Ft Southeast of West End of Guard Rail, and 2 Ft North of An Orange Fiberglass Whiness Post. Note-Access to Datum Point Through 6 Inch Logo Cap. Datum Point is 0.55 Ft Below Cap. Elevation = 664.62 NAVD 88 Datum

**J.U.L.I.E.**

Note: the exact location of all utilities shall be verified by the contractor prior to construction activities. For utility locations call: J.U.L.I.E. 1 (800) 892-0123



**KEY PLAN**

**SCOPE DOCUMENT**

This drawing indicates the general scope of the project in terms of architectural design concept, the dimensions of the building, the major architectural elements and the type of structural, mechanical, and electrical systems. As Scope Documents, the Drawings do not necessarily indicate or describe all work required for full performance and completion of the requirements of the Construction Documents, on the basis of the general scope indicated or described. The Contractor shall furnish all items required for the proper execution and completion of the work.

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**PROJECT**

CPP 2016-2017  
 renovations at  
**WHEELING HIGH SCHOOL**  
 900 South Elmhurst Road  
 Wheeling, IL 60090

THE BOARD OF EDUCATION  
 TOWNSHIP HIGH SCHOOL  
 DISTRICT 214  
 2121 South Goebbert Road  
 Arlington Heights, Illinois  
 60005

**CLIENT PROJECT NUMBER**

STR PROJECT NUMBER  
**15041**

**DRAWING DATES**

NUMBER	DATE	COMMENTS
1	09/08/2016	ISSUE FOR PLANNING AND ZONING APPROVAL

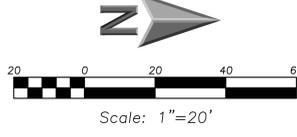
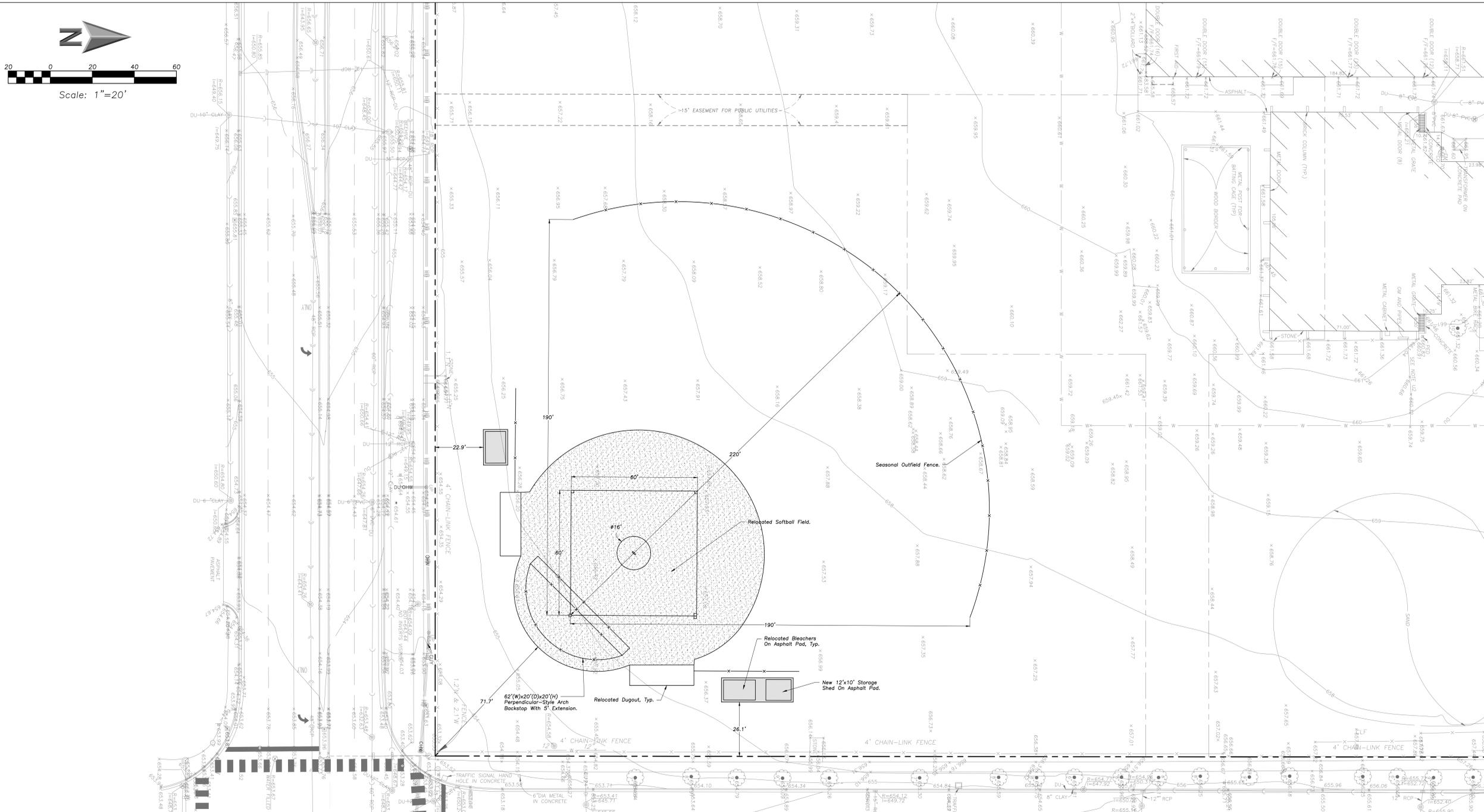
Exhibit received September 8, 2016

DRAWING TITLE

PROPOSED SITE PLAN

SHEET NUMBER

C-2.0



**SOIL EROSION & SEDIMENTATION CONTROL NOTES**

- Illinois Urban Manual Shall Govern All Soil Erosion and Sediment Control, and Related Work.
- Contractor Shall Be Responsible for Compliance With IEPA NPDES and ILRI10 Permit Requirements for Project.
- Soil Disturbance Shall Be Conducted in Such a Manner as to Minimize Erosion. Soil Stabilization Measures Shall Consider the Time of Year, Site Conditions, and the Use of Temporary or Permanent Measures.
- Soil Erosion and Sediment Control Features Shall Be Constructed Prior to the Commencement of Upland Disturbance.
- Temporary Soil Stabilization Shall Be Applied to Topsoil Stockpiles and Disturbed Areas, Where Construction Activity Will Not Occur For a Period of More Than 14 Calendar Days. Temporary Measures Shall Be Applied Within 7 Calendar Days of the End of Active Hydrologic Disturbance. The Sediment Control Measures Shall Be Maintained On a Containing Basis Until the Site is Permanently Stabilized and All Inspections are Complete. Permanent Stabilization Shall Be Completed Within 14 Days after Completion of Final Grading of Soil.
- All Temporary and Permanent Erosion Control Measures Shall Be Removed Within 30 Days After Final Site Stabilization is Achieved or After the Temporary Measures are No Longer Needed. Trapped Sediment and Other Disturbed Soil Areas Shall Be Permanently Stabilized.
- Final Site Stabilization is Defined By the EPA General Permit as Meaning That All Soil Disturbing Activities At the Site Have Been Completed, and That a Uniform Perennial Vegetative Cover With a Density Of 70 Percent Of the Cover For Unpaved Areas Not Covered by Permanent Structures Has Been Established Or Equivalent Permanent Stabilization Measures (Such As the Use of Riprap, Gabions, Or Geotextiles) Have Been Employed.
- All Storm Sewer Structures That Are, Or Will Be, Functioning During Construction Shall Be Protected, Filtered, Or Otherwise Treated to Remove Sediment. The General Contractor Shall Use and Maintain "Dandy Flag" Inlet Protectors (or equal) and Filter Wattles Around the Grate in Landscaped Areas And "Catch-All" Inlet Protectors (or equal, such as Rock Chop Sediment) in Paved Areas to Prevent Siltation and Discharge into Waterways.
- All Temporary and Permanent Sediment And Erosion Control Measures Must Be Maintained, Repaired, And Inspected in Conformance With All Applicable IEPA-NPDES Phase II Requirements.
- Following the Termination of Construction Activities And Issuance Of the Required "Notice of Termination", the Permittees Must Keep a Copy Of the Storm Water Pollution Prevention Plan, Inspection Reports, And Records Of All the Data Used to Complete the Notice of Intent For a Period Of At Least Three Years Following Final Stabilization.
- Install And Maintain Silt Fence At the Perimeter Of the Construction Zone And Wetland Areas And As Shown On the Plans. Maintain Silt Fence Throughout Construction And Until Vegetation Has Been Fully Established.
- Contractor Shall Provide Qualified Soil Erosion and Sediment Control Inspector Services in Accordance with NPDES and Governmental Requirements. Inspections Shall Occur at Every Seven Calendar Days Or Within 24 Hours of a 0.5" or Greater Rainfall Event. Engineer Shall Be Copied on Inspection Logs.
- The Erosion Control Measures Indicated On the Drawings Are the Minimum Requirements. Additional Measures May Be Required As Directed by the Qualified Soil and Erosion Sediment and Control Inspector Or Governing Agency.
- Unless Otherwise Indicated on the Drawings, Stabilize All Disturbed Ground Areas Where Slopes Exceed 6:1 or Within Swales with North American Green Biomat SC150BN Erosion Control Blanket, or Approved Equal.
- Report Releases of Reportable Quantities of Oil or Hazardous Materials if They Occur in Accordance with IEPA NPDES Requirements.
- All Concrete Washout Shall Conform to the "Temporary Concrete Washout Facility" Standards (Code 654) of the Illinois Urban Manual, Latest Edition.
- If Necessary, the SWPPP Shall Be Modified To Reflect Changes Required During the Effective Period Of the IEPA NPDES General Permit No. ILRI10 and Local and County Permits.
- Dewatering of Excavations Shall be Performed in a Manner Such as Through the use of Filter Bags or Polymer Treated Dewatering Cycles, so as to not Discharge Sediment Laden Water into Storm Sewers Tributary to Open Water.

**GEOMETRY NOTES**

- All Dimensions Contained Herein Reference Back Of Curb, Face Of Retaining Wall, Edge Of Pavement, Center of Structure And Outside Face Of Building Foundation Unless Otherwise Noted.
- Refer to Architectural Drawings for Exact Locations of All Buildings.

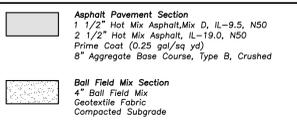
**INTENDED SEQUENCE OF MAJOR SEDIMENT AND EROSION CONTROL MEASURES**

- Install Stabilized Construction Entrance
- Install All Downslope and Sideline Perimeter Controls Before Commencement of Any Ground Disturbing Activity. Do Not Disturb An Area Until it is Necessary For Construction To Proceed.
- Cover and Stabilize Disturbed Areas as Soon as Possible.
- When Practical, Time Construction Activities to Limit Impact From Seasonal Climate Changes or Weather Events.
- Construct Sedimentation Basins and Structures.
- Perform Grading Operations and Installation of Site Infrastructure and Pavement.
- Install Permanent Seeding and Planting.
- Remove Accumulated Sediment From Basins and Along Silt Fence.
- Construction of Infiltration Measures Shall Take Place Following Stabilization of Upstream Disturbed Areas.
- Remove Temporary Sediment and Erosion Control Measures Following Final Stabilization of All Disturbed Areas.

**GRADING NOTES**

- The Grading and Construction of Proposed Improvements Shall Be Done in a Manner Which Will Allow For Positive Drainage, and Not Cause Ponding of Stormwater on the Surface of Proposed Improvements.
- All Landscaped Areas Disturbed By Construction Shall Be Reseeded With 6 Inches (Min.) to 12 Inches (Max.) Topsoil and Sodded Unless Noted Otherwise on the Landscape Drawings.
- Refer to Architectural Drawings for Locations and Patterns of Expansion and Control Joints in Concrete Pavement and Sidewalks.
- Accessible Parking Spaces and Loading Spaces Shall Be Sloped at Maximum 2.0% in Any Direction. Maximum Sidewalk Cross Slopes Shall be 2.0%. Maximum Longitudinal Sidewalk Slope Shall be 4.9%. Contact Engineer if Conflicts Exist.

**PAVING & SURFACE LEGEND**



**GENERAL NOTES**

- The Location of Existing Underground Utilities, Such as Watermain, Sewers, Gas Lines, Etc., As Shown On the Plans, Has Been Determined From the Best Available Information and is Given For the Convenience of the Contractor. However, the Owner and the Engineer Do Not Assume Responsibility in the Event That During Construction, Utilities Other Than Those Shown May Be Encountered, and That the Actual Location of Those Which Are Shown May Be Different From the Location As Shown On the Drawings. Contact Engineer Immediately if Surface and/or Subsurface Features Are Different Than Shown On the Drawings.
- Notify the Engineer Without Delay of Any Discrepancies Between the Drawings and Existing Field Conditions.
- Notify the Owner, Engineer and the Village of Wheeling a Minimum of 48 Hours in Advance of Performing Any Work.
- All Areas, On or Off Site, Disturbed During Construction Operations and Not Part of the Work As Shown Herein Shall Be Restored To Original Condition to the Satisfaction of the Owner at No Additional Cost to the Contractor. It is incumbent Upon Contractor to Show That Damaged Areas Were Not Disturbed By Construction Operations.
- These Drawings Assume That the Contractor Will Utilize An Electronic Drawing File (DWG) and State All Site Improvements Accordingly.
- No Person May Utilize the Information Contained Within These Drawings Without Written Approval From Eriksson Engineering Associates, Ltd.
- The Engineer is Furnishing These Drawings For Construction Purposes to the Owner, Architect, Surveyor, or Contractor. Prior to the Use of These Drawings for Construction Purposes, the User Of This Media Shall Verify All Dimensions and Locations of Buildings With the Foundation Drawings And Architectural Site Plan, and Coordinate All Dimensions and Locations of All Site Items. If Conflicts Exist The User Of This Information Shall Contact The Engineer Immediately.
- Provide An As-Built Survey Prepared By A Licensed Professional Land Surveyor in Accordance With the Authorities Having Jurisdiction Which Shall Include As a Minimum All Detention Basins and Best Management Practices, Include All Storm and Sanitary Sewers, Structure Locations, Sizes, Rim and Invert Elevations, Final Detention Volume Calculations For the Basin(s), Watermain and Valve and Appurtenance Locations.
- The Illinois Department of Transportation Standard Specifications for Road And Bridge Construction, Latest Edition, And All Addenda Thereto, Shall Govern the Earthwork And Paving Work Under This Contract Unless Noted Otherwise.

**SURVEY PROVIDED BY:**

Plot of Survey and Topography Provided By TFW Surveying & Mapping Inc. For Township High School District 214 on August 20, 2015. Order Number 150751, Project Number 2057.

**PROJECT BENCHMARKS**

- Project Benchmark: USGS Monument LAK COO 1B (A2852) Monument Described As Located Within the City of Wheeling Approximately 2.0 Mi East of Buffalo Grove in Section 2, T42N, R11E. Station is Located in the Southwest Corner of Intersection of IL Rt 21 (Milwaukee Ave) and Lake Cook Rd. Station is Located Between Lake Cook Rd Off Ramp and IL Rt 21 Approximately 240 Ft West of IL Rt 21 and 55 Ft South of Centerline of Lake Cook Rd. Station is 49 Ft Southeast of Light Pole 45.5 Ft Southwest of Monrille, 7.6 Ft South of Guard Rail, 17 Ft Southeast of West End of Guard Rail, and 2 Ft North of An Orange Fiberglass Whiffers Post. Note-Access to Datum Point Through 6 Inch Logo Cap. Datum Point is 0.55 Ft Below Cap. Elevation = 664.62 NAVD 88 Datum

**J.U.L.I.E.**

Note: the exact location of all utilities shall be verified by the contractor prior to construction activities. For utility locations call: J.U.L.I.E. 1 (800) 892-0123

**LEGEND**

EXISTING	PROPOSED
Manhole	Manhole
Catch Basin	Catch Basin
Inlet	Inlet
Area Drain	Area Drain
Clean Out	Clean Out
Storm Sewer	Storm Sewer
Sanitary Sewer	Sanitary Sewer
Combined Sewer	Combined Sewer
Water Main	Water Main
Gas Line	Gas Line
Overhead Wires	Overhead Wires
Electrical Cable (Buried)	Electrical Cable (Buried)
Telephone Line	Telephone Line
Fire Hydrant	Fire Hydrant
Valve Vault	Valve Vault
Buffalo Box	Buffalo Box
Downspout	Downspout
Hand Hole	Hand Hole
Light Pole	Light Pole
Utility Pole	Utility Pole
Sign	Sign
Fence	Fence
Accessible Parking Spot	Accessible Parking Spot
Curb & Gutter	Curb & Gutter
Depressed Curb	Depressed Curb
Curb Elevation	Curb Elevation
Gutter Elevation	Gutter Elevation
Pavement Elevation	Pavement Elevation
Sidewalk Elevation	Sidewalk Elevation
Ground Elevation	Ground Elevation
Contour Line	Contour Line
Deciduous Tree	Deciduous Tree
Coniferous Tree	Coniferous Tree
Bruckline	Bruckline



**STR PARTNERS LLC**  
350 WEST ONTARIO STREET  
SUITE 200  
CHICAGO IL 60654  
  
T 312.464.1444  
F 312.464.0785  
www.strpartners.com

Project Name/Location:  
**WHEELING HIGH SCHOOL  
2017 ADDITIONS AND  
RENOVATIONS**

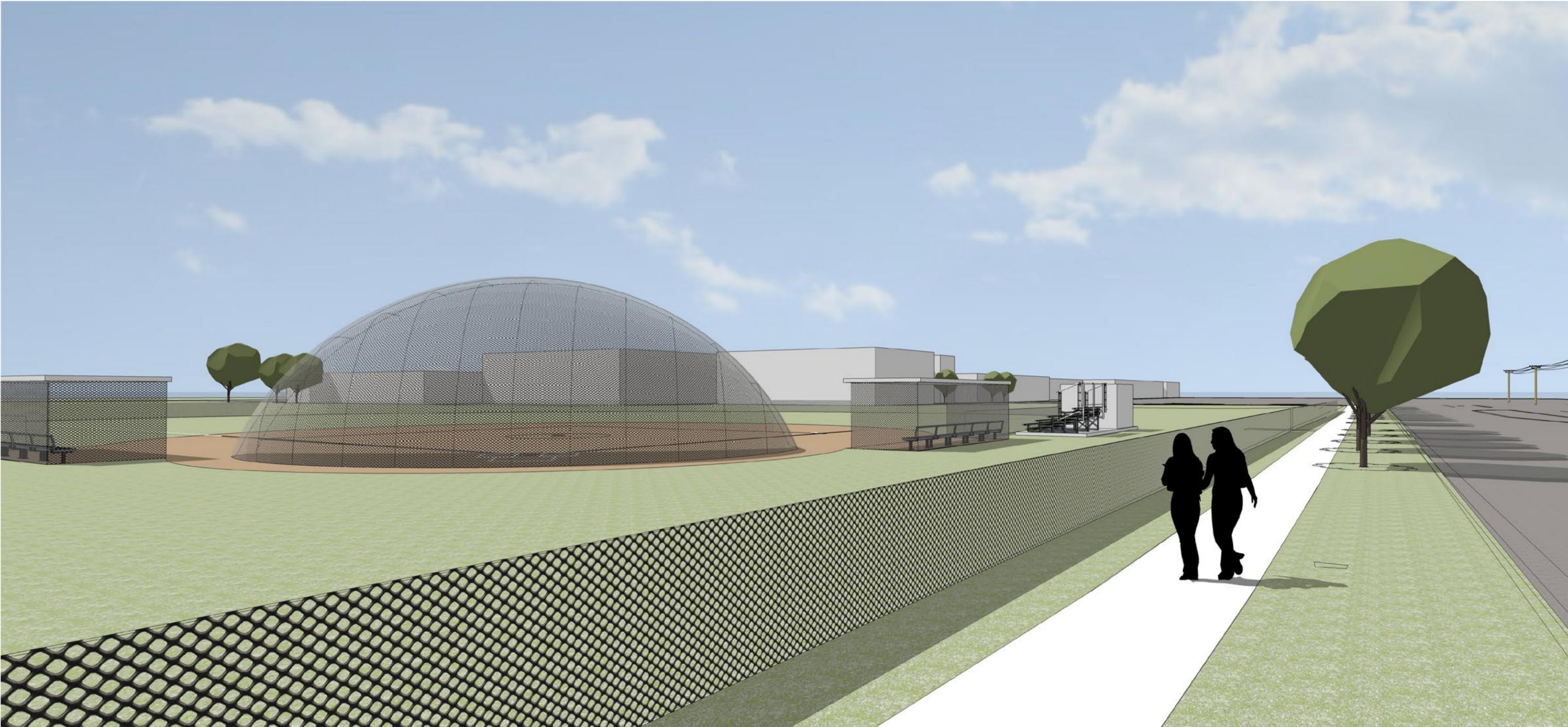
Township High School District 214

Subject:  
**BASEBALL FIELD  
AERIAL**

September 9, 2016

Sketch:  
**1**

Exhibit received September 8, 2016



**STR PARTNERS LLC**  
350 WEST ONTARIO STREET  
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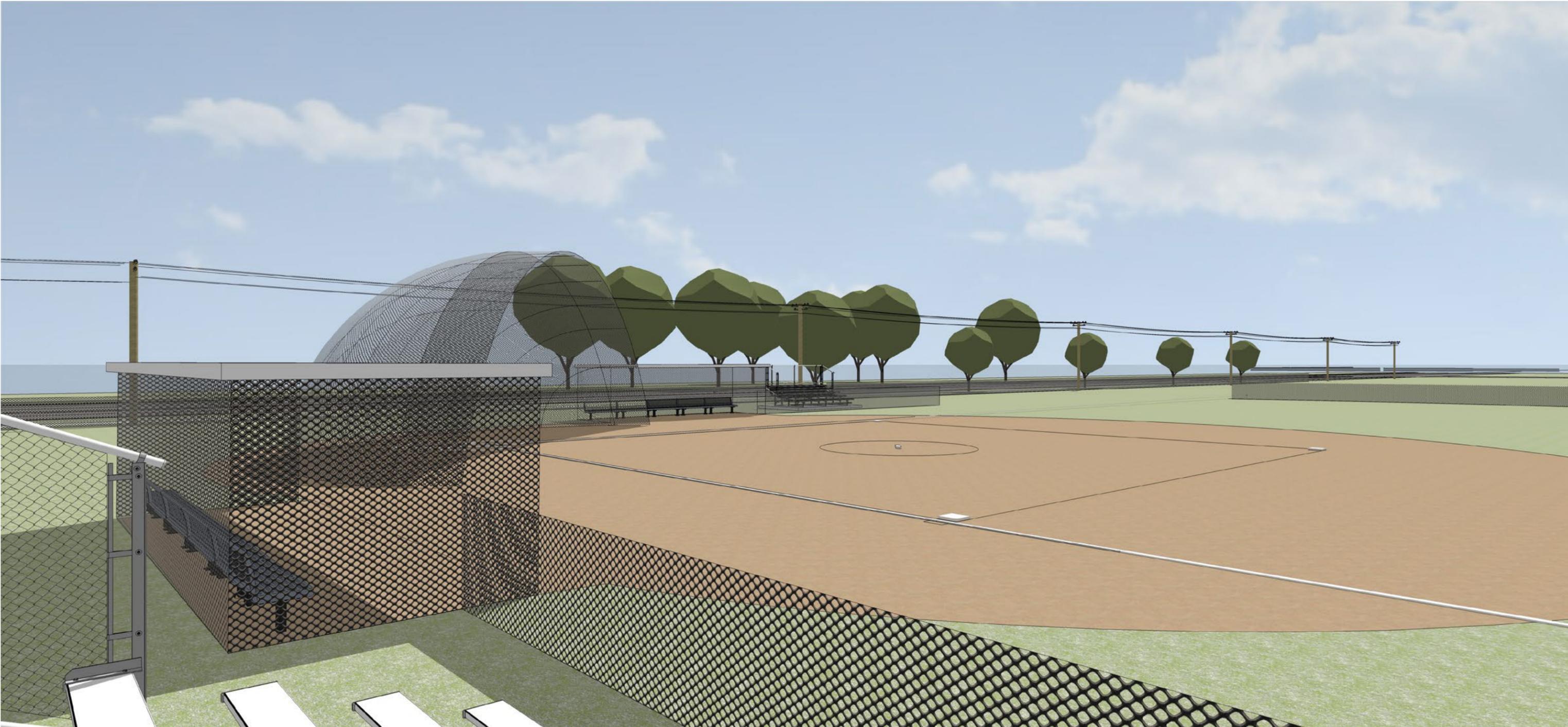
Township High School District 214

Subject:  
**BASEBALL FIELD  
VIEW FROM SIDEWALK**

September 9, 2016

Sketch:  
**2**

Exhibit received September 8, 2016



**STR PARTNERS LLC**  
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SUITE 200  
CHICAGO IL 60654  
  
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Project Name/Location:  
**WHEELING HIGH SCHOOL  
2017 ADDITIONS AND  
RENOVATIONS**

Township High School District 214

Subject:  
**BASEBALL FIELD  
VIEW FROM BLEACHER**

September 9, 2016

Sketch:  
**3**

Exhibit received September 8, 2016



Exhibit received September 8, 2016

- LEGEND**
- BL BUILDING LINE
  - BOL BOLLARD
  - CLF CHAIN-LINK FENCE
  - CON CONCRETE
  - CS CONCRETE
  - DC DOWN SPOUT
  - DI DUCTILE IRON
  - DUE ELEVATION
  - DU DRAINAGE & UTILITY EASEMENT
  - EM ELEVATION
  - EM ELECTRIC METER
  - FF FINISHED FLOOR
  - FM FIRE METER
  - FM GAS METER
  - HB HEADBOLT
  - HH HAND HOLE
  - IN INVERT
  - LSP LANDSCAPE PERIMETER
  - OW OVERHEAD WIRES
  - PE PEDESTAL
  - PU PUBLIC UTILITY EASEMENT
  - PUB PUBLIC UTILITY & DRAINAGE EASEMENT
  - PVC POLYVINYL CHLORIDE PIPE
  - R RIM
  - RCP REINFORCED CONCRETE PIPE
  - RM RETAINING WALL
  - SA SANITARY
  - ST STORM
  - TR TRANSFORMER
  - TS TRAFFIC SIGNAL HAND HOLE
  - TS TRAFFIC SIGNAL
  - UG UNDERGROUND WIRE
  - WM WATERMAIN
  - MAN MANHOLE
  - RO ROUND OPEN GRATE MANHOLE
  - SO SQUARE OPEN GRATE MANHOLE
  - VA VALVE VAULT
  - FL FLARED END SECTION
  - TR TRAFFIC SIGNAL ON POLE
  - LI LIGHT ON 8' ARM
  - SP SPOT LIGHT
  - SN SIGN WITH SIGN NOTE NUMBER
  - BB B-BOX (BUFFALO BOX)
  - FI FIRE HYDRANT
  - BV BROADLEAF VARIETY WITH TRUNK DIAMETER IN INCHES
  - B6-12 B6-12 CONCRETE CURBING
  - DC DEPRESSED CURBING
  - CON CONCRETE

NORTH  
1" = 50'

**EASEMENT NOTES:**

1. GRANT OF EASEMENT RECORDED AS DOCUMENT 26610035 GRANTS A WATER MAIN EASEMENT OVER WHEELING HIGH SCHOOL PROPERTY. DOCUMENT LISTS EXHIBIT A ATTACHED. NO EXHIBIT A WAS ATTACHED OR PROVIDED.
2. GRANT OF EASEMENT RECORDED AS DOCUMENT 20581479 GRANTS AN EASEMENT TO COMMONWEALTH EDISON COMPANY "WITHIN HINTZ ROAD" AND ALSO "THE WEST 25 FEET OF THE SOUTH 25 FEET OF THAT PART LYING NORTH OF THE NORTH LINE OF SAID HINTZ ROAD". DOCUMENT DOES NOT LIST ANY DOCUMENT NUMBER OR RIGHT OF WAY WIDTH FOR HINTZ ROAD AT THE TIME OF GRANT, EASEMENT CANNOT BE DEPICTED.
3. GRANT OF EASEMENT RECORDED AS DOCUMENT 16607265 GRANTS AN EASEMENT TO NORTHERN ILLINOIS GAS COMPANY WITHIN "THE WEST SIDE OF ILLINOIS ROUTE 83". DOCUMENT DOES NOT LIST ANY DOCUMENT NUMBER OR RIGHT OF WAY WIDTH FOR ILLINOIS ROUTE 83 AT THE TIME OF GRANT, EASEMENT CANNOT BE DEPICTED.
4. GRANT OF EASEMENT RECORDED AS DOCUMENT 16607266 GRANTS AN EASEMENT TO NORTHERN ILLINOIS GAS COMPANY WITHIN "THE WEST SIDE OF ILLINOIS ROUTE 83". DOCUMENT DOES NOT LIST ANY DOCUMENT NUMBER OR RIGHT OF WAY WIDTH FOR ILLINOIS ROUTE 83 AT THE TIME OF GRANT, EASEMENT CANNOT BE DEPICTED.

NOTE:  
NO LEGAL DESCRIPTION PROVIDED TO THE SURVEYOR

NOTE:  
AT THE REQUEST OF THE CLIENT, THE DATUM FOR THIS SURVEY IS BASED ON STATE PLANE COORDINATES, DUE TO THE REQUESTED DATUM, THE BEARING SHOWN HEREON DO NOT MATCH RECORDED DOCUMENTS. REFER TO RECORDED DOCUMENTS FOR COMPLETE DETAILS ON RECORDED BEARING SYSTEM.

NOTE:  
SOUTH STREET HERETOFORE DEDICATED HAS PURPORTEDLY BEEN VACATED. THIS SURVEYOR HAS NO RECORD OF THE VACATION.

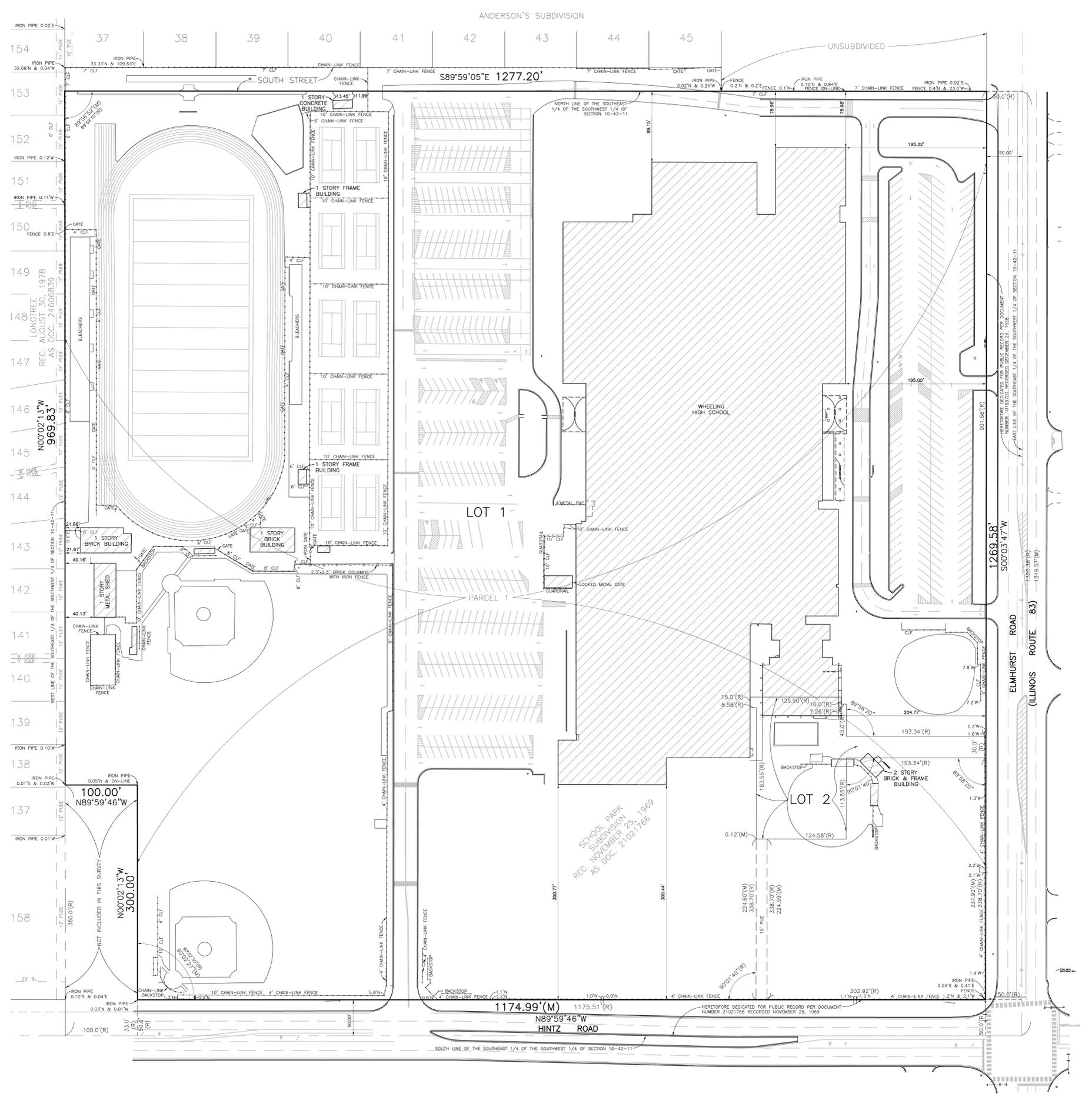
**TFW SURVEYING & MAPPING, INC.**  
LAND SURVEYING - TOPOGRAPHIC MAPPING - CONSTRUCTION LAYOUT  
888 EAST BELVIDERE ROAD SUITE 413 CHRYSLER BLVD  
847-548-6400 FAX 847-548-6699  
tfw@tfwsurvey.com www.tfwsurvey.com

**PLAT OF SURVEY**  
OF

PARCEL 1:  
LOTS 1 AND 2 IN SCHOOL-PARK SUBDIVISION, BEING A SUBDIVISION OF PART OF THE SOUTHWEST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 25, 1969 AS DOCUMENT NUMBER 21021766, IN COOK COUNTY, ILLINOIS.

PARCEL 2:  
LOT 159 IN LONGTREE, BEING A SUBDIVISION OF THE SOUTH 1/2 OF THE SOUTH 1/2 OF THE NORTHWEST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN AND ALSO THE SOUTHWEST 1/4 OF THE SOUTHWEST 1/4 (EXCEPTING THE WEST 1/2 OF THE SOUTH 1/2 OF THE SOUTH 1/2 OF THE SOUTHWEST 1/4 OF SAID SOUTHWEST 1/4) OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED AUGUST 30, 1978 AS DOCUMENT NUMBER 24606839, IN COOK COUNTY, ILLINOIS.

Exhibit received September 8, 2016



**AREA SUMMARY**

PARCEL 1	1,590,269 SQ. FT. OR 36.5075 ACRES
PARCEL 2	134,368 SQ. FT. OR 3.0847 ACRES
<b>TOTAL</b>	<b>1,724,637 SQ. FT. OR 39.5922 ACRES</b>

NO TITLE REPORT PROVIDED TO THIS SURVEYOR TO AID IN THE PREPARATION OF THIS SURVEY.

NOTE:  
TFW SURVEYING & MAPPING, INC. HAS BEEN COMMISSIONED TO PERFORM A BOUNDARY SURVEY OF ONLY THAT REAL ESTATE AS LEGALLY DESCRIBED ABOVE. ALL DATA AS SHOWN HEREON, BUT LYING BEYOND THE BOUNDARY LIMITS AS LEGALLY DESCRIBED ABOVE, INCLUDING (BUT NOT LIMITED TO) LOT LINES, EASEMENTS AND SETBACK LINES IS UNOFFICIAL AND INCOMPLETE AND IS SHOWN FOR INFORMATIONAL PURPOSES ONLY. THIS SURVEY DOES NOT INTEND TO VERIFY OR SUBSTANTIATE EASEMENTS OR BUILDING LINES (OR THE VACATION OF SAME) ON ADJOINING PROPERTIES (UNLESS OTHERWISE SPECIFICALLY REFERENCED IN A TITLE COMMITMENT AS BEING BENEFICIAL TO OR AN ENCUMBRANCE ON THE PROPERTY AS LEGALLY DESCRIBED ABOVE). REFER TO A PLAT OF SURVEY BY OTHERS AND / OR SET PUBLIC RECORD DOCUMENTS FOR COMPLETE DETAILS PERTINENT TO ALL ADJOINING PROPERTIES.

THE INTENT OF THIS SURVEY IS TO SHOW AT OR ABOVE GRADE IMPROVEMENTS ONLY. IT IS POSSIBLE THAT BELOW GRADE IMPROVEMENTS EXIST THAT THIS SURVEYOR IS NOT AWARE OF. IN SOME INSTANCES THIRD PARTY UTILITY LOCATING SERVICES HAVE PLACED WITNESS MARKERS AT GRADE TO INDICATE SOME BELOW GRADE IMPROVEMENTS OR UTILITIES. IF MARKED IN FIELD, SAID WITNESS MARKS HAVE BEEN LOCATED AND ARE SHOWN HEREON. ADDITIONAL BELOW GRADE IMPROVEMENTS OR UTILITIES MAY ALSO EXIST THAT WERE NOT MARKED BY THIRD PARTY UTILITY LOCATING SERVICES FOR THE BENEFIT OF THIS SURVEY.



STATE OF ILLINOIS 155  
COUNTY OF LAKE

I, THOMAS F. WASILEWSKI, ILLINOIS PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT I HAVE SURVEYED THE PROPERTY AS DESCRIBED ABOVE AND THAT THE PLAT HEREON DRAWN IS A REPRESENTATION OF SAID SURVEY. DIMENSIONS ARE SHOWN IN FEET AND DECIMAL PARTS THEREOF. THIS PROFESSIONAL SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

CERTIFIED AT CHRYSLER BLVD, ILLINOIS THIS 20th DAY OF AUGUST, 2015

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2915  
LICENSE EXPIRES NOVEMBER 30, 2016

**REQUEST FOR PLAN COMMISSION ACTION**  
**STAFF PROJECT REVIEW**

**TO:** Chairperson Ruffatto and Members of the Wheeling Plan Commission

**FROM:** Andrew C. Jennings, Director of Community Development  
Brooke A. Jones, Senior Planner

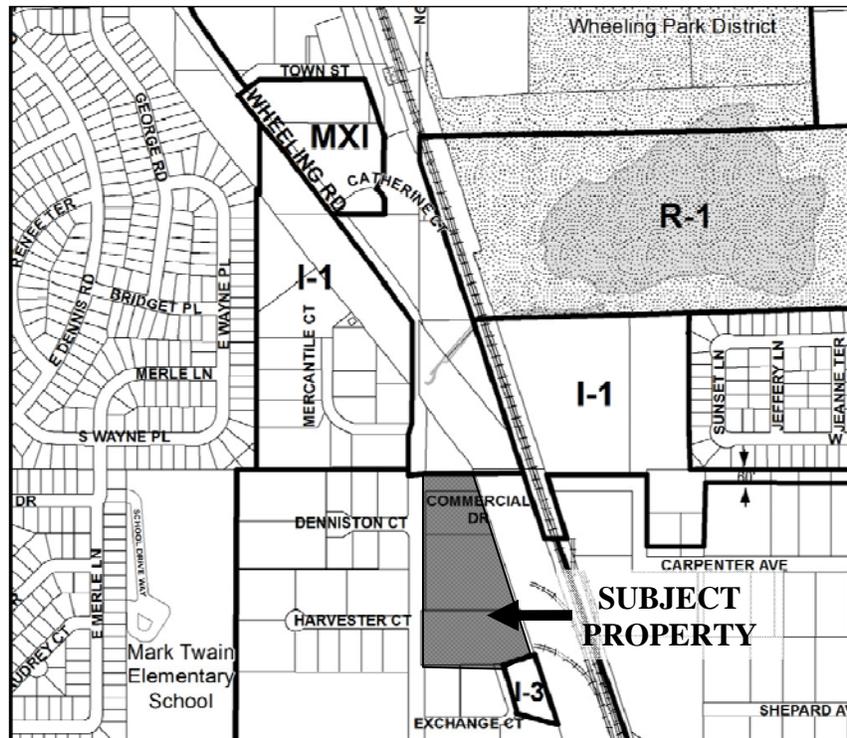
**RE:** **Docket No. 2016-14A,B,C&D**  
**Builders Asphalt**  
**581-571 Wheeling Road**  
**(2016-14A) Text Amendments to Title 19, Zoning, Related to ‘Concrete / Asphalt Plant’**  
**(2016-14B) Rezoning of the Property from I-2 Limited Industrial District to I-4 Heavy Industrial District**  
**(2016-14C) Special Use-Site Plan Approval of an Asphalt Plant**  
**(2016-14D) Variations from Title 19, Zoning, Relating to the Construction of an Asphalt Plant**

**DATE OF REPORT:** September 15, 2016

**DATE OF MEETING:** September 22, 2016

**PROJECT OVERVIEW:** The petitioner is seeking multiple zoning actions to relocate an asphalt plant to 571 Wheeling Road, which is currently zoned I-2 Limited District.

**LOCATION MAP:**



**GENERAL**

**PROPERTY**

**INFORMATION**

<b><u>Applicant Name:</u></b>	Builders Asphalt
<b><u>Property Owner:</u></b>	571 Wheeling LLC
<b><u>Common Property Address:</u></b>	581-571 Wheeling Road
<b><u>Common Property Location:</u></b>	Located on the east side of S. Wheeling Road, across from Dennison Court and Harvester Court.
<b><u>Neighboring Property Land Use(s):</u></b>	North: Industrial South: Industrial West: Industrial East: Industrial
<b><u>Comprehensive Plan Designation:</u></b>	Industrial
<b><u>Property size:</u></b>	7.27 acres (total lot)
<b><u>Existing Use of Property:</u></b>	Industrial (some vacant)
<b><u>Proposed Use of Property:</u></b>	Asphalt plant
<b><u>Existing Property Zoning:</u></b>	I-2 Limited Industrial District
<b><u>Previous Zoning Action on Property:</u></b>	None.

**BACKGROUND INFORMATION**

The asphalt plant at 231 S. Wheeling Road has been in operation for approximately fifty (50) years, with Orange Crush LLC has operating the plant for approximately thirty (30) years. The company is now proposing to relocate the plant roughly 650' south to an assemblage of parcels also on Wheeling Road. In conjunction with the relocation, the majority of the plant equipment would be replaced.

The subject property, once assembled, would total 7.27 acres in total area, roughly twice the size of the existing plant. The assemblage includes 571 S. Wheeling Road (formerly Main Steel), the unimproved right-of-way of Commercial Drive (Village parcel), and an adjacent industrial/office building at 581 S. Wheeling Road.

The petitioner is seeking multiple zoning actions to relocate an asphalt plant to 581-571 Wheeling Road, which is currently zoned I-2 Limited District. The following is a summary of the proposed zoning requests:

1. Amend the Zoning Code to define the proposed use (concrete / asphalt plant);
2. Rezone the property from I-2 Limited Industrial District to I-4 Heavy Industrial District;
3. Grant special use / site plan approval for the proposed asphalt plant; and
4. Grant zoning variations related to heights of accessory structures (silos, tanks, and drum tower), reduction in minimum required green space, height of the fence/screening and allowance for more than one principal building.

**UPDATE FROM PLAN COMMISSION CONCEPT REVIEW MEETING**

On June 9, 2016 Plan Commission conceptually reviewed the Orange Crush/Builders Asphalt proposal to establish an asphalt plant at the subject property. At the concept review meeting, the Plan Commission had the following comments (**petitioner responses provided in bold**):

1. The trees along Wheeling Road should be greater than 3” caliper to provide greater screening immediately; **As shown on the Landscape all trees installed along Wheeling Road were specified to be greater than 3” caliper.**
2. Provide neighborhood meeting minutes; **Neighborhood meeting minutes from May 31, 2016 have been provided.**
3. Provide screening on the east side; **The east side of the property has a minimum of 6’ of green space which will be planted with Dark Green Arborvitae.**
4. Provide a truck stacking plan; **The traffic plan shows the truck stacking plan.**
5. Consider closing one access drive to achieve more green space; **As shown on the site plan, we have eliminated two of the existing driveways off Wheeling Road.**
6. Address air quality sediment issues, storm water quality and noise issue; **We have submitted a report from Green Toxicology, LLC to address the air quality and Shiner Associates, Inc. to address the noise issue.**
7. Address silo height and placement; **The size of the silos has been reduced from 300 ton to 250 ton, this lowered the height to 74’ which is only a couple feet taller than existing asphalt plant silo height.**
8. Address access to the ComEd right-of-way; **We have decided not to pursue the ComEd ROW.**
9. Property beautification screening on the east side of property is encouraged; **The landscape plan which shows substantial landscaping and screening around the entire property.**
10. Clarify timing of operation; The hours of operation will be 6am to 6pm Monday through Saturday. The plant typically is open from April to December depending on the weather. This is the same operation hours at the existing plant.
11. List fence height variation on list of requested variations; **Along Wheeling Road, we will have a 10’ chain link fence with gray slats, on the south side will be a 6’ fence, along the east side will be concrete block 12’ high and north side of the property will be a 8’ fence.**
12. Address height of the piles; **The aggregate piles will not exceed 12’ in height.**

**ZONING CODE TEXT AMENDMENTS**

**Chapter 19-01, Definitions, Sections 19.01.010:** The petitioner is proposing to add the following definition to the Zoning Code:

**Concrete / Asphalt Plant**

A facility for the mixing and crushing and the storage and transporting of raw or recycled aggregate, along with concrete, and liquid asphalt to make Asphalt Cement Concrete (A.C.C.) or concrete for the purpose of sale.

**Appendix A (Use Table):** The petitioner is proposing to add the use classification of “Concrete / Asphalt Plant” as a Special Use in the I-4 Heavy Industrial District.

### **REZONING**

The petitioner is requesting that the entire property be rezoned from I-2 Limited Industrial District to I-4 Heavy Industrial District. It should be noted that currently only one property in the Village is zoned I-4: The Village Public Works facility at 77 W. Hintz Road. Staff has provided a rezoning exhibit.

### **ZONING VARIATIONS**

The proposal requires three zoning variations. The petitioner has requested the following three variations:

1. Reduction in the required minimum green space from 25% to 13% of the site;
2. Increase in the maximum allowed accessory building height from 20-feet to 74-feet for the tallest proposed accessory structure (silos). Other proposed accessory structures also require height variations including the liquid storage tanks (49-feet) and drum tower (54-feet); and
3. Increase in maximum allowed outdoor storage fence/screen from 8-feet to 12-feet. The proposed fence height along the east property line (Wheeling Road) will be 10-feet. The proposed storage bin screen along the east (railroad tracks) property line will be 12-feet.

### **SITE PLAN REVIEW**

**Scale of Site Plan:** 1' = 80'

**Proposed General Site Layout:** The site is configured to accommodate the function of the asphalt plant. The site is divided into three primary areas: material production/loading area, material storage area, and recycling area. The production/loading area (the part of the operation that is most accurately called the asphalt plant), occurs in the middle of the site. There will be four 74-foot tall silos 40-feet from the front property line. The petitioner anticipates expanding the number of silos in this area to a total of eight in the future. Staff is suggesting a condition of approval that would allow for a total of eight silos (as shown on the site plan). There are also three 48-foot tall vertical liquid asphalt tanks and a 54-foot tall drum tower. Other features of the production/loading area include the baghouse, multiple feed bins and a conveyor. In this area are also two buildings: a small one-story office building and a small two-story lab/control room. The parking is adjacent to the office building. The material storage bins are to the east of the asphalt plant along the train tracks. The south end of the site is devoted to the recycler. The recycling area consists of a concrete pile, an asphalt pile, a road material recycler and conveyor to recycle bins. Stormwater basins are located at the southeast corner of the site and the northern edge.

**Total Number of Parking Spaces:** The two proposed buildings combined have a total office area of 1,500 sq. ft. and 1,500 sq. ft. of lab (active manufacturing) area. The total required parking for these uses is nine parking stalls. Ten parking stall has been provided.

**Site Circulation:** A full traffic study has been provided. Trucks will enter the site through the south drive and exit out the north drive. Truck turning diagrams have been provided in the traffic study to demonstrate the three proposed truck movements. It should be noted that the site plan used for the truck turning diagrams is not consistent with proposed site plan. While the site plan from the traffic study indicates four driveways, only two are being accessed by the trucks, which is consistent with the two-driveway proposed site plan. The petitioner should verify that the proposed truck turns will be viable based upon the actual proposed site plan. Furthermore, staff has concerns with potential left-turning trucks into the site, which may conflict with left-turning vehicles onto Harvester Court. The truck turning diagrams provided show only right-turning trucks into the site. Is the petitioner restricting left-turns into the site? If so, this would address staff's concern regarding the offset drives and lack of left-turn lane on Wheeling Road. In addition, the Fire Department has concerns regarding fire access drives, overhead obstructions, and roadway access over the underground vaults. Staff suggests that the petitioner should address these concerns with a written response and revised truck-turning diagrams.

**Bicycle parking:** Bike parking for industrial uses may be provided within the buildings. Outdoor bicycle parking may be unsafe for this facility.

**Site Lighting:** A mix of building/structure mounted lights and pole lights are proposed. The light poles are 22.5-feet in height. The proposed illumination levels meet the requirements of the Zoning Code.

**Ownership:** The petitioner owns the 571 and 581 Wheeling Road parcels. The Village owns the Commercial Drive right-of-way at the northern edge of the site. Prior to permit approval, the Village will need to vacate the Commercial Drive right-of-way for use by the petitioner. Staff has added a condition of approval to address this issue.

**Sidewalks:** There is an existing sidewalk in the Wheeling Road right-of-way.

### **APPEARANCE REVIEW**

**Building Sizes:** The one-story office building is approximately 1,200 sq. ft. The two-story laboratory/control room building is approximately 2,700 sq. ft. in size. The control room is located on the second floor and is only a small portion of first floor.

**Elevation Plan Review:** The most visible features of the site, when viewed from Wheeling Road will be the silos, vertical liquid storage tanks and conveyor associated with the plant. Most of the buildings, material piles, and other lower features of the plant will be hidden by the 10-foot fence along Wheeling Road. An elevation plan for the asphalt plant features is provided. Building elevations are also provided.

**Exterior Building Materials/Colors:** The primary wall material of the buildings will be face brick. The building elevation plan indicates the second story control room will have gray siding to match paint. Staff does not understand what materials and colors are matching. The Plan Commission may wish to see brick and siding material and color samples. .

**Screening:** A chain link fence with gray slats is proposed for three sides of the property (west, north and south). The west property line (Wheeling Road) will have a 10-foot high fence, which requires a zoning variation. The south side will have a 6-foot fence. The north side will have an 8-foot fence. Along the east (adjacent to the railroad tracks) will be 12-foot high concrete block walls, which are the back sides of the storage bins, and also requires a zoning variation for its height. To soften the view of the bins are a line of 6-foot tall arborvitae. The Plan Commission may wish to discuss the proposed fence material.

### **LANDSCAPING PLAN REVIEW**

**Existing Landscaping to Remain:** 20 trees will remain on the site. 14 deciduous trees will remain in the landscape buffer along Wheeling Road. 6 evergreen trees will remain at the southeast corner of the site.

**Proposed Landscaping:** The petitioner is proposing improvements along Wheeling Road. This area will include ornamental grass, ornamental trees, dwarf evergreens, deciduous shrubs, and flowering shrubs. In the northern stormwater basin will be stormwater seed mix and a prairie seed mix. At the southeastern stormwater basin will be a prairie seed mix.

### **STANDARDS FOR REZONING**

Following are standards for rezoning with the petitioner's responses. (**Senior Planner comments are in bold.**)

1. Will the proposed rezoning comply with the intent and purpose of Title 19, Zoning? (Explain how the proposed uses allowed by the rezoning will conform to the zoning code.)

“Applicant seeks a text amendment in order to allow establishment as a special use of an asphalt plant with its associated uses in the I-4 district. There is currently operating an asphalt plant which has operated for over 40 years. The text amendment will allow for the relocation for the relocation and improvement of the existing asphalt plant. The plant represents a use which is needed both within the Village and nearby areas as the predominance of roads are constructed of Asphalt. The area surrounding the subject property is all zoned industrial and used for industrial uses so the establishment of the I-4 District in connection with the proposed special use for the Asphalt plant will be in character with the surrounding area.”

**The intent of the rezoning, in combination with the proposed text amendments, is to establish appropriate limitations on the unique land use that is proposed. The Village previously created the I-4 Heavy Industrial District specifically to accommodate such unique open land uses. These types of uses are characterized by visible machinery, outdoor material storage, truck parking, and frequent use of construction equipment. The proposed rezoning is consistent with the limited application of the I-4 district to impose the maximum restriction on heavy/open industrial land uses.**

2. Will the proposed rezoning comply with the Comprehensive Plan, Official Map and all other plans and policies adopted by the Village? If not, explain why the proposed use is in the best interest of the Village. (Explain in detail how the uses allowed by the rezoning follow the Comprehensive Plan or how a rezoning contrary to the Comprehensive Plan is appropriate.)

“The comprehensive plan calls out this area for Manufacturing uses. The rezoning combined with the special use and its conditions bring the facility into conformance with the goals and intents of the Comprehensive Plan. Granting the amendment allows for the existing facility to be relocated which allows for the existing location to be redeveloped more in conformance with the Village’s plans for Properties along or near Dundee Road.”

**The subject site is shown as “Industrial” on the Comprehensive Plan. The proposed rezoning furthers the vision of the Comprehensive Plan in that it would facilitate a change in land use at 231 S. Wheeling, which is shown as Open Space / Mixed Use on the Comprehensive Plan.**

3. How have the physical or economic conditions pertaining to the subject area changed, making the existing zoning inappropriate and the proposed rezoning appropriate? (Explain why the current zoning designation is inappropriate. What specific physical or economic conditions indicate that a change in zoning is required?)

“The proposed I-4 zoning district, combined with the special use renders the zoning appropriate for the property. The surrounding area is all zoned manufacturing. The special use designation will restrict the property to the proposed use and will not allow the other non-regulated I-4 uses from being established.”

**There is no current zoning district which allows the proposed land use. The rezoning is necessary, in conjunction with the text amendment, in order for the proposed use to locate anywhere within the Village of Wheeling.**

4. How is the proposed rezoning desirable and needed in the Village? (Why is the new zoning district needed? What does the rezoning accomplish? How will it benefit the Village?)

*“The rezoning to I-4 will permit the Asphalt Plant to (1) operate in a location with similar (though not identical) uses; (2) operate in an area of nearby industrial uses adjacent to railroad tracks; and (3) maintain an appropriate distance from residential uses.”*

**The proposed rezoning and text amendment is needed in order for the proposed land use to locate within the Village.**

5. How will the proposed rezoning be compatible with and not unduly depreciate the use and value of the surrounding property? (Consider the types of uses that could be permitted by the rezoning. Are these uses compatible with existing neighboring land uses? Will the potential uses have a negative impact on surrounding property values?)

“The rezoning of the subject property will allow for the relocation of the existing asphalt plant away from the Town Center Development to a location with no impact on the surrounding industrial uses. The use is needed at this location due to the nature of asphalt

production and the need for the plant to be located nearby the roads and parking lots being paved.”

**The land use facilitated by the proposed rezoning should be configured to minimize potential negative impacts. The special use-site plan review by the Plan Commission should address the potential negative impacts.**

6. How will the proposed rezoning contribute to a rational pattern of land uses which is beneficial to the Village? (Is the proposed rezoning sensible? Are intense uses that create traffic, noise, odor, light, or smoke going to be permitted along a narrow residential street?)

“Locating the facility on a road which carries industrial traffic within an area that has industrial uses is the most rational area to relocate the existing facility. The studies included with the application demonstrate that there are no negative impacts that will be created by the new facility (an improvement of the existing plant) The fact that the existing plant has operated for 50 years without complaint demonstrates that the new location, of a ‘state of the art’ plant will have no negative impacts. The relocation of the existing plant will benefit the Village through relocation from near the Town Center development.”

**The proposed rezoning, combined with a zoning code text amendment, would establish a very limited zoning framework for concrete/asphalt plants. The land use facilitated by the rezoning would be configured to minimize the potential negative impacts on adjacent properties.**

### STANDARDS FOR SPECIAL USE

Following are standards for special use with the petitioner's responses in italics. (**Senior Planner comments are in bold.**)

1. State why the Special Use is necessary for the public convenience at the proposed location.

*“The Village of Wheeling has roads that are made of asphalt as well as residents have driveway and buildings have asphalt parking lot. Asphalt can only be delivered a certain distance prior to installation. Having the plant in the community will help keep the cost of road improvement low.”*

**Asphalt production requires proximity to the construction site. There are few locations centrally located in Wheeling that could accommodate the proposed use without significant negative impacts on surrounding land uses.**

2. State how the Special Use will not alter the essential character of the area in which it is to be located.

*“The current asphalt plant property will be relocated roughly ¼ mile south on Wheeling Road. The development occurred adjacent and near our existing plant evidence that the*

*establishment of a new state of the art facility will not cause any changes to the industrial character of the area.”*

**Other properties in the area have outdoor storage yards. Provided the facility is adequately screened and landscaped, it does not appear that the essential character of the area will not change.**

3. State how the location and size of the Special Use, the nature and intensity of the operation involved in or conducted with it, the size of the site in relation to it, and the location of the site with respect to streets giving access to it will be in harmony with and not impede the normal, appropriate and orderly development of the district in which it is to be located and the development of surrounding properties.

*“The proposed location is currently in a developed industrial area, the only new development would be at the existing location. With our relocation the Village can move forward with redevelopment of the downtown. We do not see a negative impact to the street system since the current operation uses Wheeling Road.”*

**Upon review of the noise and environmental studies, it appears that the proposed land use should have little, if any, negative impact on the surrounding properties. Staff has questions regarding the traffic study, especially with regard to the status of left turns into the facility, which may impact traffic safety.**

4. State how the location, nature and height of buildings, walls and fences, and the nature and extent of the landscaping on the site shall be such that the use will not hinder or discourage the appropriate development and use of adjacent land and buildings, or will not impair the value thereof.

*“The relocated asphalt plant will provide a 10’ high chain link fence with slats to screen the view from Wheeling Road and we will keep the existing parkway trees and plant additional trees and landscaping to provide screening. We will have 12’ high concrete blocks on the east side of the property with solid evergreens for screen from the Metra train. The slender appearance of the proposed silos negate any negative visual impact on their construction.*

**The Plan Commission should make a determination regarding the style and height of the proposed fence. The 10-foot fence requires a height variation.**

5. State how the parking areas will be of adequate size for the particular use, properly located and suitably screened from adjacent residential uses, entrance and exit drives shall be laid out so as to prevent traffic hazards and nuisances and the development will not cause traffic congestion.

*“The relocated plant will be moved further away from residential uses and will have great screening from existing industrial buildings. The 10 parking spaces are adequate for the number of employees who will be on site.”*

**The parking provided meets the requirement of the Zoning Code. Furthermore, the petitioner indicates the provided parking is adequate for the number of employees at this facility.**

6. State how the property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulation in that zone.

*“If we do not receive special use permit to allow the operation of the asphalt we will not relocate the operation or upgrade the equipment and keep at our current location with outdated equipment.”*

**For the petitioner’s purposes, the only potential use of the site is the use proposed. If the use is not allowed in the subject location, the petitioner will not relocate to the subject property.**

### **STANDARDS FOR VARIATIONS**

Following are standards for variations with the petitioner's responses. **(Senior Planner comments are in bold.)**

1. State how the particular physical surroundings, shape, or topographical condition of the specific property involved would result in a practical difficulty or particular hardship upon or for the owner, lessee or occupant, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out. *(How do the unique characteristics make it difficult to follow the regulations of the code? You may want to consider Question 2 first.)*

*“The variation is based on the area needed to allow for the efficient use of the site and to allow for upgrading of the applicant’s existing facility. The triangular shape of the site forces an inefficiency in how the site is utilized and restricts the traffic circulation on the site. This requires the need to reduce the amount of landscaped area to allow for truck circulation and storage of the necessary materials. The height of the storage silos is needed so as to allow for sufficient storage of asphalt to meet the requirements mandated by road standards. Due to the shape of the site it would be difficult to site additional silos (which would be required in order to satisfy the volume requirements needed) without compromising site circulation.”*

**The variation related to fence height is necessary as a mitigation measure for the appearance of the plant itself. The variation related to the height of the plant accessory structures is necessary because the nature of the operation requires structures of that height. Due to the limited size and configuration of the property, there is not enough space for additional green space and the necessary functions of the proposed use.**

2. Indicate how the hardship is due to unique circumstances that do not generally apply to the other properties or uses. *(How is the property different from other properties with similar uses that do not require a variation?)*

*“The unique situation is that existing facility is the only asphalt plant in Wheeling. The site being proposed is the only site that has been found in Wheeling which can both accommodate the facility and is located in area where there will be no significant impacts on neighbors. The size of the site along with the needs for truck circulation prohibits the additional landscaped area. The height of the silos is driven by the paving industry’s and the Illinois Department of Transportation’s requirement for different asphalt mixtures. This requires the additional height and number of silos in order to the time the mixing plant operates the silos need to be able to store greater amounts of asphalt and different mixtures than the existing plant allows for. This need is particular to the paving industry and is not generally required for other industrial uses in the I-4 district.”*

**The variation related to the height of the plant is necessary because the nature of the operation requires structures of that height. The fence height will help mitigate the visual impacts of the facility operations.**

3. Describe how the alleged difficulty or hardship has not been created by any person presently having an interest in the property. *(Is the problem or hardship a result of actions taken by the applicant?)*

*“The Village has been proposing that the existing plant be both relocated and its operation improved. This site is the only site which has become available which will accommodate the facility. Its size and shape restricts the amount of landscaped area which can be established on site. The nature of the operations is not conducive to extensive planting areas within the site as they will not survive. The paving industry requires varied asphalt mixes requiring the number and size of the proposed silos.”*

**The variations not related to the existing conditions are related to the property configuration (minimum amount of green space), mitigating efforts (height of fence) and nature of the proposed use (height of plant).**

4. State how the property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations in that zone. *(What are the specific negative impacts on the future use of the property if the variation is not granted?)*

*“The site would not accommodate the needed activities (production of asphalt, material recycling, storage of materials and truck circulation) if additional landscaping was required to meet the required standard. A reduction in the height of the silos would require operating the plant for producing asphalt for longer hours. It will also not allow for the varied ‘mixes’ as the paving industry requires.”*

**For the petitioner’s purposes, the only potential use of the site is the use proposed. If the use and associated variations are not allowed in the subject location, the petitioner would not relocate to the subject property.**

5. State how the granting of the variation will not alter the essential character of the locality. (*Will the variation allow for construction or other use of the property that will change the appearance or character of the neighborhood or area?*)

*“The majority of the surrounding buildings do not meet the required landscape percentages. We are proposing additional landscaping along Wheeling Road which will screen the site. The silos are very slender structures so their height will not negatively impose on the surrounding area the way a much taller building would. The neighborhood already has silos which have not caused any detriment to the development nearby.”*

**Although there is no use in the immediate area that is completely similar to the proposed use, the proposed use is similar in some respects to several area uses, such as having storage yards and significant truck traffic. The lack of green space will not be visible to the neighboring properties due to the screening of the plant operations.**

6. Describe how the proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the neighborhood. (*Will the variation have a negative impact on other properties? Will it create traffic, noise, odor, light, or smoke? Will buildings be placed too close to other buildings? Will emergency access to the rear of the property still be possible?*)

*“The variations will not cause any negative impacts to the surrounding area. Landscaping would be internal to the site so that its benefits would not be shared by surrounding properties as the screening along Wheeling Road. The variations allow for the efficient use of the property for its intended uses in a manner which is self-contained. The slender design of the silos prevents their height from causing negative impacts (impairment of light and air, increase in the danger of fire, etc) due to their height.*

*The traffic study indicates that no traffic impact will be caused by establishment of the facility at this location. There has been no showing of any negative impacts from the facility at its existing location; the relocation to a larger property and its improvements will not cause any negative impacts but will guaranty that no impacts result.”*

**The proposed variations, in particular the height variation, are needed for the proposed use to be allowed anywhere within the Village. According to the traffic, environmental and acoustic studies provided, the variations do not appear to cause negative impacts on the vicinity.**

### **STAFF REVIEW**

**Fire Department Review:** The Fire Department has provided a comment memo, dated September 15, 2016. Many of the comments can be addressed at the time of building permit. However, Planning Staff believes that the comments number 2 through 6 should be addressed by the petitioner by the time the Plan Commission is ready to make a motion on Docket No. 2016-14. A written response and revised truck turning radius should be provided to these comments.

**Engineering Division Review:** The Engineering Division has received the submittal and will have comments prior to the meeting.

**Impact on adjacent uses:** Staff suggests that the Plan Commission request clarification regarding left-turns into the facility. The truck turning diagrams suggest that left-turns into the site are restricted. This restriction may be necessary to ensure traffic safety due to the proposed off-set drive at Harvester Court. The Plan Commission may wish to ask the petitioner to address the safety of traffic at the off-set drive near Harvester Court.

**Staff Recommended Action:** Staff recommends that the Plan Commission review the following list of items with the petitioner prior to taking action on Docket Nos. 2016-14ABCD:

- Request a response to the Fire Department memo comments #2 through #6;
- Consider requesting revised truck-turning exhibits consistent with the proposed site plan;
- Request clarification regarding left-turns into the site and determine if left-turns should be restricted due to safety concerns with the off-set drive near Harvester Court.
- Request brick and siding material and color samples for the buildings; and
- Discuss the proposed style of the fencing along the east, north, and south property lines.

### **PROPOSED MOTIONS**

If the Plan Commission finds that the petitioner has satisfied the requirements for the granting of a Zoning Code text amendment, rezoning, special use-site plan approval, and zoning variations for an asphalt plant, appropriate motions would be to:

**Recommend approval of Docket 2016-14A**, amending Title 19, Zoning Code, to read as follows:

1. Amend Section 19.01.010, Definitions, to insert the following:

**Concrete / Asphalt Plant**

A facility for the mixing and crushing and the storage and transporting of raw or recycled aggregate, along with concrete, and liquid asphalt to make Asphalt Cement Concrete (A.C.C.) or concrete for the purpose of sale.

2. Amend Appendix A: Use Table, as follows:

...

***Industrial Districts – Permitted and Special Uses (cont).***

P=Permitted Use, S=Special Use Permit, Blank=Not Allowed				
	<b>I-1 Light Industrial and Office District</b>	<b>I-2 Limited Industrial District</b>	<b>I-3 General Industrial District</b>	<b>I-4 Heavy Industrial District</b>
<b>Industrial Uses</b>				
...				
<i>Asphalt/Concrete Plant</i>				S

...

**And Recommend approval of Docket No. 2016-14B**, granting a Rezoning from I-2 Limited Industrial District to I-4 Heavy Industrial District, as illustrated on the staff exhibit “Existing vs. Proposed Zoning” dated September 15, 2016, 571-581 Wheeling Road, Wheeling, Illinois.

**And Recommend approval of Docket No. 2016-3C** Special Use and associated Site Plan Approval as required under Chapter 19-07 Industrial Districts, Section 19.07.050 I-4 Heavy Industrial District and Chapter 19-10 Use Regulations, Section 19.10.030 Special Uses, to permit an asphalt/concrete plant at 571-581 Wheeling Road, Illinois, in accordance with the following plans and documents submitted August 11, 2016 (except as noted):

- Response Letter
- Project Description Letter
- Site Plan
- Landscape Plan
- Photometric Plan (9.01.2016)
- Elevation Plan (Asphalt Plant) (8.31.2016)
- Lab and Control Building Elevation Plans (8.31.2016)
- Lab and Control Building Floor Plan (9.01.2016)
- Office Building Floor Plan (9.01.2016)
- Preliminary Engineering Plans
- Perspective View
- Plats of survey
- Traffic Study
- Environmental Study
- Acoustical Study.

And with the following conditions:

1. That a total of eight silos are permitted in the location shown on the site plan,
2. That prior to permit approval, a plat of vacation shall be executed for the Village right-of-way (Commercial Drive), and

3. Others to be determined at the hearing...

**And Recommend approval of Docket No. 2016-3D** granting zoning variations from Title 19, Zoning, of the Wheeling Municipal Code:

Chapter 19-07, Industrial Districts, Section 19.07.050, I-4 Heavy Industrial District

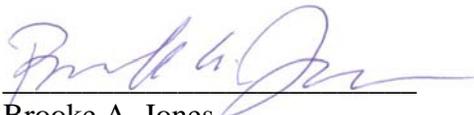
1. Subsection D3, reducing the minimum required green space from 25% to 13%.
2. Subsection F, increasing the maximum permitted height of an accessory structure from 20-feet to 74-feet.

Chapter 19-10, Use Regulations, Section 19.10.070, Accessory Uses, Buildings and Structures, Section B Fences and Screen, Subsection 7c Required fences and screening for outdoor storage, increasing the maximum permitted height from 8-feet to 12-feet.

As shown on the following plans:

- Site Plan, and
- Elevation Plan (Asphalt Plant).

  
\_\_\_\_\_  
Andrew C. Jennings, AICP  
Director of Community Development

  
\_\_\_\_\_  
Brooke A. Jones  
Senior Planner

**Attachments:**      [Aerial photo of existing conditions \(staff\)](#)  
  
[Fire Department memo, dated 9.15.2016](#)  
  
[Response Letter](#)  
  
[Project Description Letter](#)  
  
[Neighborhood Meeting Minutes from May 31, 2016](#)  
  
[Site Plan](#)  
  
[Landscape Plan](#)  
  
[Photometric Plan](#)  
  
[Elevation Plans \(Asphalt Plant\)](#)

[Lab and Control Building Elevation Plans](#)

[Lab and Control Building Floor Plan](#)

[Office Building Floor Plan](#)

[Preliminary Engineering Plans](#)

[Perspective View](#)

[Plats of survey](#)

[Rezoning staff exhibit](#)

[Traffic Study](#)

[Environmental Study](#)

[Acoustical Study](#)

# Builders Asphalt – 571-581 Wheeling Road

Docket Nos. 2016-14 A,B,C&D (Minor Site Plan and Appearance Approval of Park Modifications)  
Plan Commission Meeting – September 22, 2015



Existing conditions of subject property – aerial view



## MEMO – Fire Prevention Bureau

**TO:** Brooke Jones, Associate Planner

**FROM:** Ronald S. Antor, Fire Inspector

**CC:** Andrew Jennings, Village Planner  
Andrew Jennings, Director of Community Development  
Keith Maclsaac, Fire Chief  
FPB File

**DATE:** September 15, 2016

**SUBJECT:** Proposed asphalt plant – Orange Crush – 571 Wheeling Road – Plans received for review by the Wheeling Fire Department, September 1, 2016.

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The Wheeling Fire Department has reviewed the submittals received related to the above referenced project and has the following comments:

### Site Plan

1. A looped water main shall be provided on the site. The site water main shall comply with the Village of Wheeling Municipal code which includes, but is not limited to the “Manual of Practice for the Design of Public & Private Improvements” and the 2012 Edition of the International Fire Code. Water main improvements required would include:
  - a. An 8” looped water main shall be provided with hydrants not exceeding 300 foot spacing between the hydrants along the fire apparatus access route.
  - b. A fire hydrant shall be provided within 50’ of each building’s Fire Department Connection (FDC).
  - c. A separate water service is required for the fire protection service supply and the domestic service supply for each building. The fire service supply main size shall be based on the building’s needed fire flow and shall not be less than 6” in diameter. The domestic service shall be sized to meet village requirements as approved by the Department of Community Development, Engineering Division.
2. Overhead obstructions along the fire apparatus access route shall not hang over the roadway in a manner that hinders Fire Department access. A minimum of 13’6” overhead clearance shall be provided over the fire apparatus access roads.
3. A clear space of at least 4-feet shall be maintained around the circumference of any fire hydrants. This includes light fixtures, transformers, and landscaping.
4. Other site landscaping features shall not impede access to fire protection equipment, i.e. Fire Department Connection (FDC).
5. All fire access drives shall be a minimum of 20’.

Ms. Brooke Jones

SUBJECT: Proposed asphalt plant – Orange Crush – 571 Wheeling Road – Plans received for review by the Wheeling Fire Department, September 1, 2016.

September 15, 2016

Page 2

6. The roadway over the underground detention vault shall be capable of supporting fire apparatus with a gross vehicle weight of 86,000 pounds
7. Fire Department Knox Boxes shall be provided at the locations of any fencing with gates that restrict access to the site.

**571 Wheeling Road – Two new structures**

1. The petitioner is proposing to include two new structures with the new asphalt plant. Based on the information provided with this submittal, the structures would be classified as (B) Business Use Group occupancies as defined in the 2012 Edition of the International Building (IBC) and Fire Prevention Codes (IFC).
2. All construction relating to the structures would need to comply with the Village's Building and Fire Prevention Codes (2012 Editions of the International Building Code & International Fire Code – with amendments).
3. As noted in Comment #2, the proposed buildings will need to comply with the Village's Building and Fire Prevention Codes. Some of the items that this would include and would need to be addressed during the permitting process are:
  - a. Each building is required to be provided with an automatic fire sprinkler system.
    - i. The Fire Department Connection (FDC) for each building's fire sprinkler systems shall be located at the front of the building or other location as approved by the Fire Department.
  - b. Each building is required to be provided with a fire alarm system.
  - c. A second means of egress (exit) is required for elevated control room per the Village's Building Code.

At this time there are no other Fire Department comments related to the project as presented in the documents reviewed. The Fire Department recommends that no approvals be given for this project until all of the above comments have been addressed by the developer.

# Pal Group, Inc.

321 Center Street, Hillside, IL 60162

Tel: (708) 544-9440 Email: twinter@orange crushllc.com

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## Via: Electronic Mail

August 11, 2016

Andrew C. Jennings, AICP  
Community Development Director  
Village of Wheeling  
2 Community Boulevard  
Wheeling, IL 60090

RE: Proposed Asphalt Plant Equipment Upgrade and Relocation  
571 & 581 S. Wheeling Road  
Wheeling Road

Dear Mr. Jennings,

We are submitting the following items for you review on the Proposed Asphalt Plant Equipment Upgrade and Relocation to have a plan commission and public hearing meeting on September 8, 2016.

- Site Plan prepared by Rolf C. Campbell & Associates dated August 11, 2016
- Landscape Plan prepared by Rolf C. Campbell & Associates dated August 11, 2016
- Traffic Study prepared by Sam Schwartz Engineering, D.P.C. dated August 10, 2016
- Acoustical Study prepared by Shiner & Associates, Inc. dated August 9, 2016
- Environmental Study prepared by Green Toxicology, LLC dated August 2, 2016
- Preliminary Engineering prepared by Mackie Consultants, LLC dated August 11, 2016
- Application A1 Cover Sheet & Contact Information
- Application A2 Plan Submittal
- Application A3 Special Use Standards
- Application 4 Zoning Variations Standards
- Application A5 Rezoning and Zoning Code Text Amendments
- Application PH Actions Requiring Public Hearing
- Meeting minutes from Neighborhood meeting on May 31, 2016
- Project description letter dated August 10, 2016
- Perspective View of Asphalt Plant from Northgate Parkway prepared by Rolf C. Campbell & Associates dated August 9, 2016
- ALTA / ACSM Land Title Survey prepared by Chicago Land Survey Company for 571 S. Wheeling Road dated December 1, 2015.
- ALTA / ACSM Land Title Survey prepared by Land Divisions, Inc. for 581 S. Wheeling Road dated June 17, 2016.

At the Plan Commission Workshop Session on June 9, 2016 we were provided 12 recommendations, below is a response to those recommendations.

Exhibit received August 11, 2016

# Pal Group, Inc.

321 Center Street, Hillside, IL 60162

Tel: (708) 544-9440 Email: twinter@orange crushllc.com

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- 1) **The trees along Wheeling Road should be greater than 3" caliper to provide greater screening immediately.**  
As shown on the Landscape all tree installed along Wheeling Road are specified to be greater than 3" caliper.
- 2) **Provide neighborhood meeting minutes.**  
As part of this submittal we are providing the minutes from the neighborhood meeting on May 31, 2016.
- 3) **Provide screening along the east side.**  
The east side of the property has a minimum of 6' greenspace which will be planted with Dark Green Arborvitae.
- 4) **Provide truck stacking plan**  
The traffic study shows the truck stacking plan.
- 5) **Consider closing one access drive to achieve more greenspace.**  
As shown on the Site Plan we have eliminated two of the existing driveway off Wheeling Road.
- 6) **Address air quality issues, storm water quality and noise issues.**  
We have submitted a report from Green Toxicology, LLC to address the air quality and Shiner Associates, Inc. to address the noise issue.
- 7) **Address the silo height and placement**  
The size of the silos has been reduced from 300 ton to 250 ton, this lowered the height to 74' which is only a couple feet taller than existing asphalt plant silo height.
- 8) **Address access to the ComEd ROW**  
We have decided not to pursue the ComEd property
- 9) **Property beautification / screening is encouraged.**  
The landscape plan which shows substantial landscaping and screening around the entire property.
- 10) **Clarify timing of operations.**  
The hours of operation will be from 6:00 A.M. to 6:00 P.M. Monday through Saturday. The plant typically is open from April to December depending on the weather. This is the same operation hours at the existing plant.
- 11) **List fence height variation on list of requested variations.**

# Pal Group, Inc.

321 Center Street, Hillside, IL 60162

Tel: (708) 544-9440 Email: [twinter@orange crushllc.com](mailto:twinter@orange crushllc.com)

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Along Wheeling Road, we will have a 10' chain link fence with gray slats, on the south side will be a 6' fence, along the east side will be concrete block 12' high and north side of the property will be a 8' fence.

## 12) Address height of the piles

The aggregate piles will not exceed 12' in height.

We would like to set up a meeting at your earliest convenience to review the submittal and further discuss the text amendment and variances.

Please confirm we will be on the September 8, 2016 plan commission meeting for we can send out notification by August 24<sup>th</sup>. If there is any additional information required or any questions, please do not hesitate to give me a call.

Sincerely,  
**Pal Group, Inc.**



Tim Winter

# Pal Group, Inc.

321 Center Street, Hillside, IL 60162

Tel: (708) 544-9440 Email: [twinter@orangecrushllc.com](mailto:twinter@orangecrushllc.com)

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**Via: Electronic Mail**

August 10, 2016

Village of Wheeling  
Community Development Department  
Attn: Andrew Jennings, AICP  
2 Community Boulevard  
Wheeling, IL 60090

RE: Project Description Letter  
571 & 581 S. Wheeling Road  
Wheeling, IL

Dear Mr. Jennings,

Orange Crush, LLC currently operates the asphalt plant located at 231 S. Wheeling Road and has operated the plant since the Early 1990's. The plant has been in operation at this location since 1960's. For the past few years we have been in discussion with the Village of Wheeling to relocate our operations to help facilitate the Village with the redevelopment of the property around the Metra station.

Currently we own the property located at 571 S. Wheeling Road which is roughly 650' south of the current facility. Also we have an executed purchase and sales agreement with the owners at 581 S. Wheeling Road, which is directly to the south of 571 S. Wheeling Road, to purchase their property. Directly north of 571 S. Wheeling Road is village R.O.W. which is not being used for roadway and which we have discussed with the Village staff the possibility of acquiring the south half of the Village R.O.W.

This assemblage of property would double our size of the existing facility and would allow for a more efficient layout. It does not mean that the intensity of the facility will double. The additional land will help the facility be more energy, environmentally, and traffic efficient.

The current asphalt plant is 50 years old and does not meet the current demands or the requirements of Illinois Department of Transportation (IDOT). With the proposed relocation we would upgrade the equipment to meet current technology along with IDOT's current specification for asphalt production.

The current asphalt plant can only store two different asphalt products at any one time. If a different 'mix' of asphalt is required it has be made at time of pick up. The current production rate is limited to 200 tons per hour. The upgraded equipment would increase the production rate to 400 ton per hour which would reduce the number of hours the plant would have to operate.

# Pal Group, Inc.

321 Center Street, Hillside, IL 60162

Tel: (708) 544-9440 Email: twinter@orange crushllc.com

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As part the equipment upgrade we would increase the quantity and size of the silos from 2 – 200 ton silos to 8 – 250 ton silos. We would also increase the height of the silos from 65' to 74'. With the more efficient equipment and additional silos we would be able to make the daily orders in less time and store more products during the day. This would allow us to further reduce the time we are operating batch plant.

To go along with additional asphalt products that IDOT specifics we will need to have additional aggregates on property to meet IDOT's specifications. The need for additional types fo roadway aggregate is a further reason for the need to increase the total site area.

The remaining additional land is required for a road material recycler. Per IDOT specifications recycled asphalt is required to be used in all asphalt production. At the current location we have broken asphalt delivered from construction sites to the Wheeling facility. The broken asphalt then has to be hauled to a facility with an asphalt recycler. After processing the material is then hauled back to the Wheeling facility to be incorporated into the new asphalt mixes. Allowing for the asphalt recycling facility to be located on the new enlarged site with having the recycler at the proposed facility we will eliminate the double truck traffic required for hauling material in and off the facility. The recycler will also recycle concrete from road material to aggregate for the road base, all recycle material will meet the requirements of the IDOT. Since the upgraded asphalt plant will meet IDOT's requirements we will have lab on site in order to test the material verifying it does meets IDOT's specification.

While this is considered relocation of our existing facility, in proposing this program we are upgrading the equipment we use, making the overall site more efficient, and reducing truck traffic on the surrounding road network. We will still be able to supply our customers in and around Wheeling.

Thank you for your consideration of the equipment upgrade and relocation of the asphalt plant at 231 S. Wheeling Road to 571 S. Wheeling Road.

Sincerely,  
**Pal Group, Inc.**



Tim Winter

---

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## MEETING MINUTES

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**SUBJECT:** PROPOSED ASPAHLT NEIGHBORHOOD MEETING

**DATE:** MAY 31, 2016

---

On May 31, 2016 Pal Group, Inc. and Builders Asphalt, LLC held a neighborhood open house at the Wheeling Park District Community Recreation Center for future neighbors understand our equipment upgrade and relocation.

On May 24, 2016 a memo was sent to all property owners on the east and west side of Wheeling Road including cul-de-sac's north of Hintz Road and south of our existing plant informing them of this meeting, two business showed up.

Below are the names of the businesses and their concerns

**Public Electric Construction Company**  
**415 Harvester Court**  
**Wheeling, IL 60090**

- 1) Concerned on how the relocation of the asphalt plant will affect his property value.
- 2) Has an employee who said they will quit if the asphalt plant is relocated because of the contamination.
- 3) He questioned how the upgraded equipment would reduce the truck traffic
- 4) He asked how the road material recycler operates.

**Jelco, Inc.**  
**450 Wheeling Road**  
**Wheeling, IL 60090**

- 1) Had a concern the relocated plant would increase the truck traffic across their building
- 2) Had a concern with the noise that will be added due to the relocated plant.
- 3) Had concern about dust the asphalt plant will create.
- 4) They asked about the screening along Wheeling Road, will the existing parkway trees remain.

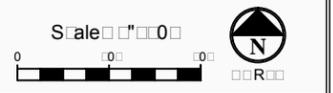
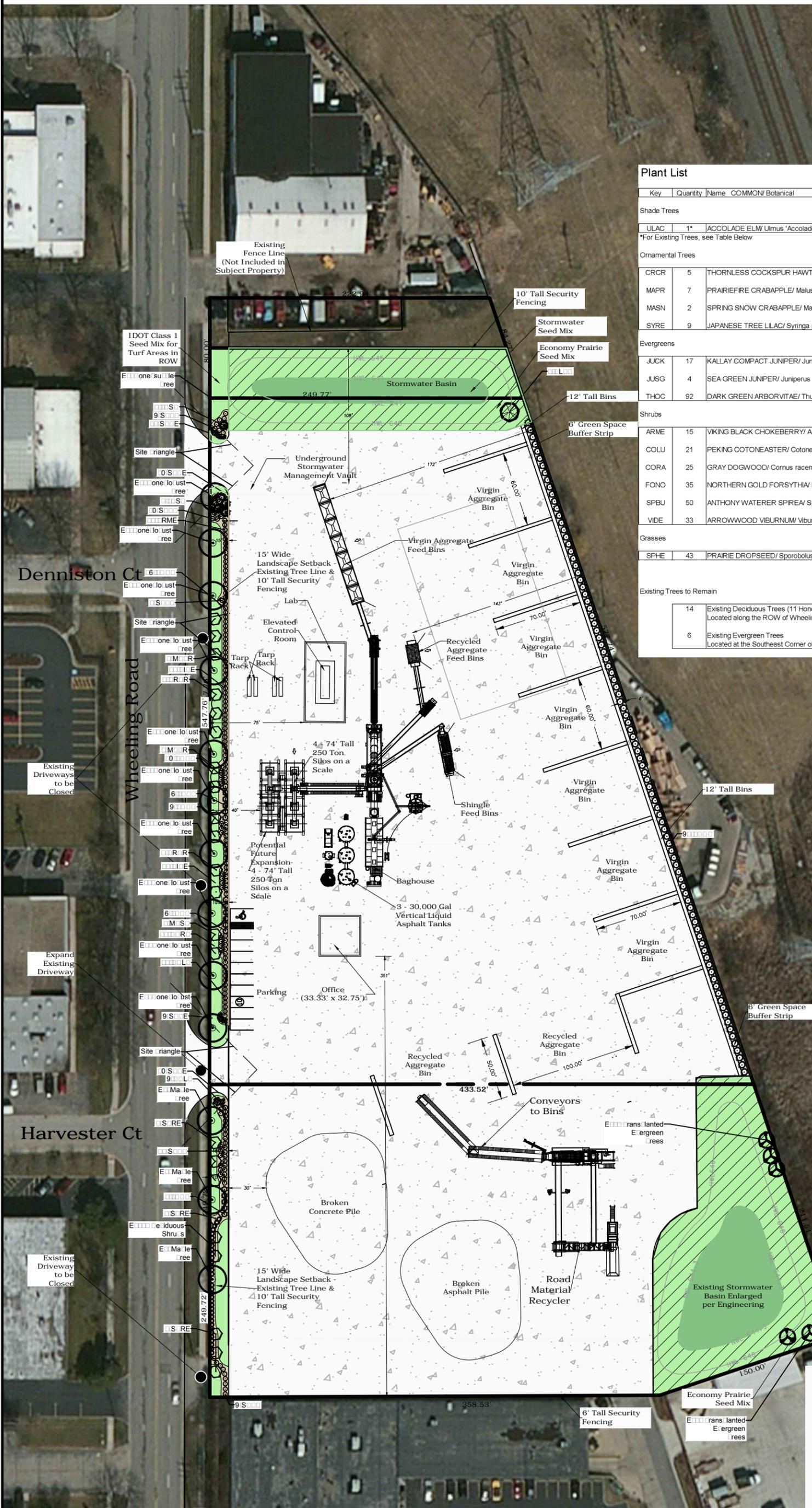
Exhibit received August 11, 2016





Plant List

Key	Quantity	Name COMMON/Botanical	Size	Comments
<b>Shade Trees</b>				
ULAC	1*	ACCOLADE ELM/ Ulmus 'Acolade'	3" BB	
*For Existing Trees, see Table Below				
<b>Ornamental Trees</b>				
CRCR	5	THORNLESS COCKSPUR HAWTHORN/ Crataegus crus-galli	2"-3" BB	2"-3" Mix Subject to Availability
MAPR	7	PRAIRIEFIRE CRABAPPLE/ Malus 'Prairiefire'	2"-3" BB	2"-3" Mix Subject to Availability
MASN	2	SPRING SNOW CRABAPPLE/ Malus 'Spring Snow'	2"-3" BB	2"-3" Mix Subject to Availability
SYRE	9	JAPANESE TREE LILAC/ Syringa reticulata	2"-3" BB	2"-3" Mix Subject to Availability
<b>Evergreens</b>				
JUCK	17	KALLAY COMPACT JUNIPER/ Juniperus chinensis 'Kallay'	24"	Planted 5'-0" on center
JUSG	4	SEA GREEN JUNIPER/ Juniperus chinensis 'Sea Green'	36"	Planted 5'-0" on center
THOC	92	DARK GREEN ARBORVITAE/ Thuja occidentalis 'Nigra'	6"	Planted 6'-0" on center
<b>Shrubs</b>				
ARME	15	VIKING BLACK CHOKEBERRY/ Aronia melanocarpa 'Viking'	3"	Planted 4'-0" on center
COLU	21	PEKING COTONEASTER/ Cotoneaster acutifolia 'Peking'	3"	Planted 4'-0" on center
CORA	25	GRAY DOGWOOD/ Cornus racemosa	3"	Planted 4'-0" on center
FONO	35	NORTHERN GOLD FORSYTHIA/ Forsythia 'Northern Gold'	3"	Planted 4'-0" on center
SPBU	50	ANTHONY WATERER SPREA/ Spiraea x bumalda	24"	Planted 4'-0" on center
VIDE	33	ARROWWOOD VIBURNUM/ Viburnum dentatum	3"	Planted 4'-0" on center
<b>Grasses</b>				
SPHE	43	PRAIRIE DROPSEED/ Sporobolus heterolepis	#1 Container	Planted 2'-0" on center
<b>Existing Trees to Remain</b>				
14	Existing Deciduous Trees (11 Honeylocust and 3 Maple) Located along the ROW of Wheeling Road			
6	Existing Evergreen Trees Located at the Southeast Corner of the Site			



**Landscaping Plan**

Proposed Asphalt Plant  
 Wheeling Road  
 Wheeling, IL

Local Route, In  
 Center Street  
 Hillside, IL 60161

9:0 Woodlands  
 Normal Hills, Illinois 60061  
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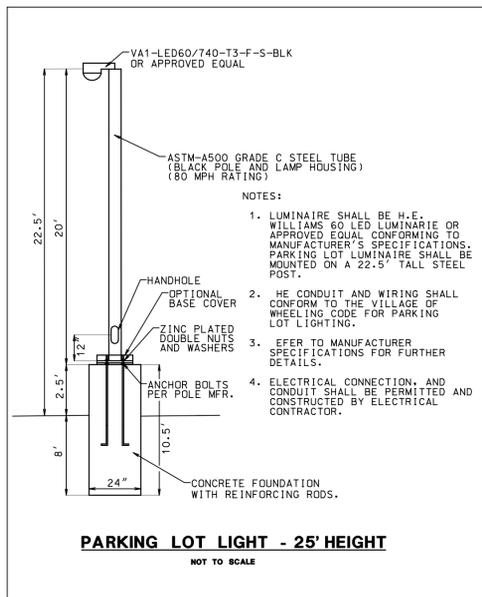
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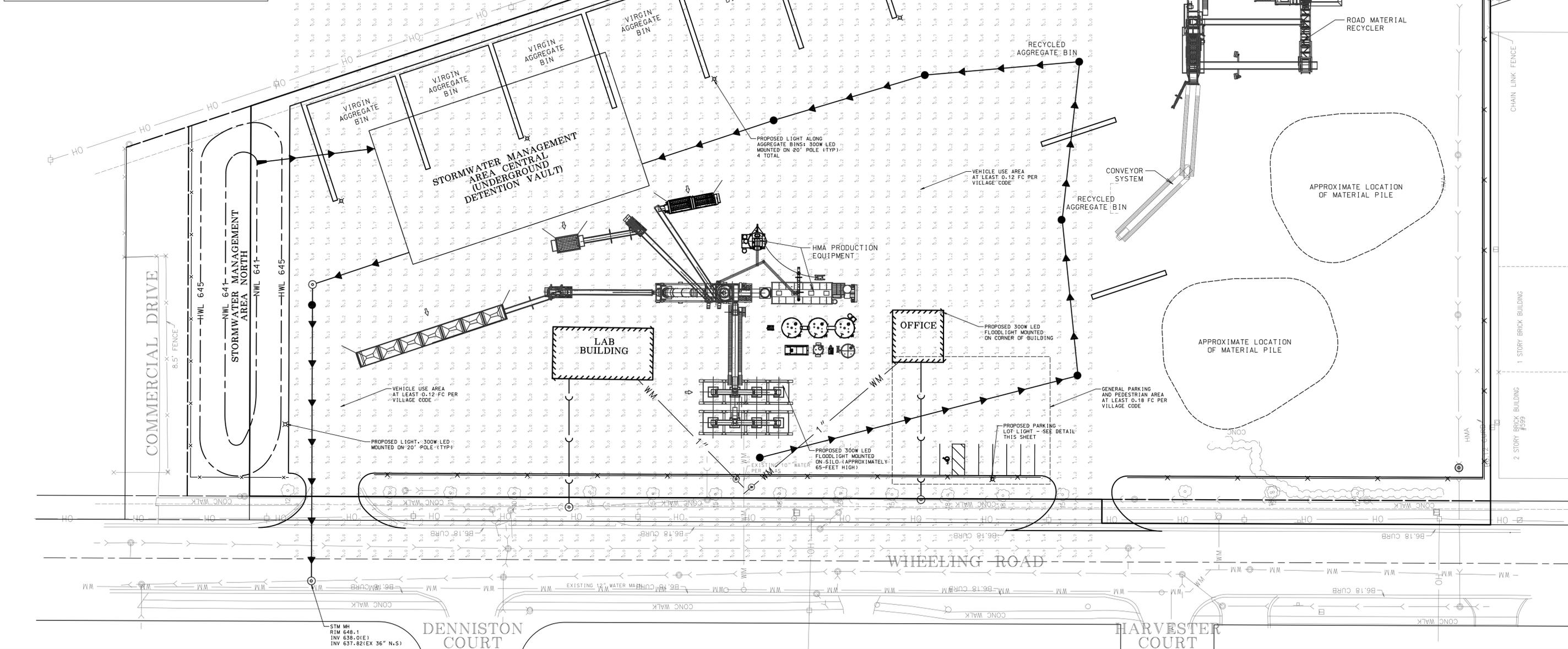
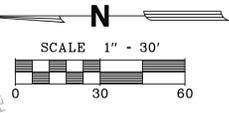
Chain link fence Example with slats



**PROPOSED AND REQUIRED ILLUMINATION DATA**

<b>GENERAL PARKING &amp; PEDESTRIAN AREA</b>
AVERAGE: 1.8 FC (0.9 REQ)
MINIMUM: 0.5 FC (0.18 REQ)
UNIFORMITY: 3:1 (4:1 SPECIFIED)
<b>VEHICLE USE AREA</b>
AVERAGE: 0.77 FC (0.45 REQ)
MINIMUM: 0.15 FC (0.12 REQ)
UNIFORMITY: 5:1 (4:1 SPECIFIED)

- NOTES:**
- LUMINAIRE SHALL BE H.E. WILLIAMS 60 LED LUMINAIRE OR APPROVED EQUAL CONFORMING TO MANUFACTURER'S SPECIFICATIONS. PARKING LOT LUMINAIRE SHALL BE MOUNTED ON A 22.5' TALL STEEL POST.
  - THE CONDUIT AND WIRING SHALL CONFORM TO THE VILLAGE OF WHEELING CODE FOR PARKING LOT LIGHTING.
  - REFER TO MANUFACTURER SPECIFICATIONS FOR FURTHER DETAILS.
  - ELECTRICAL CONNECTION, AND CONDUIT SHALL BE PERMITTED AND CONSTRUCTED BY ELECTRICAL CONTRACTOR.



STM MH  
RIM 648.1  
INV 638.01E  
INV 637.821EX 36" N.S.I

**571 WHEELING, LLC**  
321 CENTER STREET  
HILLSIDE, IL 60162  
Exhibit received September 1, 2016

DESIGNED	TKB	DATE	8-16-16
DRAWN	DWP	SCALE	1" = 30'
APPROVED	TKB		
DATE	8-16-16		
SCALE	1" = 30'		

**UTILITY PLAN  
PROPOSED ASPHALT PLANT  
WHEELING, ILLINOIS**

SHEET  
**3 OF 3**  
PROJECT NUMBER: 2820  
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ILLINOIS FIRM LICENSE 184-002694

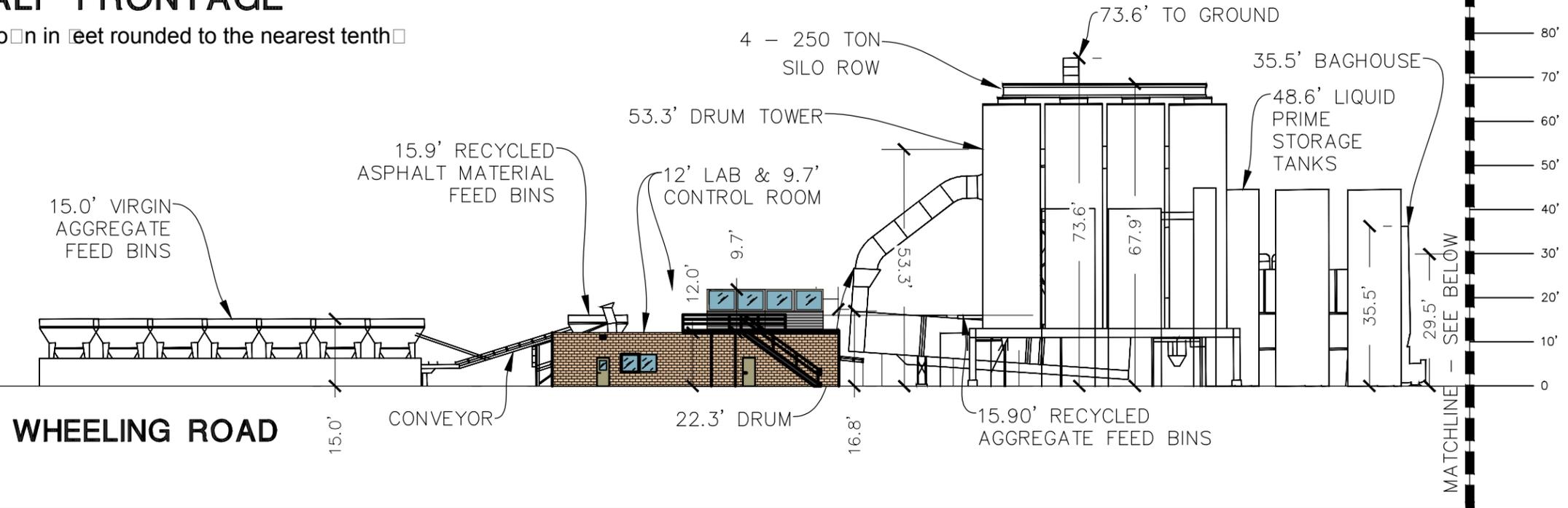
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**Mackie Consultants, LLC**  
9575 W. Higgins Road, Suite 500  
Rosemont, IL 60018  
(847)696-1400  
www.mackieconsult.com

CLIENT:

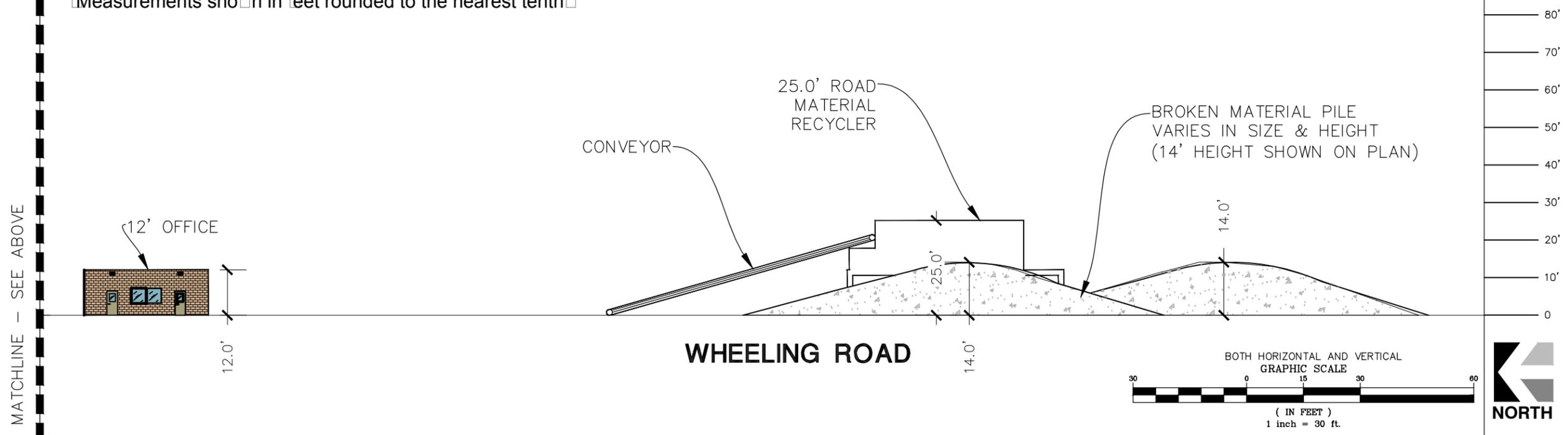
# NORTH HALF FRONTAGE

Measurements shown in feet rounded to the nearest tenth



# SOUTH HALF FRONTAGE

Measurements shown in feet rounded to the nearest tenth



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**ROLF C. CAMPBELL & ASSOCIATES**  
A MANHARD CONSULTING DIVISION

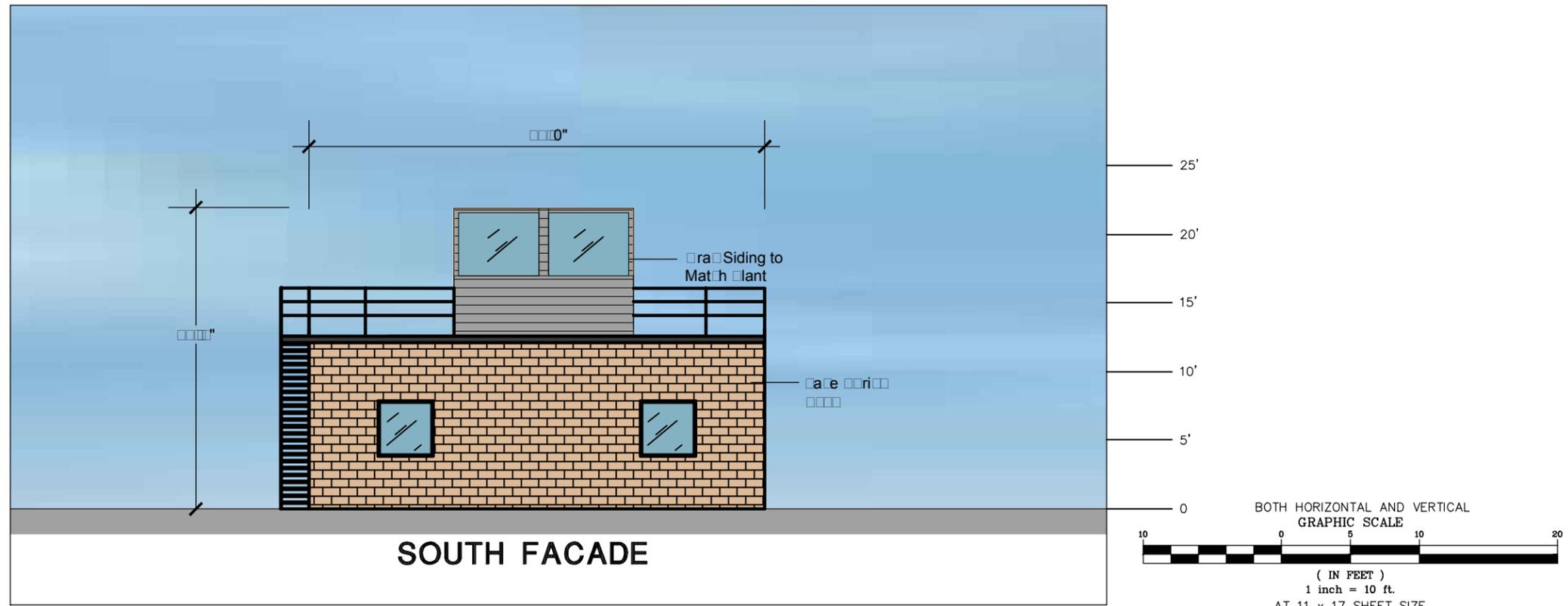
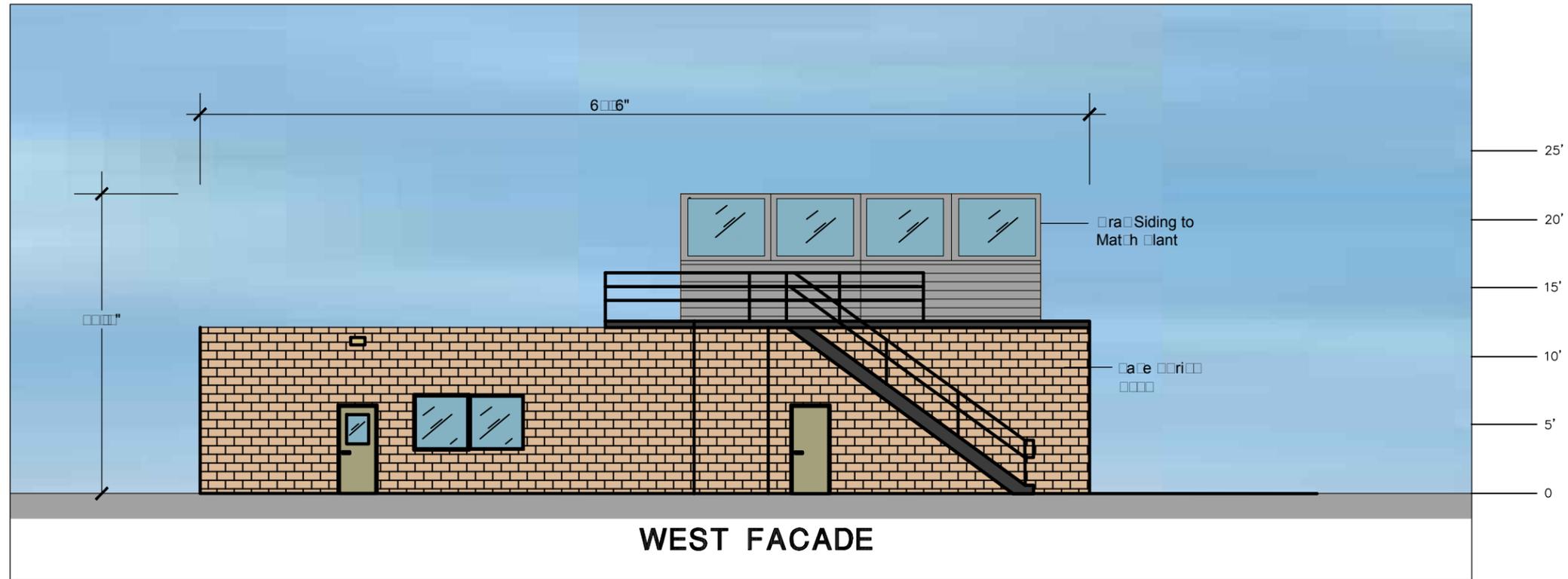
910 Woodlands Parkway Vernon Hills, Illinois 60061  
Ph: (847) 735-1000 Fax: (847) 735-1010 www.rccai.com

WHEELING ROAD ELEVATION (EASTWARD VIEW)

PROPOSED ASPHALT PLANT

PAL GROUP, INC.

DRAWN BY: BD	DATE: 8/31/16	SCALE: 1"=30'	CODE: PALWHIL01	PROJECT: LP
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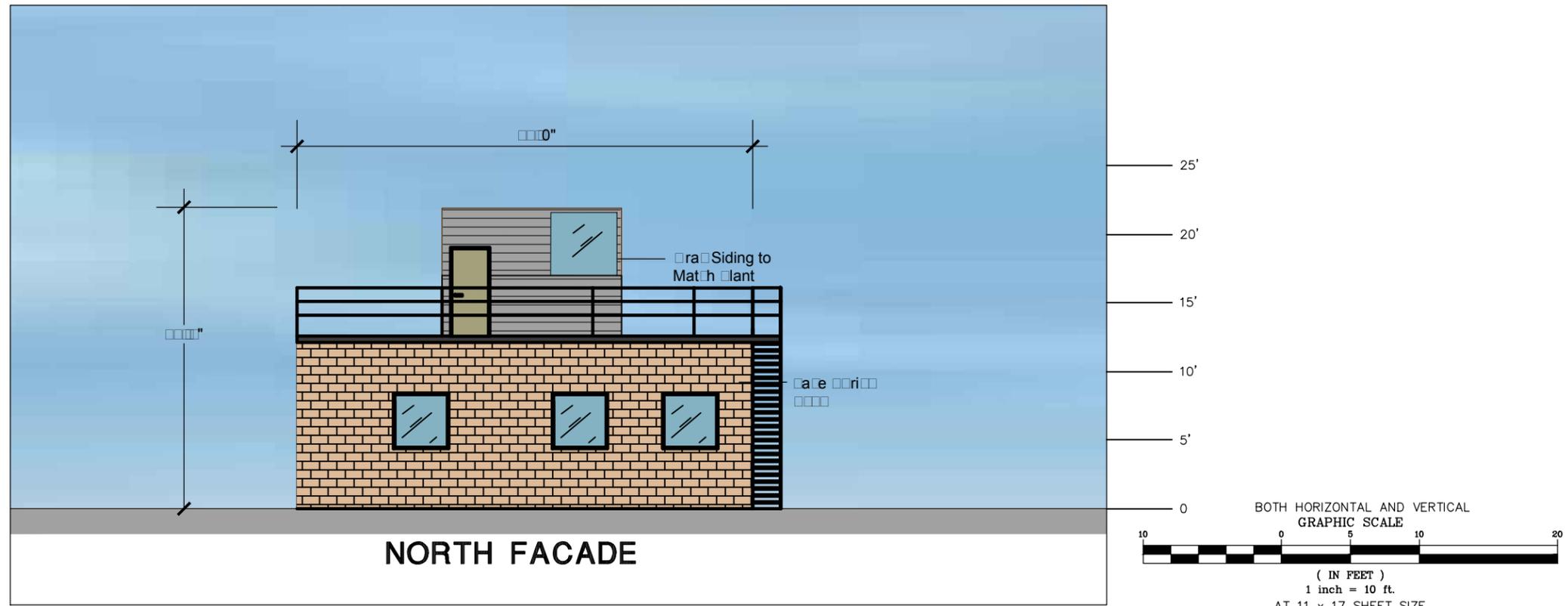
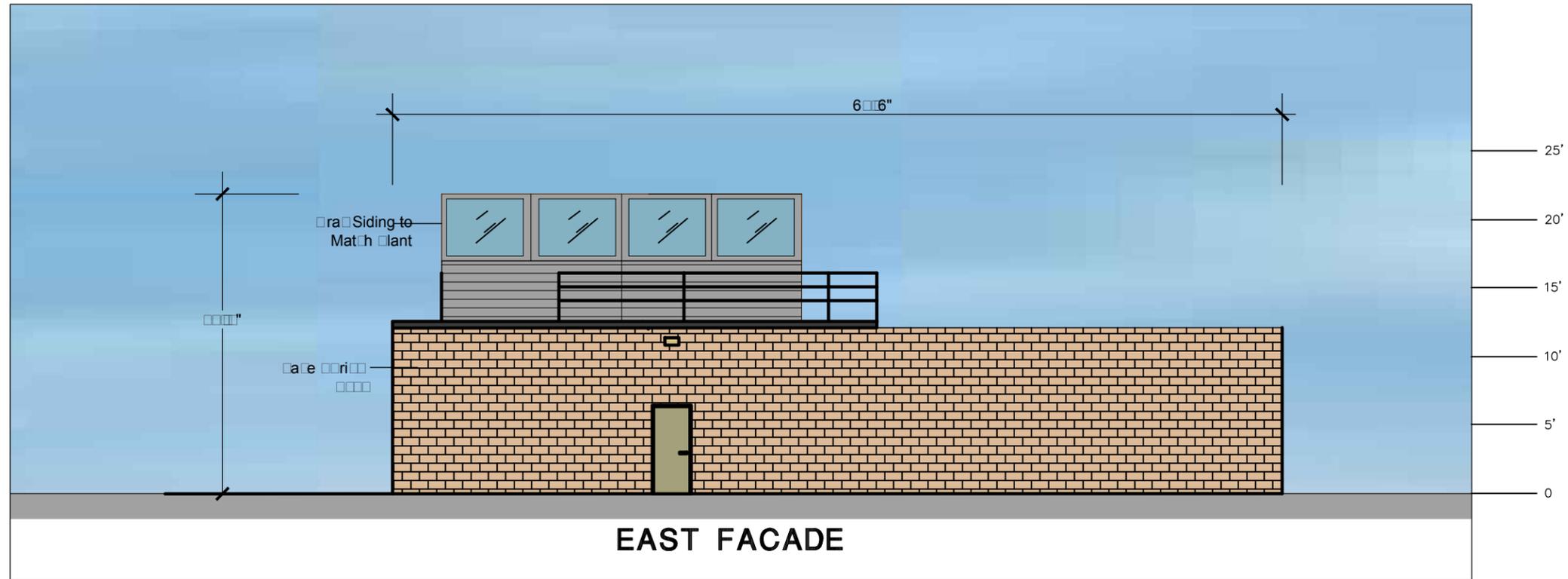
Exhibit received August 31, 2016

**ROLF C. CAMPBELL & ASSOCIATES**  
A MANHARD CONSULTING DIVISION

910 Woodlands Parkway Vernon Hills, Illinois 60061  
Ph: (847) 735-1000 Fax: (847) 735-1010 [www.rccai.com](http://www.rccai.com)

**LAB AND CONTROL BUILDING ELEVATIONS**  
**PROPOSED ASPHALT PLANT / WHEELING, ILLINOIS**  
**PAL GROUP, INC.**

<b>DRAWN BY:</b> BD	<b>DATE:</b> 8/30/16	<b>SCALE:</b> 1":10'	<b>CODE:</b> PALWHIL01	<b>PROJECT:</b> LP
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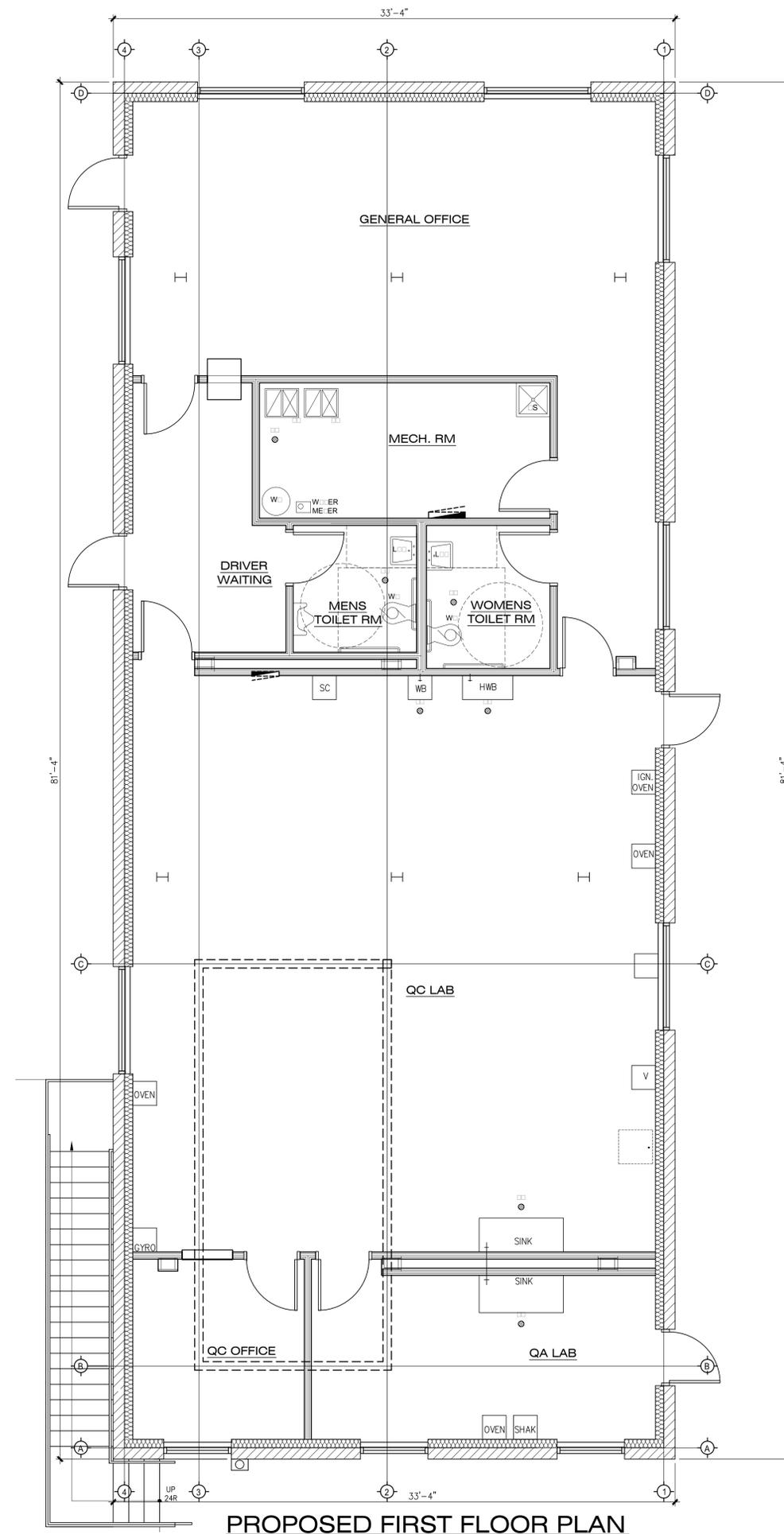
Exhibit received August 31, 2016

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A MANHARD CONSULTING DIVISION

910 Woodlands Parkway    Vernon Hills, Illinois 60061  
Ph: (847) 735-1000    Fax: (847) 735-1010    www.rccai.com

**LAB AND CONTROL BUILDING ELEVATIONS**  
PROPOSED ASPHALT PLANT / WHEELING, ILLINOIS  
PAL GROUP, INC.

<b>DRAWN BY:</b> BD	<b>DATE:</b> 8/30/16	<b>SCALE:</b> 1":10'	<b>CODE:</b> PALWHIL01	<b>PROJECT:</b> LP
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**PROPOSED FIRST FLOOR PLAN**

1/4" = 1'-0"

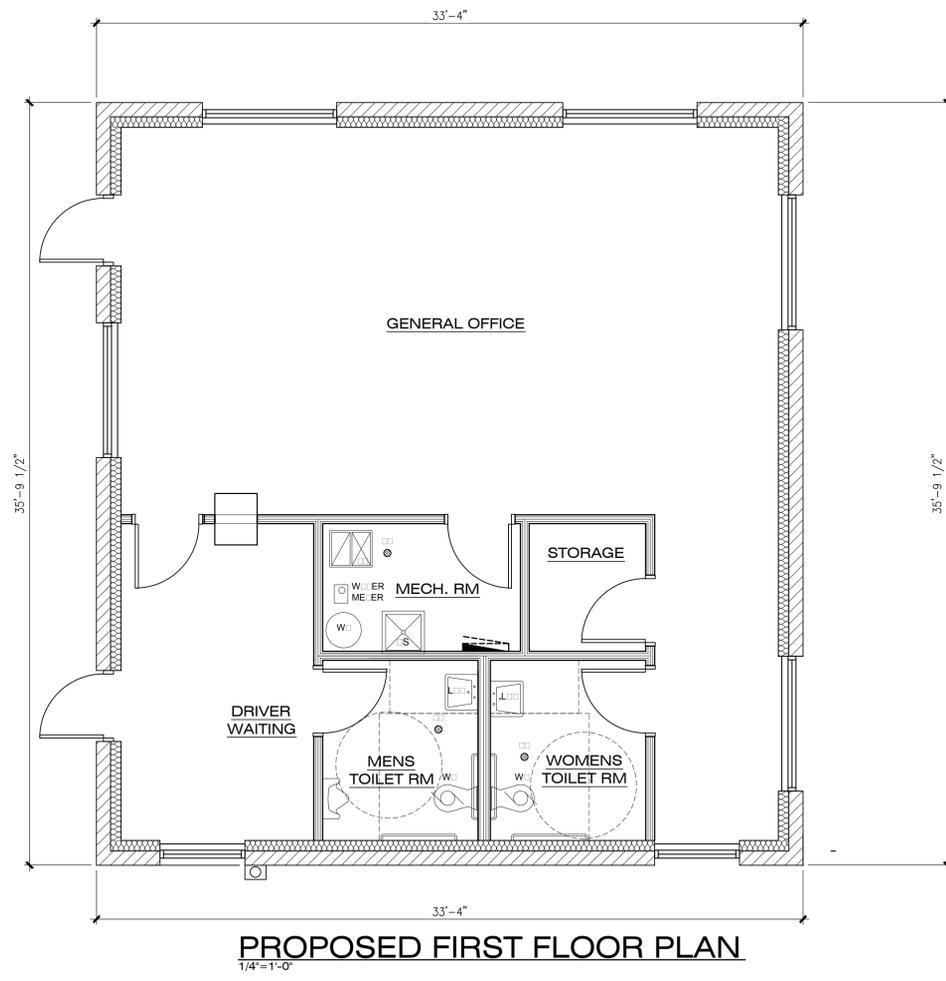
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job no.	
revisions	
scale	

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**PROPOSED OFFICE BUILDING**  
 581 S. WHEELING ROAD  
 WHEELING, ILLINOIS

**R-VARI AND ASSOCIATES**  
 708.354.4005 OFFICE  
 1420 Kensington Road Suite #06  
 CELL 312.735.0401  
 Oak Brook, IL 60523

Exhibit received September 1, 2016



date	9-1-16
drawn by	
job no.	
revisions	
scale	

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**PROPOSED OFFICE BUILDING**  
 581 S. WHEELING ROAD  
 WHEELING, ILLINOIS

**R-VARI AND ASSOCIATES**  
 708.354.4005 OFFICE  
 1420 Kensington Road Suite #06  
 Oak Brook, Il. 60523  
 CELL 312.735.0401

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**A1**

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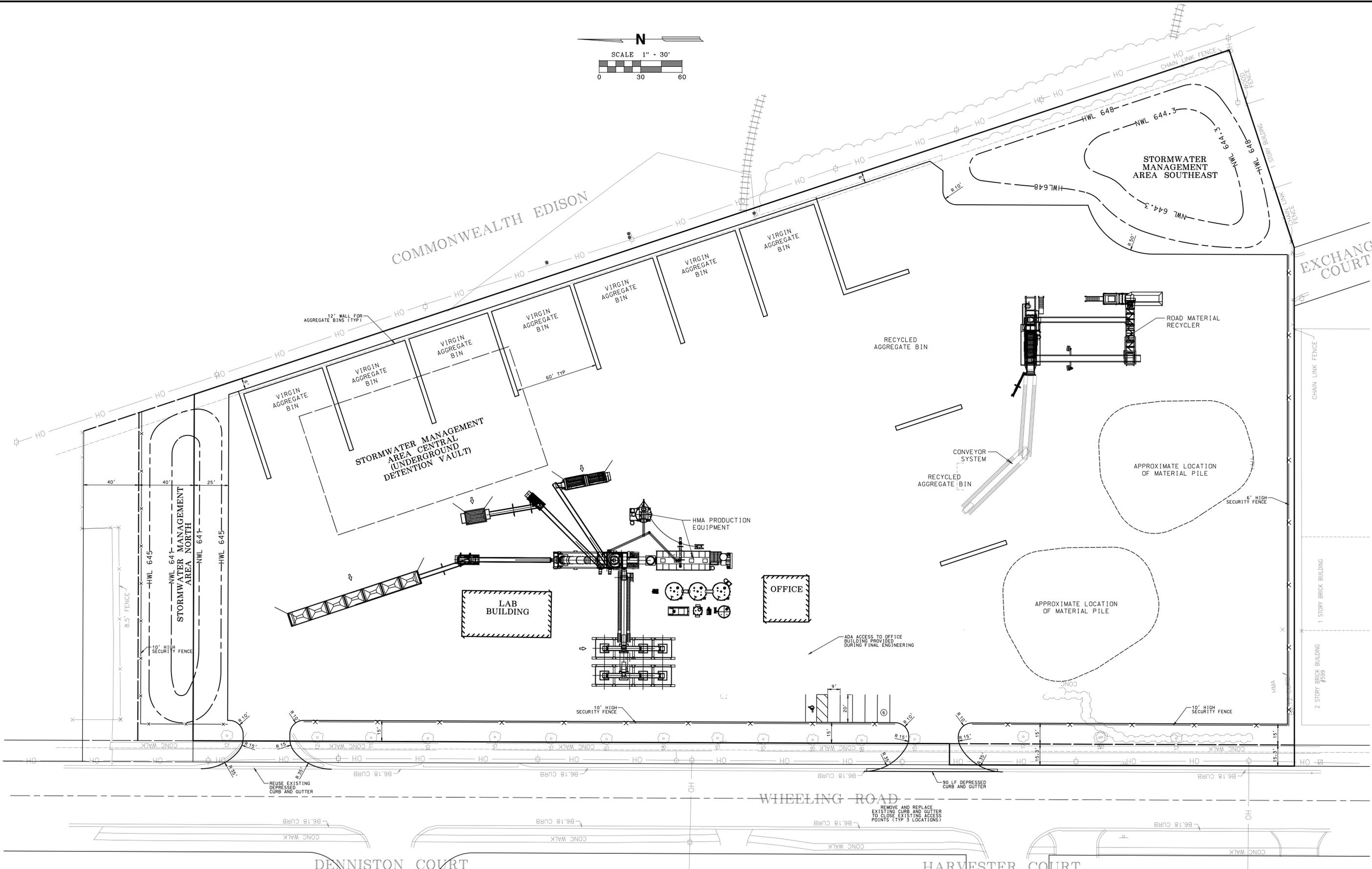
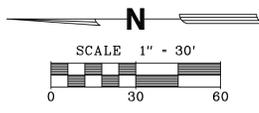


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**Mackie Consultants, LLC**  
 9575 W. Higgins Road, Suite 500  
 Rosemont, IL 60018  
 (847)696-1400  
 www.mackieconsult.com

CLIENT:  
**571 WHEELING, LLC**  
 321 CENTER STREET  
 HILLSIDE, IL 60162

DATE	DESCRIPTION OF REVISION	BY

DESIGNED	TKB
DRAWN	DWP
APPROVED	TKB
DATE	08/11/2016
SCALE	30

**OVERALL SITE PLAN**  
**PROPOSED ASPHALT PLANT**  
**WHEELING, ILLINOIS**

SHEET  
**1 OF 3**  
 PROJECT NUMBER: 2820  
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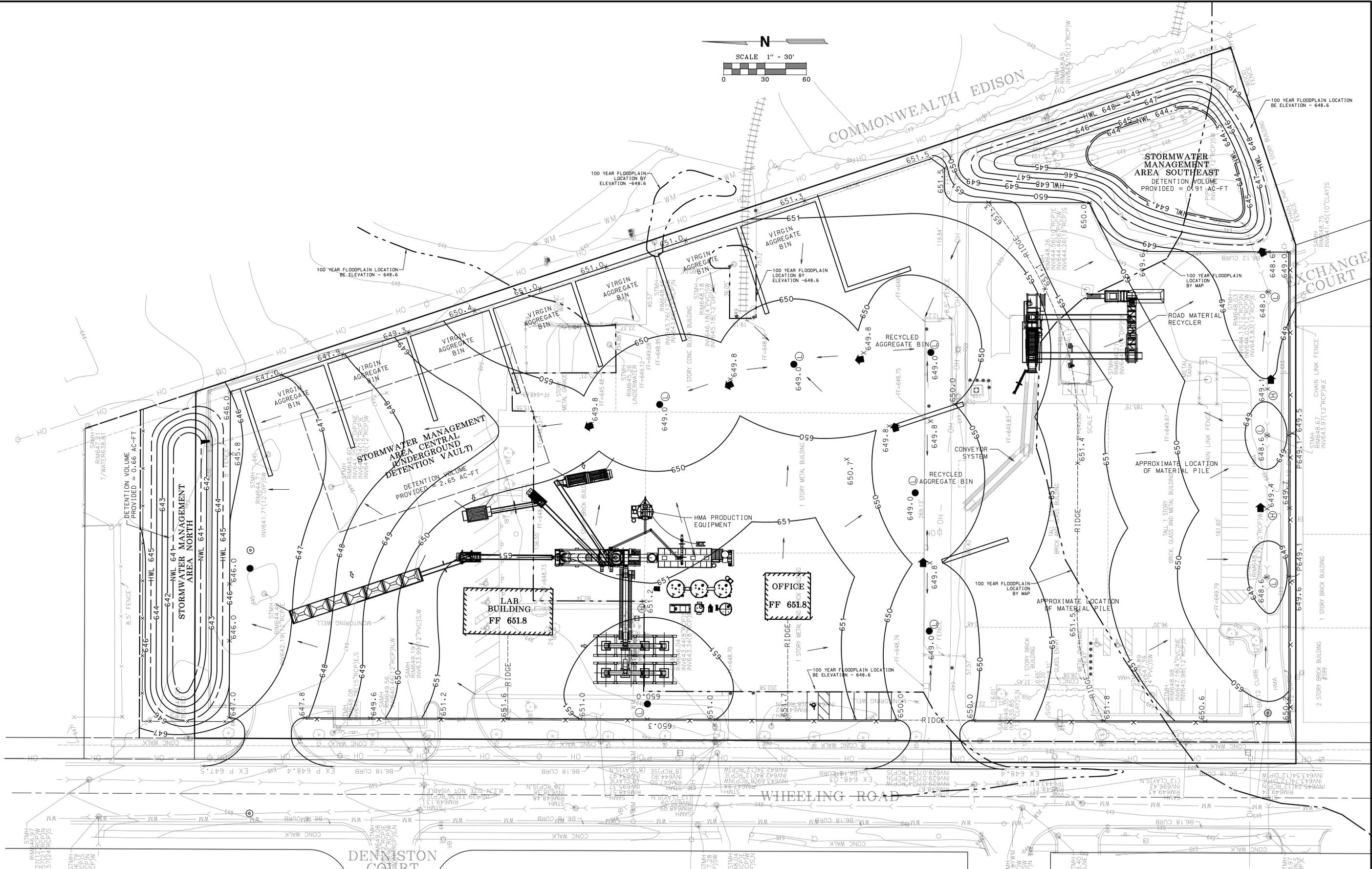
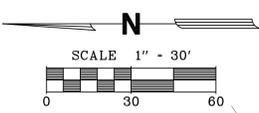


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DATE	DESCRIPTION OF REVISION	BY

DESIGNED	TKB
DRAWN	DWP
APPROVED	TKB
DATE	08/11/2016
SCALE	1" = 30'

**GRADING PLAN  
 PROPOSED ASPHALT PLANT  
 WHEELING, ILLINOIS**

SHEET  
**2 OF 3**

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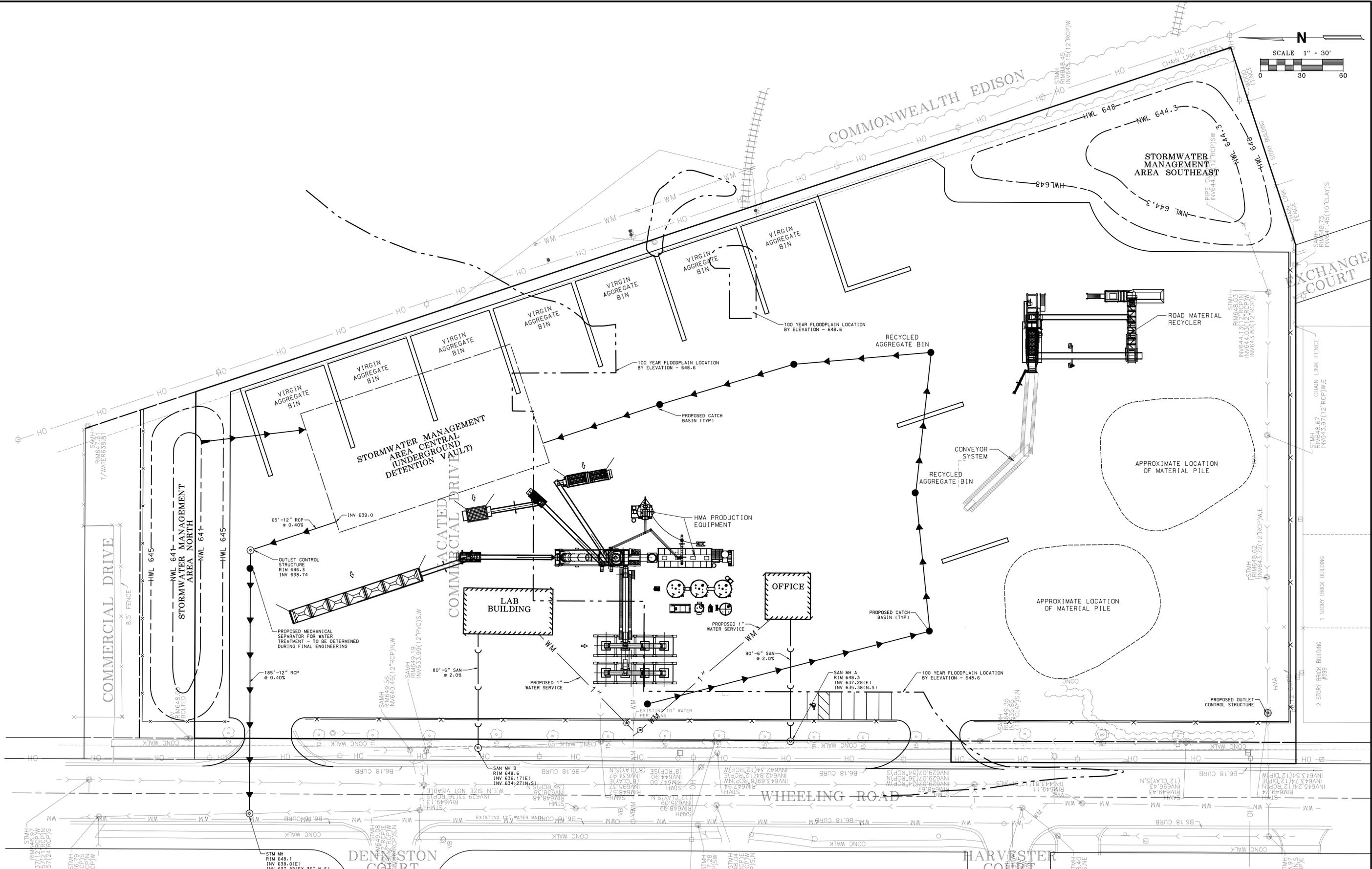
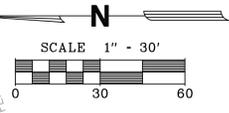


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 9575 W. Higgins Road, Suite 500  
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 (647)696-1400  
 www.mackieconsult.com

CLIENT:  
**571 WHEELING, LLC**  
 321 CENTER STREET  
 HILLSIDE, IL 60162

DATE	DESCRIPTION OF REVISION	BY

DESIGNED	TKB
DRAWN	DWP
APPROVED	TKB
DATE	08/11/2016
SCALE	1" = 30'

**UTILITY PLAN  
 PROPOSED ASPHALT PLANT  
 WHEELING, ILLINOIS**

SHEET  
**3 OF 3**  
 PROJECT NUMBER: 2820  
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# Southern Portion of Proposed Town Center from Northgate Parkway - View Looking South

Dated: 8-9-16

Prepared By:



Location of Proposed Asphalt Plan



Exhibit received August 11, 2016

Existing Photo from Google Street View 2012



# CHICAGOLAND SURVEY COMPANY INC.

PROFESSIONAL DESIGN FIRM LICENSE NO: 184-005262 EXPIRES 04/30/2017

6501 W. 65TH STREET CHICAGO, ILLINOIS 60638 (773) 271-9447  
CHICAGOLANDSURVEY@SBCGLOBAL.NET

## ALTA/ACSM LAND TITLE SURVEY OF

PARCEL 1:

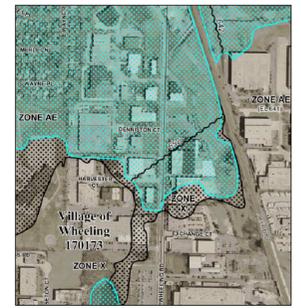
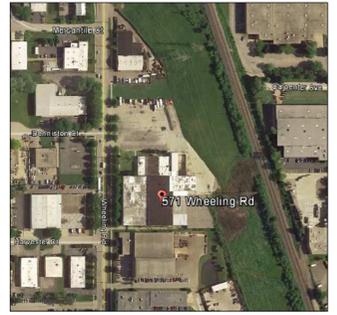
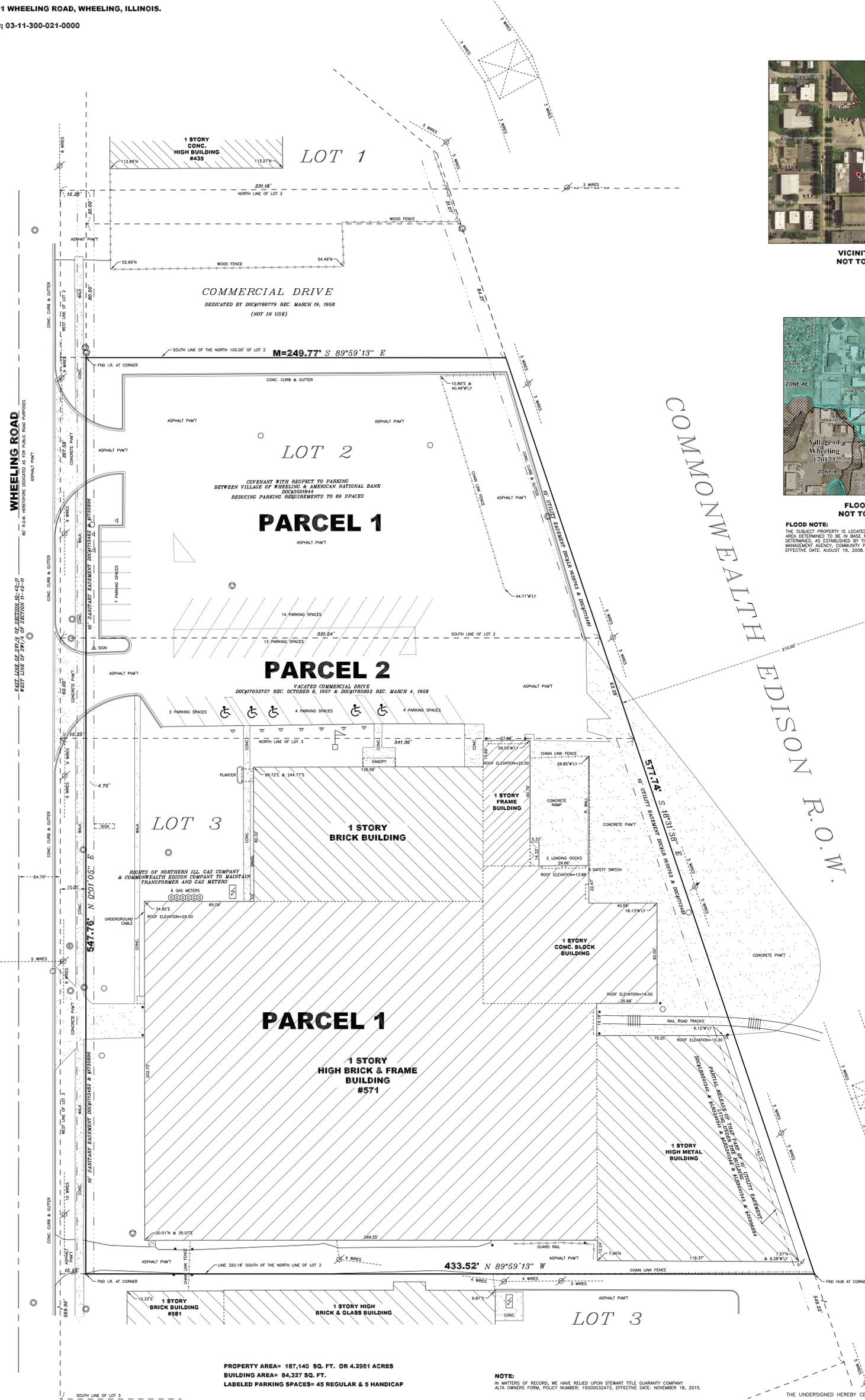
LOT 2 (EXCEPT THE NORTH 100 FEET THEREOF AND EXCEPT THE WEST 15.25 FEET THEREOF) AND THAT PART OF LOT 3 LYING NORTH OF A LINE DRAWN PARALLEL WITH AND 320.18 FEET SOUTH OF THE NORTH LINE OF LOT 3 (EXCEPTING FROM SAID PART OF LOT 3 THE WEST 15.25 FEET THEREOF), IN BLOCK 1 OF HERZOG'S 1ST INDUSTRIAL SUBDIVISION, A SUBDIVISION OF PART OF THE EAST 1/2 OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF FILED DECEMBER 13, 1955 AS DOCUMENT LR 1639763, AND CERTIFICATE OF CORRECTION FILED DECEMBER 14, 1956 AS DOCUMENT LR 1713481, IN COOK COUNTY, ILLINOIS.

PARCEL 2:

THAT PART OF VACATED COMMERCIAL DRIVE LYING SOUTH OF THE SOUTH LINE OF LOT 2, NORTH OF THE NORTH LINE OF LOT 3, EAST OF THE EAST LINE OF WHEELING ROAD (AS WIDENED BY PLAT OF DEDICATION REGISTERED AS DOCUMENT LR 2321517) AND LYING WEST OF THE EASTERLY LINE OF LOT 2 EXTENDED SOUTHERLY TO THE NORTHEASTERLY CORNER OF LOT 3 AFORESAID, IN BLOCK 1, IN HERZOG'S 1ST INDUSTRIAL SUBDIVISION OF PART OF THE EAST 1/2 OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF THE WEST 1/2 OF SECTION 11, TOWNSHIP 42 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO PLAT THEREOF REGISTERED IN THE OFFICE OF THE REGISTRAR OF TITLES OF COOK COUNTY, ILLINOIS, ON DECEMBER 13, 1955 AS DOCUMENT LR 1639763 AND CERTIFICATE OF CORRECTION THEREOF REGISTERED DECEMBER 14, 1956 AS DOCUMENT LR 1713481.

PROPERTY KNOWN AS: 571 WHEELING ROAD, WHEELING, ILLINOIS.

P.I.N.# 03-11-300-013-0000; 03-11-300-021-0000



**FLOOD NOTE:**  
THE SUBJECT PROPERTY IS LOCATED IN ZONE "AE" AREA DETERMINED TO BE IN BASE FLOOD ELEVATIONS DETERMINED AS ESTABLISHED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY COMMUNITY PANEL 17031 C 0068 J, EFFECTIVE DATE: AUGUST 19, 2008.

- LEGEND:**
- NORTH
- SOUTH
- EAST
- WEST
- O.L. ON LINE
- 3' X 3' MANHOLE
- MANHOLE
- CATCH BASIN
- POWER POLE
- LIGHT POLE
- SIGN POLE
- MONITORING WELL
- INLET
- GAS VALVE
- GUARD POST
- FIRE HYDRANT
- GAS METER
- WATER METER
- ELECTRIC METER
- WATER B. BOX
- WATER MANHOLE
- SPRINKLER
- ELECTRIC MANHOLE
- CLEANOUT
- HANDICAP PARKING SPACE
- TRANSFORMER
- FLAG POLE
- WOOD FENCE
- CHAIN LINK FENCE
- IRON FENCE
- PROPERTY LINE
- LOT LINE
- EASEMENT LINE

PROPERTY AREA= 187,140 SQ. FT. OR 4.2961 ACRES  
BUILDING AREA= 84,327 SQ. FT.  
LABELED PARKING SPACES= 45 REGULAR & 5 HANDICAP

**NOTE:**  
FOR BUILDING LINE AND OTHER RESTRICTIONS NOT SHOWN ON THE SURVEY PLAT, REFER TO YOUR ABSTRACT, DEED, AND LOCAL BUILDING REGULATIONS.

UTILITY DATA, OTHER THAN PHYSICAL EVIDENCE VISIBLE ON THE GROUND, IS SHOWN AS PROVIDED BY THE PRIVATE AND PUBLIC SOURCES AND SHOULD BE ASSUMED TO BE APPROXIMATE.

**NOTE:**  
THERE IS NO EVIDENCE OF CURRENT EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS, RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIRS, SITE USE AS SOLID WASTE DUMP, SWAMP OR SANITARY LANDFILL.

**NOTE:**  
IN MATTERS OF RECORD, WE HAVE RELIED UPON STEWART TITLE GUARANTY COMPANY ALTA OWNERS FORM, POLICY NUMBER: 15000032473, EFFECTIVE DATE: NOVEMBER 18, 2015.

**BEARING BASIS:**  
THE WESTERLY LINE OF SUBJECT PROPERTY IS ASSUMED TO BE NORTH 0 DEGREES 01 MINUTES 05 SECONDS EAST.

**ZONING:**  
1-2: LIMITED INDUSTRIAL DISTRICT  
FOR BULK RESTRICTION REFER TO:  
1. COMMUNITY DEVELOPMENT DEPARTMENT  
VILLAGE OF WHEELING  
2. COMMUNITY DEVELOPMENT  
WHEELING, ILLINOIS 60090  
TEL: (847) 459-2600

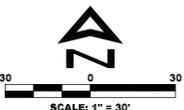
THE UNDERSIGNED HEREBY CERTIFIES, AS OF DECEMBER 1, 2015, TO:  
571 SOUTH WHEELING ROAD, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY  
STEWART TITLE COMPANY  
571 WHEELING LLC, AN ILLINOIS LIMITED LIABILITY COMPANY  
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1.2, 3.4, 6.(a), 7.(a), 7.(b), 7.(c), 8.9, 11.(a), 16, 17, 18, 19 OF TABLE A THEREOF.

DATED THIS 1 ST. DAY OF DECEMBER, 2015.



LICENSE EXPIRES 11/30/2016

BY: *Robert P. Auld*  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3384



ORDERED BY: ERIC MCLOYD  
FILE NO.: 115-65

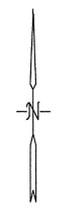
REVISED (CERTIFICATE ONLY) 12/01/2015  
REVISED (LEGAL DESCRIPTION) 08/17/2015  
FIELD WORK COMPLETED 07/22/2015

Exhibit received August 11, 2016

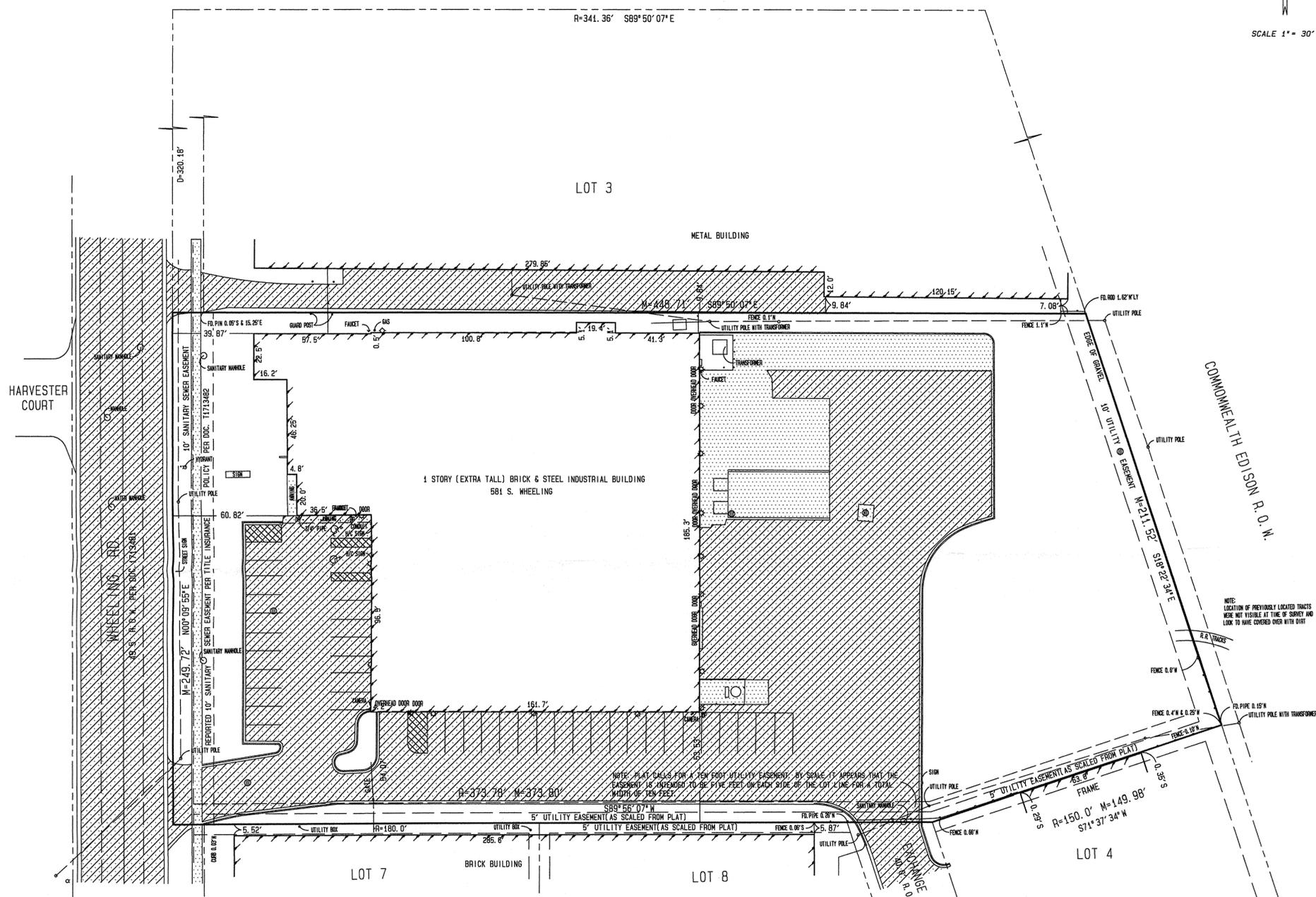
# LAND TITLE SURVEY

Order No. 1613163

THAT PART OF LOT 3, IN BLOCK 4 OF HERZOG'S 1st INDUSTRIAL SUBDIVISION, HEREINAFTER DESCRIBED, LYING SOUTH OF A LINE DRAWN PARALLEL WITH AND 320.18 FEET SOUTH OF THE NORTH LINE OF LOT 3 IN HERZOG'S 1st INDUSTRIAL SUBDIVISION OF PART OF THE EAST HALF OF SECTION 10, TOWNSHIP 42 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF THE WEST HALF OF SECTION 11, TOWNSHIP 42 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF REGISTERED IN THE OFFICE OF THE REGISTRAR OF TITLES ON DECEMBER 13, 1955 AS DOCUMENT T-1639763 AND CERTIFICATE OF CORRECTION THEREOF REGISTERED DECEMBER 14, 1956 AS DOCUMENT NO. T-1713481, ALL IN COOK COUNTY, ILLINOIS.



SCALE 1" = 30'



NOTE: LOCATION OF PREVIOUSLY LOCATED TRACTS WERE NOT VISIBLE AT TIME OF SURVEY AND LOW TO HAVE COVERED OVER WITH GRASS

**GENERAL NOTES:**

- THE LEGAL DESCRIPTION, EASEMENTS AND BUILDING LINES SHOWN HEREON ARE PER TITLE COMMITMENT PREPARED BY FIRST AMERICAN TITLE INSURANCE COMPANY, KNOWN AS FILE NO. 1496991 WITH AN EFFECTIVE DATE OF AUGUST 29, 2006.
  - NOTE: UTILITY INFORMATION SHOWN IS BASE ON VISIBLE SURFACE EVIDENCE ONLY  
WARNING: BEFORE BEGINNING ANY CONSTRUCTION CONTACT J. U. L. I. E. FOR UTILITY STAKING AT 800-892-0123
  - UTILITY LETTERS WERE NOT PROVIDED BY CLIENT FOR OUR REVIEW THUS INFORMATION CONTAINED THEREIN HAS NOT BEEN SHOWN.
  - PER TABLE "A" OF 2016 ALTA/NSPS ALTA STANDARDS, ITEMS 6a AND 17 REQUIRE THE CLIENT TO PROVIDE THE INFORMATION TO BE SHOWN ON THE PLAT. AT THE TIME THE PLAT WAS ISSUED, NO INFORMATION HAD BEEN PROVIDED AND THUS NOT SHOWN.
  - PER FIRM FLOOD INSURANCE RATE MAP PANEL 68 OF 832 AND KNOWN AS MAP NUMBER 1703410006U MAP REVISED AUGUST 19, 2008, THE SUBJECT PROPERTY FALLS WITHIN ZONE AE (FLOOD AREA WITH BASE FLOOD ELEVATIONS DETERMINED) AND ZONE X (AREA WITH A 0.2% ANNUAL CHANCE FLOOD AND AREA DETERMINED TO BE DETERMINED OUTSIDE THE 0.2% ANNUAL CHANCE FLOOD).
  - EASEMENT PROVISIONS PER SCHEDULE B OF FIRST AMERICAN TITLE INSURANCE COMPANY, FILE NO. 2760077.
- EXCEPTION 1 - 20 DOES NOT PERTAIN TO MATTERS OF SURVEY  
EXCEPTION 21 - APPLIES AND AFFECTS THE PROPERTY AS SHOWN  
EXCEPTION 22 - APPLIES AND AFFECTS THE PROPERTY AS SHOWN  
EXCEPTION 23 - DOES NOT PERTAIN TO MATTERS OF SURVEY  
EXCEPTION 24 - DOCUMENT WAS NOT PROVIDED FOR OUR REVIEW THUS INFORMATION CONTAINED THEREIN HAS NOT BEEN REVIEWED (WOULD APPLY AND AFFECT THE PROPERTY AS SHOWN)  
EXCEPTION 25 - DOES NOT PERTAIN TO MATTERS OF SURVEY  
EXCEPTION 26-31 DOES NOT PERTAIN TO MATTERS OF SURVEY

**LEGEND**

---	LIMITS OF BUILDING
---	R - RECORD
---	M - MEASURED
D	DEED
○	FOUND IRON PIPE
○	SET IRON PIPE
○	HYDRANT
○	LIGHT
-----	CONCRETE
-----	PAVERS
-----	ASPHALT

**LDI LAND DIVISIONS, Inc.**  
Professional Surveying Services  
P.O. Box 935  
West Dundee, IL 60119  
(847) 841-8305 (847) 551-9171  
fax (847) 551-9193



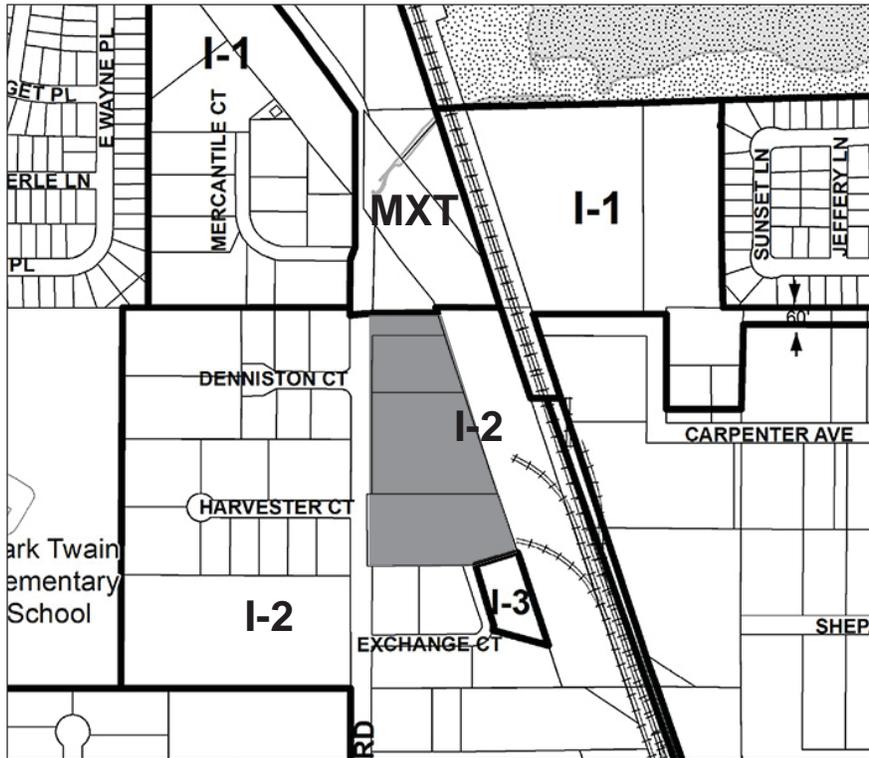
TO: FIRST AMERICAN TITLE FOR COMMITMENT FILE NO.: 2760077  
321 CENTER STREET, LLC.

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 6a, 7a AND 17 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON: JUNE 15, 2016.  
DATE ISSUED: JUNE 17, 2016.

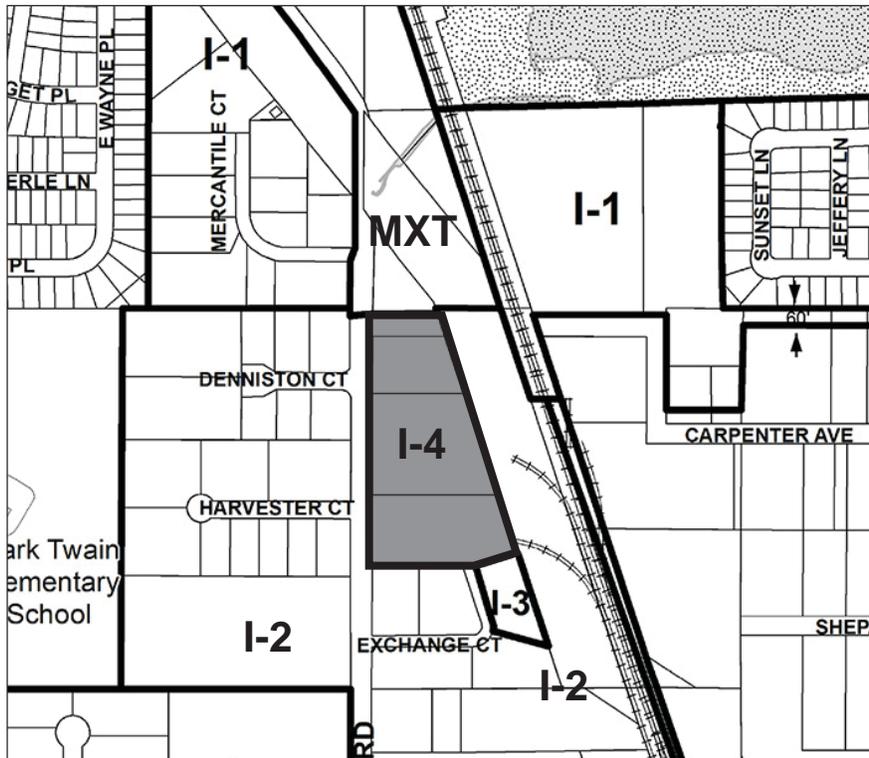
*CWS*  
COURTNEY W. SHROPSHIRE IV, PLS 2783

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Exhibit received August 11, 2016



EXISTING ZONING (I-2)



PROPOSED ZONING  
(REZONE I-1 PORTION TO I-2)

# Memorandum

To: Tim Winter  
From: Tim Doron, Senior Traffic Consultant  
Date: August 10, 2016  
Re: 571 S. Wheeling Road, Wheeling Illinois  
Project No: 16-03-1710

As requested, Sam Schwartz Consulting (Sam Schwartz) has performed a site access evaluation for the Orange Crush property ("Site") located at 231 S. Wheeling Road in Wheeling Illinois. Builders Asphalt intends to move its operation approximately 600 feet south to 571 S. Wheeling Road. The purpose of the relocation is to expand and modernize the plant operations with newer technology and a more efficient site plan with updated equipment to produce asphalt faster and more cost effectively. The new plant will also have the ability to recycle used product on site thus reducing the need to have extra trucks enter the site and haul away the recyclables to another plant. The move also furthers the development of one of the Villages prime commercial districts. Our evaluation process included a site visit, review of new plans, application of AutoTurn software, and 12 hour traffic counts. This data was applied to the new site to evaluate the new traffic operations.

## Site Area Context

The existing site is a 3.781-acre parcel and is located at 231 S. Wheeling Road in Wheeling Illinois. The new site will be located at 571 S. Wheeling Rd on a 7.27-acre parcel. The surrounding land uses are primarily industrial or office. The Metra North Central Rail line borders the sites on the east. Wheeling Road, a Collector Road under the jurisdiction of the Village of Wheeling, provides the primary access to the sites, and connects north with IL 68 Dundee Road, and to the south to Hintz Road. Wheeling Road is a four undivided lane roadway with no turn lanes in the vicinity of the site and a posted speed limit of 30 miles per hour. Parking and access to the Metra Train Station is located further north. Further south are larger industrial sites. As such, both the existing and new site are ideally located to accommodate truck traffic and provide regional access via a good system of collector and arterial roadways. The new site location is shown on Figure 1.

## Existing Conditions

### *Traffic Volumes*

Wheeling Road is an IDOT designated Class 2 Truck Route with a 2014 Average Daily Traffic count (ADT) of 9,850 vehicles per day. This volume is more than 30% less than the 2010 counts of 14,900 vehicles per day (VPD). The property is served by two one-way driveways, with the south drive generally operating as inbound only, and the north drive serving as outbound only. Trucks using the site include larger debris hauling dump trucks, as well as medium type asphalt trucks. Sam Schwartz performed turning movement counts at the existing site drives on Thursday

Exhibit received August 11, 2016

Orange Crush  
571 South Wheeling Road  
Wheeling Illinois  
August 10, 2016

June 9, 2016 for twelve hours from 6:00 AM to 6:00 PM. Although volumes change depending on the construction season and activity, there are approximately 230 trips both into and out of the site daily. The vast majority of trips are trucks (+95%). An examination of the count data indicates that the great majority of vehicle traffic at the site drives occur outside the peak hours of Wheeling Road. This activity is shown on Figure 2 which identifies peak hourly volumes at the drive for four time periods: Peak hours of the street (both AM and PM), and peak hour of the site or generator (both AM and PM). A summary of results are as follows:

Site traffic volumes vs:

- Morning peak hour of Wheeling Road (7:00 AM to 8:00 AM)
  - 23 vehicles in and 22 vehicles out
- Evening peak hour of Wheeling Road (4:30 PM to 5:30 PM)
  - 0 in and 0 out
- Morning peak hour of the site/generator (9:00 AM to 10:00 AM)
  - 40 in and 42 out
- Evening peak hour of the site/generator (12:00 PM to 1:00 PM)
  - 23 in and 20 out

Daily site traffic, approximately 474 total trips in and out together, represents approximately 4.8 percent of the daily traffic on Wheeling Road (9,850 vehicles per day).

## **New Site Evaluation**

### *Traffic*

Traffic for the new site is not expected to increase of any significance by virtue of modernizing the plant. The amount of traffic generated by the site will continue to be metered by the amount of roadway construction projects. Some traffic will actually be reduced by means of the recycler now being located on the property. This will eliminate the necessity of large trucks hauling away recovered debris for recycling at another facility – estimated at 30-50 trucks per day. Additionally peak traffic to and from the site is not during the peak hour of the adjacent streets.

### *Site Operations*

The site will be serviced by medium and large trucks. The new site plan allows for a much more efficient circulation and operations. Vehicles will continue to enter through the south drive and exit from the north drive in a counter clockwise pattern. The trucks will generally make one of three major movements on site.

- Enter and back into the broken material/debris pile and exit
- Enter and drive/back to the Virgin Aggregate bin located on the east side of the site
- Enter and proceed to the scales and product silos before exiting

Orange Crush  
571 South Wheeling Road  
Wheeling Illinois  
August 10, 2016

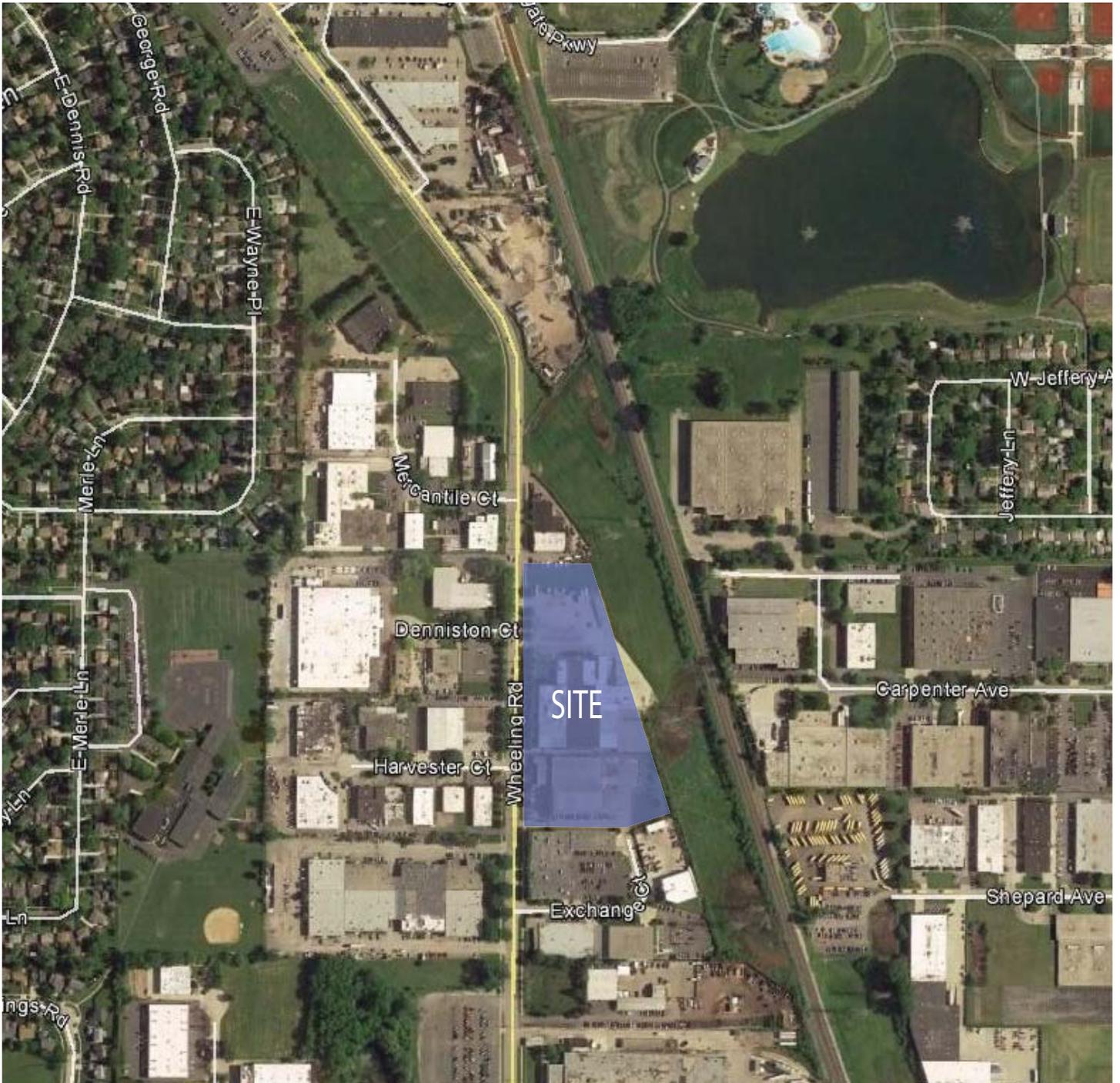
All three of these movements were tested using AutoTurn software that evaluated the ability of very large trucks performing these movements. For purposes of analysis a WB-50 truck model was used. This truck size meets or exceeds the expected vehicles on the property. The model indicates more than adequate room for trucks to enter the drives and maneuver on the property.

### **Finding and Conclusion**

The site works well with the new layout and increased size. The ability to recycle used debris on the site will reduce the need for trucks to enter and exit to haul away this material. This will reduce truck traffic on the street as well. Traffic operates under current conditions very well and is not expected to change. Traffic should continue to occur outside the busy times of the street. Consequently Wheeling Road, which is designed for the use by trucks as a designated truck route, will readily accommodate the traffic at the new site.



Not to Scale





Not to Scale

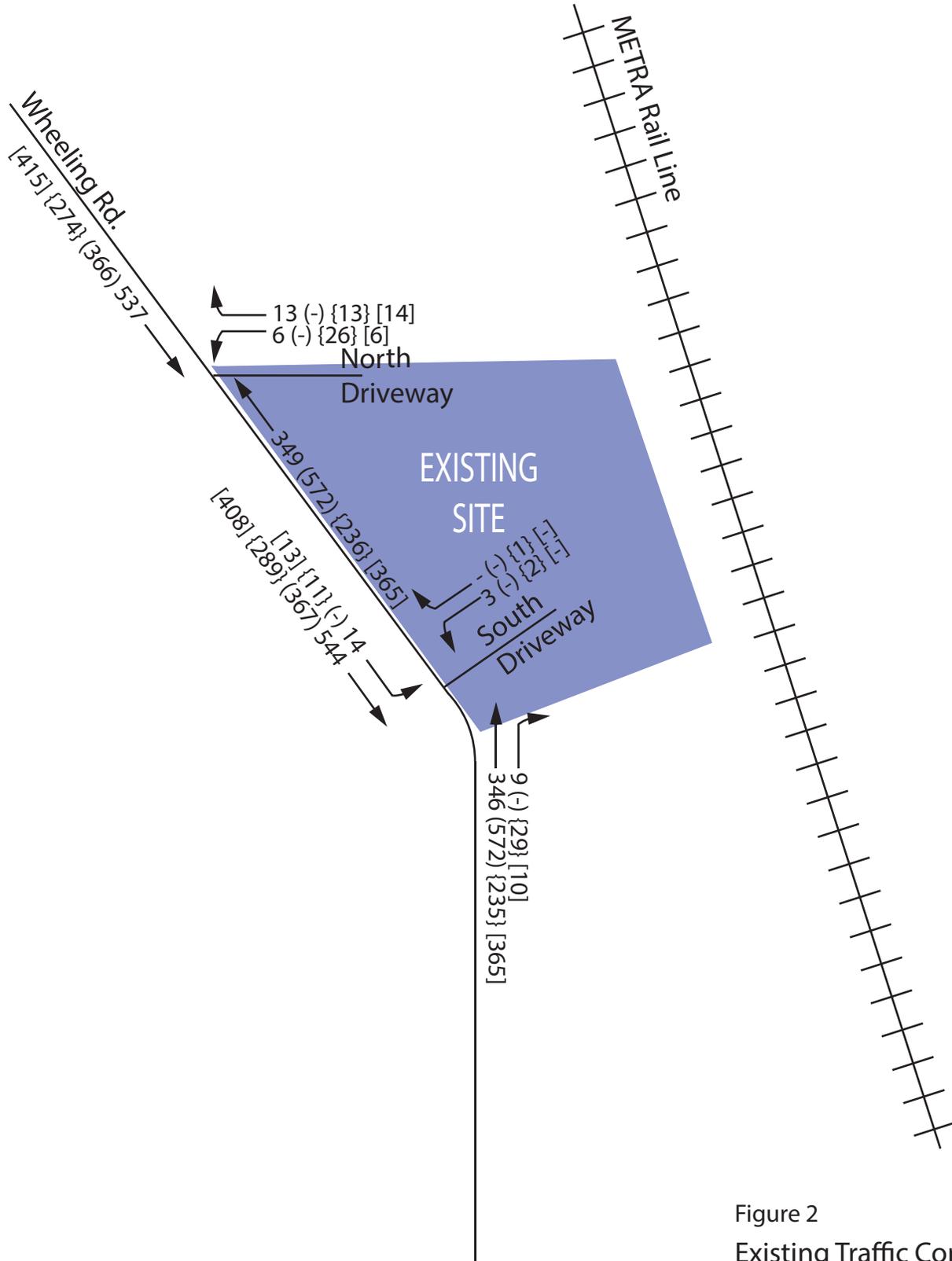
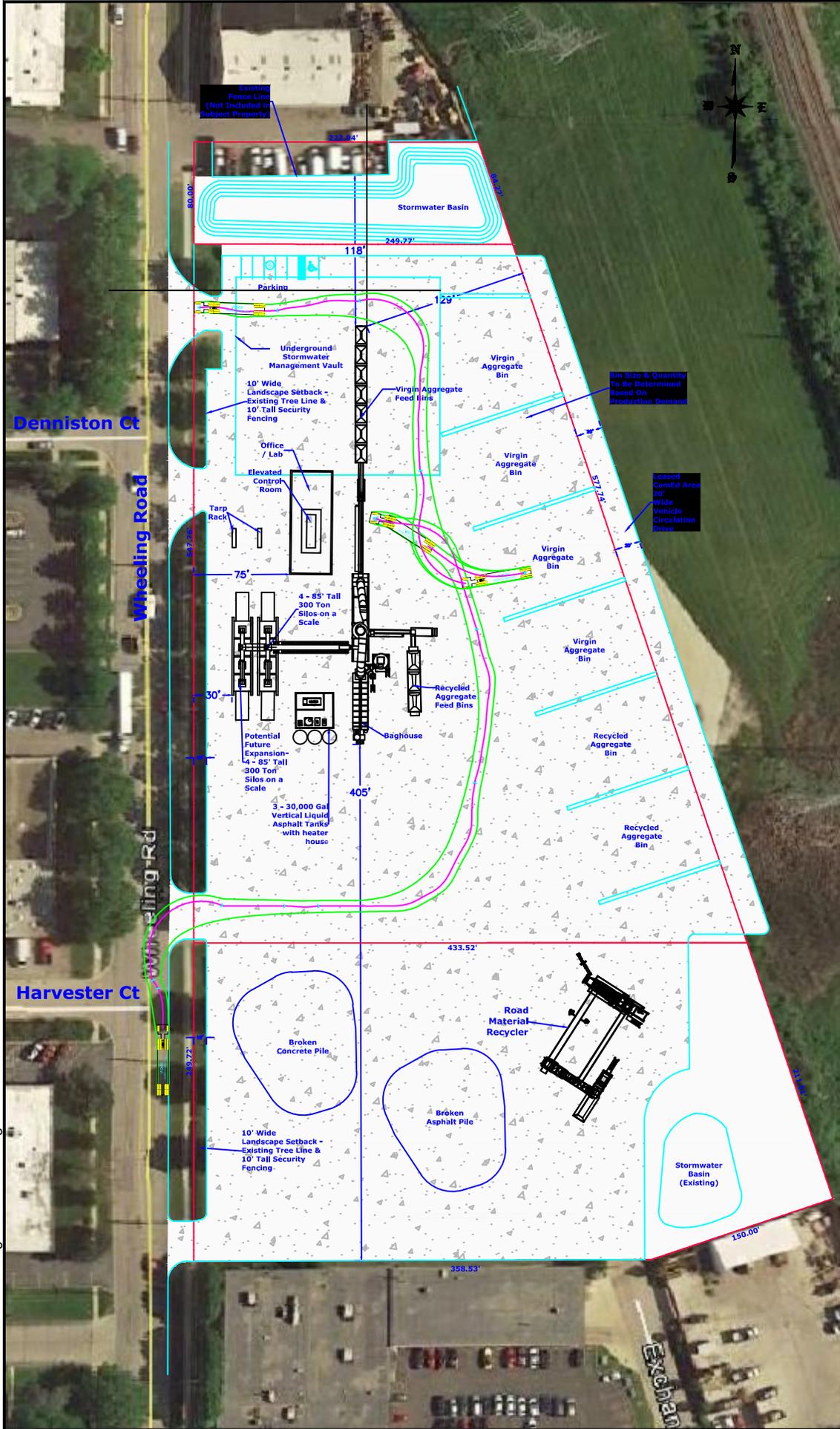


Figure 2

Existing Traffic Conditions

- xx = AM Peak Hour of Street
- (xx) = PM Peak Hour of Street
- {xx} = AM Peak Hour of Site
- [xx] = PM Peak Hour of Site





ORIE

**ORANGE CRUSH**

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REORE

**Sam Schwartz Engineering D.P.C.**

WERIE S: REE  
S: E 600  
IL 606

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SELE

FOR REVIEW & FILING  
NOT FOR CONSTRUCTION


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REORE

**ORANGE CRUSH RELOCATION**

31 S. WHEELING RD.  
WHEELING, ILLINOIS

ESI

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RWI

RW

KSB

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SLE

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REOREMER

16-03-1710

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AUGUST 5, 2016

RWI MER

**FIGURE 4**

EE##

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users shu es to range rush .d.g



**Report of Laura C. Green, Ph.D., D.A.B.T.**

Regarding the hot mix asphalt plant proposed by Pal Group, Inc.  
to be located at 571 S. Wheeling Road,  
Wheeling, Illinois

August 2, 2016

I, Dr. Laura C. Green, am a board-certified toxicologist, and the president of an independent consulting firm, Green Toxicology LLC. I received a B.A. in Chemistry from Wellesley College in 1975, and a Ph.D. in Nutrition and Food Science from the Massachusetts Institute of Technology (M.I.T.) in 1981. I also served on the part-time faculty of M.I.T. for 25 years, teaching environmental toxicology. Since 1978, I have specialized in assessing risks to health from many different sources and activities, including the production of hot mix asphalt.

I have been asked to assess whether the proposed hot mix asphalt plant would be harmful to the health of the community. As explained below, I have found that it would not.

Hot mix asphalt is the primary material used in the U.S. to pave roads, parking lots, driveways, runways, and other outdoor surfaces. It is produced at hot mix asphalt plants by combining heated, liquefied asphalt cement (a product made at petroleum refineries) with heated sand, gravel, stone, and other "construction aggregate," including pieces of reclaimed asphalt pavement. As a finished product, hot mix asphalt pavement contains about 5% asphalt cement and about 95% construction aggregate.

Hot mix asphalt plants are quite common, with almost 4,000 such facilities located in the U.S. Hot mix asphalt is usually produced at temperatures of between 300 and 325 degrees Fahrenheit, and needs to be applied (at job-sites) at no cooler than about 250 degrees Fahrenheit. It therefore needs to be produced relatively close to where it will be used. This is why hot mix asphalt is produced at thousands of small facilities near where the material will be applied, and often relatively close to residential communities, rather than at a few large facilities at distant locations.

The gases and vapors emitted from the exhaust stack of a hot mix asphalt plant are as listed in the table below. As shown, more than 99% of the exhaust gases are made up of four chemicals — nitrogen, water vapor, oxygen, and carbon dioxide. These substances are emitted by essentially all combustion sources, including gasoline and diesel-powered vehicles, fuel-fired home heating systems, fossil fuel-fired electric power plants, wood or biomass-fired power plants, and many other sources. Asphalt plants also emit small amounts of particles to ambient air, including inhalable particles typically size-classified as PM<sub>10</sub> and PM<sub>2.5</sub>.

Typical concentrations of gases and vapors emitted from a hot-mix asphalt facility	
Compound	Concentration in stack gas
Nitrogen (+argon & other inert gases)	63 %
Water	22 %
Oxygen	10 %
Carbon dioxide	4 %
Carbon monoxide	0.008 %
Nitrogen oxides	0.005 %
Sulfur dioxide	0.0006%
Volatile and semi-volatile organic compounds (VOCs and SVOCs)	0.001%
Total	100 %

Some of the compounds emitted by hot-mix asphalt plants (and other combustion sources) can be hazardous *if people are exposed to high enough levels*. The relevant question when considering the potential air quality and human health impacts of emissions from a particular plant is whether the emissions are so large as to affect local air quality and health, or whether these emissions are instead low enough that they would be harmless.

Hot mix asphalt plants have been extensively tested with regard to airborne emissions, including trace amounts of volatile and semi-volatile organic compounds, and their potential environmental and health impacts have been assessed by many federal and state agencies. The U.S. Environmental Protection Agency (EPA), has extensively tested, or overseen the testing of, hot mix asphalt production.<sup>1</sup> On the basis of this testing, the U.S. EPA has determined that even very large facilities – which produce 1,000,000 tons per year of hot-mix asphalt – are not major sources of air pollution.<sup>2</sup>

The facility proposed here will produce no more than about 200,000 tons per year of hot-mix asphalt. It will be equipped with more modern and efficient pollution-controls than those evaluated by U.S. EPA. And it will be fueled by natural gas, which is a cleaner, higher-quality fuel than was used in some of the hot mix asphalt plants that were evaluated by U.S. EPA.

<sup>1</sup> See <http://www.epa.gov/ttn/chief/ap42/ch11/related/c11s01.html> and associated links, especially the *Emission Assessment Report* at <http://www.epa.gov/ttn/chief/ap42/ch11/related/ea-report.pdf>.

<sup>2</sup> See *Federal Register*: February 12, 2002, Volume 67, No. 29, pp. 6521-6536, available at [http://frwebgate.access.gpo.gov/cgi-bin/getdoc.cgi?dbname=2002\\_register&docid=02-3348-filed.pdf](http://frwebgate.access.gpo.gov/cgi-bin/getdoc.cgi?dbname=2002_register&docid=02-3348-filed.pdf). Note that hot mix asphalt plants are there called “asphalt concrete manufacturing plants.”

The quality of outdoor air in neighborhoods surrounding hot mix asphalt plants has been analyzed many times, including in studies that my colleagues and I have conducted. For example, we studied the impacts and potential risks to community-health posed by a large hot mix asphalt facility located in a very densely populated neighborhood in northern Virginia, quite close to Washington, D.C. We found that, despite the close proximity of the plant to many homes, nuisance odors were not a problem. We also found that pollutant emissions were sufficiently small so as not to be harmful to health. And monitoring of outdoor air at a neighboring elementary school revealed no more than acceptably small concentrations of particulate matter (measured as PM<sub>10</sub>).

Because the proposed Wheeling Road facility will be both smaller and newer than the facility that we studied in the Washington D.C. area, its impacts will be even smaller than those that we found to be safe. Of course, it will be important that the facility be run using best management practices, including suppression of dust from aggregate-handling, use of asphalt-deodorizers, if and as needed, and sound-minimizing equipment and techniques.

Fortunately, local measurements indicate that the quality of outdoor air is good. Data collected and compiled by the Illinois Environmental Protection Agency at the nearest ambient air monitoring station to the proposed site (in Northbrook, Illinois) show that (i) concentrations of major pollutants are well within safe limits, *per* U.S. EPA's National Ambient Air Quality Standards, and (ii) concentrations of trace pollutants are typical of those found in the region and generally regarded as acceptably small.

Since (i) impacts from the proposed facility would be small, and (ii) air quality in the area is currently good, I find that operation of proposed project would not significantly change air quality in the area and would not render air unhealthful. In other words, I believe that the proposed facility would be safe for Wheeling.

---

Pal Group, Inc.  
321 Center Street  
Hillside, IL 60162

August 9, 2016

Attn: Mr. Tim Winter, P.E.

Re: Builders Asphalt, LLC  
Relocation of Wheeling Plant

Dear Mr. Winter:

This report summarizes our measurements, analysis, and recommendations concerning environmental noise emissions from the proposed Builders Asphalt plant to nearby Wheeling residential properties. We reviewed site drawings, zoning maps and other information provided by Builders Asphalt. We conducted hot mix asphalt sound level measurements at the Skokie and existing Wheeling plants and ambient sound level readings at the proposed plant site.

Builders Asphalt is proposing to relocate batch plant operations from their existing 45 year old Wheeling plant located at 231 Wheeling Road to 571 Wheeling Road. As part of the relocation, equipment will be modernized and recycling operations (rock crushing) will be added. Hot mix asphalt equipment and operations will be similar to the newly opened Skokie Plant.

The proposed location will be about 600 ft. south of the existing operations and is bounded by Wheeling Road on the west and Union Pacific freight and passenger tracks on the east. Chicago Executive Airport is about one mile to the southeast. The new parcel is zoned I-2. The site is surrounded by industrially zoned property. The nearest residential zones are located about 840 ft to the southwest (Mark Twain Elementary School), 880 ft to the northwest (residences on E. Wayne Place and S. Wayne Place) and 920 ft to the northeast (residences on W. Manchester Drive and Sunset Lane). Business zoned property is more distant than residential zoned property.

#### Noise

We used acoustical measurements of asphalt and vehicle operations at the Builders Asphalt Skokie Plant (collected on July 15, 2016) and Orange Crush Wheeling Plant (collected on July 18, 2016). Asphalt plant noise sources included low-frequency noise from the burner and rotating drum, and the baghouse with associated induced-draft fan and motors. Asphalt loading from silos onto trucks included multiple bursts of compressed air, and a buzzer to signal truck drivers.

Recycling equipment in Skokie is not yet operational. The existing Wheeling Plant has no recycling operations and all aggregate is trucked into the site. We relied on acoustical measurements conducted at the O'Hare plant on July 28 and 29, 2011. We found that the major sources of recycler noise were the crusher and screener, and an infrequently used jackhammer.

Wheeling has noise performance noise standards contained within the zoning code. The Wheeling noise regulations are based on zoning of the receiving property and a table of octave

Exhibit received August 11, 2016

band sound levels (Section 8.24.050(b)) is provided. Since no averaging period is stated, it is assumed that readings are energy-averaged, as is common with other ordinances and standards including Illinois. For the sake of clarity and brevity, overall A-weighted sound levels (not the eight octave frequency bands contained in the standards) will be referenced in this report.

Under the Wheeling regulations, a maximum sound level of 55 dBA is permitted between industrial and residential district boundaries. Between industrial and business districts, 59 dBA is permitted. As a point of reference, the sound level of a truck on city streets at a distance of 100 ft is approximately 74-78 dBA.

To document area noise exposure due to the ambient environment, we conducted early morning sound level measurements on the proposed site at 571 Wheeling Road between 6:51 and 8:03 a.m. on Friday, July 18, 2016. The microphone was positioned 200 ft east of Wheeling Road and captured sounds from the roadway, UP passenger/freight tracks, Chicago Executive Airport and other activities in the area. We feel that this location is typical for receivers in the area. The resulting ambient sound level of 59 dBA exceeds the Wheeling limits for residential use.

In order to model noise emissions from the modernized and relocated plant, we used CadnaA to calculate sound levels at noise-sensitive receivers. CadnaA is industry-accepted software used to calculate sound levels due to multiple sources at multiple receiver points. In order to be conservative, the shielding effect of any concrete walls and structures within the plant has not been modeled. Figure 1, which follows this report, presents the results of our modeling superimposed on an aerial photograph.

Figure 1 identifies the calculated sound level at the nearest residentially zoned property, which is about 840 ft west of the plant. At the eastern property line of the Mark Twain Elementary School, the calculated sound level is 49 dBA. Sound levels at more distant residences are slightly lower. We calculated a sound level of 48 dBA at the corner of W. Manchester Drive and Sunset Lane and 47 dBA at the corner of E. Wayne Place and S. Wayne Place.

Vibration

Under Section 19.10.080(j), Wheeling specifies maximum permitted displacement along residential boundaries. We used data from the O'Hare plant conducted on September 21, 2011 between 12:00 and 2:00 pm and compared it with limits contained in the ordinance.

From our observations, vibration will be emitted by recycling and asphalt plant operations. For recycling, the crusher is the main source (measured at a distance of 225 ft at O'Hare). For the asphalt plant, the drum is the primary source (measured at 120 ft). The distance to the nearest Wheeling residential property line is 840 ft, which greatly exceeds measurement distances at the O'Hare plant.

**Table 1. Results of Vibration Measurements at Orange Crush O'Hare Plant  
September 21, 2011**

	Displacement, in		
<u>Frequency</u>	<u>Wheeling Limit</u>	<u>O'Hare Crushing</u>	<u>O'Hare Drum</u>
0 to 10	0.0008	0.000004	0.000001
10 to 20	0.0005	0.0000005	0.0000003
20 to 30	0.0002	0.0000002	0.0000001
30 to 40	0.0002	0.00000002	0.00000001
40 and over	0.0001	0.00000001	0.000000007

Measured vibration levels are lower than the Wheeling standard by factors of 100 to 10,000. At residential property lines, Wheeling vibration limits can be easily met.

Conclusion

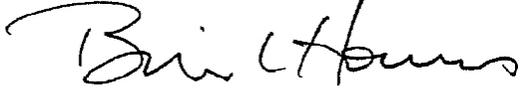
At the nearest residential zoning area, we calculate a sound level of 49 dBA, using sound level readings from the existing Wheeling plant, the newly opened Skokie plant and the O'Hare plant. The modeled sound level is in compliance with limits established in the noise ordinance section of the Wheeling zoning code of 55 dBA. The result of our early morning ambient sound level readings on the site of 59 dBA exceeds the Wheeling limit for residential zoning. Thus, expected noise at receiver locations from transportation and other sources can likely exceed emissions of the relocated plant.

Readings from the O'Hare asphalt and recycling plant were used and compared Wheeling limits. The Wheeling limits exceed measured levels by at least a factor of 100, so the limits can be met at residences.

If you have questions concerning this report, please do not hesitate to contact us.

Respectfully submitted,

Shiner + Associates, Inc.

A handwritten signature in black ink that reads "Brian L. Homans". The signature is written in a cursive style with a large initial "B".

Brian L. Homans

BLH/mo/07

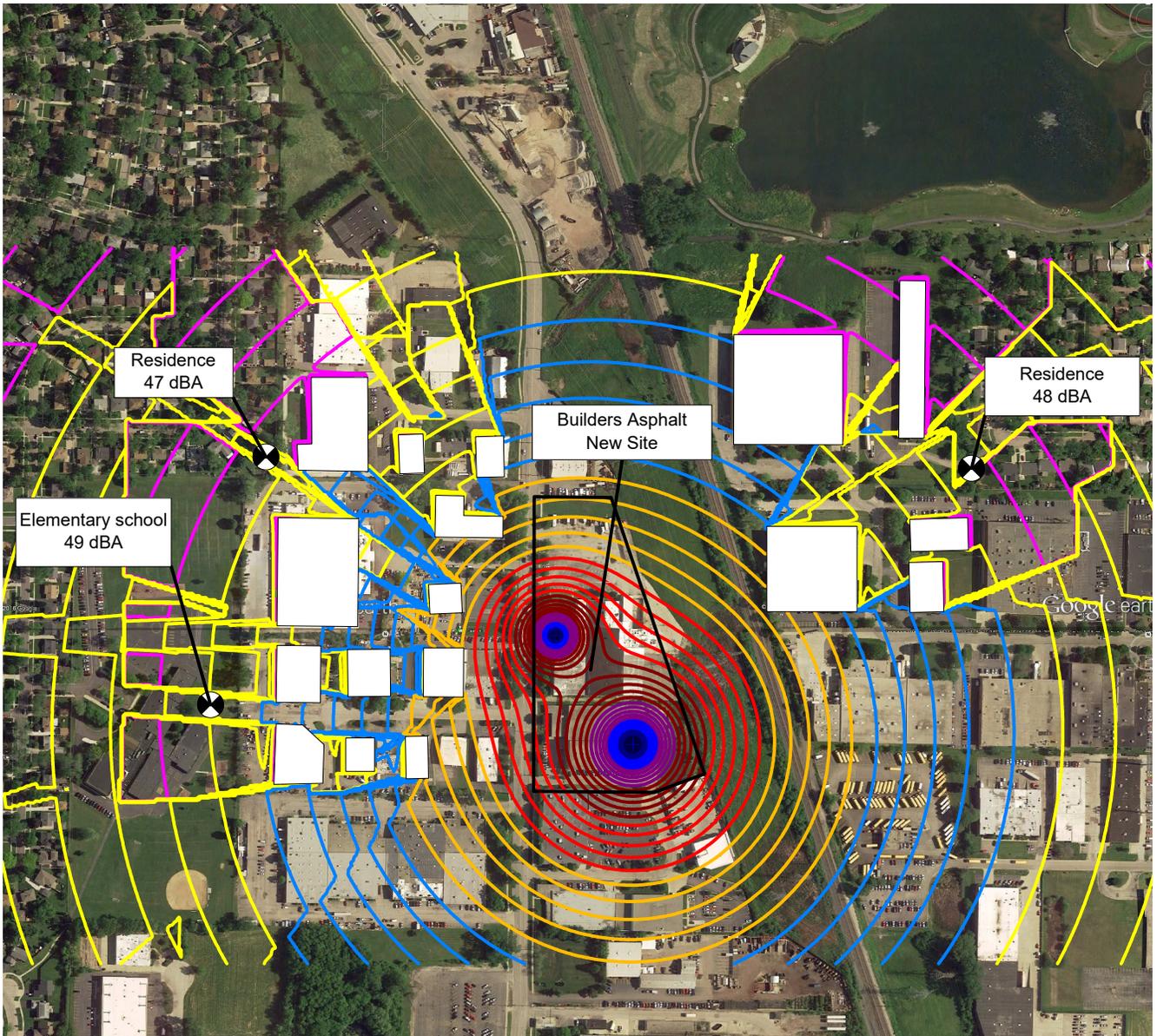
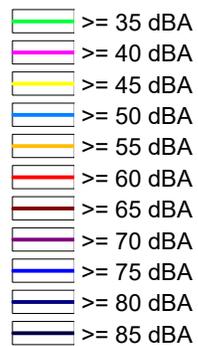


Figure 1. Builders Asphalt

Predicted New Wheeling Plant Sound Level Contours

dB re 20  $\mu$ Pa, A-weighted



Scale 1 : 3286

Rev

Date

0

7/27/16